
ROCKWALL CITY COUNCIL MEETING

Monday, April 21, 2025 - 4:00 PM

City Hall Council Chambers - 385 S. Goliad St., Rockwall, TX 75087

I. Call Public Meeting to Order

II. Work Session

1. Hold work session with representative(s) from Parkhill regarding the city facilities planning study.
2. Hold a work session to discuss the 2025 Existing Conditions Report, prepared by the Planning Department of the City of Rockwall, including land use, key developments, current conditions, and future planning considerations based on recent population growth, development, and legislative changes.

III. Executive Session

The City of Rockwall City Council will recess into executive session to discuss the following matter as authorized by chapter 551 of the Texas government code:

1. Discussion regarding (re)appointments to city regulatory boards and commissions, pursuant to §551.074 (Personnel Matters).
2. Discussion regarding possible sale/purchase/lease of real property (1) in the vicinity of downtown, (2) in the vicinity of The Harbor District, and (3) in the vicinity of SH-205, pursuant to Section §551.072 (Real Property) and Section §551.071 (Consultation with Attorney).
3. Discussion regarding Economic Development prospects, projects, and/or incentives, pursuant to §Section 551.087 (Economic Development)

IV. Adjourn Executive Session

V. Reconvene Public Meeting (6:00 P.M.)

VI. Invocation and Pledge of Allegiance - Councilmember McCallum

VII. Proclamations / Awards / Recognitions

1. Rockwall Fire Department "Best Practices" Recognition by TX Fire Chiefs Association

VIII. Appointment Items

1. Appointment with Planning & Zoning Commission representative to discuss and answer any questions regarding planning-related cases on the agenda.

IX. Open Forum

This is a time for anyone to address the Council and public on any topic not already listed on the agenda or set for a public hearing. To speak during this time, please turn in a (yellow) "Request to Address City Council" form to the City Secretary either before the meeting or as you approach the podium. Per Council policy, public comments should be limited to three (3) minutes out of respect for others' time. On topics raised during Open Forum, please know Council is not permitted to respond to your comments during the meeting since the topic has not been specifically listed on the agenda (the Texas Open Meetings Act requires that topics of discussion/deliberation be posted on an agenda not less than 72 hours in advance of the Council meeting). This, in part, is so that other citizens who may have the same concern may also be involved in the discussion.

X. Take Any Action as a Result of Executive Session

XI. Consent Agenda

These agenda items are routine/administrative in nature, have previously been discussed at a prior City Council meeting, and/or they do not warrant Council deliberation. If you would like to discuss one of these items, please do so during "Open Forum."

1. Consider approval of the minutes from the April 7, 2025 city council meeting, and take any action necessary.
2. Consider approval of an ordinance temporarily altering (reducing) the speed limit on the IH-30 frontage roads during (re)construction within the corporate city limits, and take any action necessary. **(2nd Reading)**
3. Consider authorizing the City Manager to execute a professional engineering services contract with Kimley-Horn and Associates, Inc. for the Forest Trace Reconstruction Project in an amount not to exceed \$234,100, to be paid for out of the 2018 Street Bond funds and water/wastewater funds, and take any action necessary.
4. Consider authorizing the City Manager to execute a professional engineering services contract amendment with Kimley-Horn and Associates, Inc. to perform additional engineering design services and specifications for the drainage for the North Lakeshore Drive (State Highway 66 to Masters Boulevard) Reconstruction Project in the amount of \$258,100, to be paid for by 2018 Street Bond funds, and take any action necessary.

XII. Action Items

If your comments are regarding an agenda item below, you are asked to speak during Open Forum.

1. Discuss and consider proposed changes to the city's solid waste collection services contract, and take any action necessary.
2. **MIS2025-004** - Discuss and consider a request by Phil Wagner of the Rockwall Economic Development Corporation (REDC) for the approval of a Miscellaneous Request for a Variance to the Utility Placement requirements of the General Overlay District Standards to allow overhead utilities along a portion of SH-276 between John King Boulevard and Rochelle Road and a portion of Corporate Crossing [FM-549] between the IH-30 Frontage Road and SH-276, City of Rockwall, Rockwall County, Texas, being right-of-way, and take any action

necessary.

XIII. Public Hearing Items

If you would like to speak regarding an item listed below, please turn in a (yellow) "Request to Address City Council" form to the City Secretary either before the meeting or as you approach the podium. The Mayor or Mayor Pro Tem will call upon you to come forth at the proper time. Please limit your comments to no more than three minutes.

1. **Z2025-011** - Hold a public hearing to discuss and consider a request by Tyler Adams of Greenlight Studio on behalf of Matt Zahm of ZAPA Investments, LLC for the approval of an **ordinance** for a *PD Development Plan* for seven (7) *Townhomes* on a 0.87-acre parcel of land identified as Lot 3, Block A, Harbor Hills Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 32 (PD-32), situated within the *Interior Subdistrict* and the *Residential Subdistrict*, generally located on the northeast side of Glen Hill Way, northwest of the intersection of Glen Hill Way and Ridge Road [FM-740], and take any action necessary **(1st Reading)**.
2. **Z2025-012** - Hold a public hearing to discuss and consider a request by David Gamez for the approval of an **ordinance** for a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision* on a 0.17-acre parcel of land identified as Lot 3, Block A, Gamez Addition, City of Rockwall, Rockwall County, Texas, zoned Single Family 7 (SF-7) District, situated within the Southside Residential Neighborhood Overlay (SRO) District, addressed as 614 E. Boydston Avenue, and take any action necessary **(1st Reading)**.
3. **Z2025-013** - Hold a public hearing to discuss and consider a request by Brandon Spruill of Spruill Homes on behalf of Hallie Fleming for the approval of an **ordinance** for a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision* on a ten (10) acre tract of land identified Tract 22-02 of the W. M. Dalton Survey, Abstract No. 72, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, addressed as 588 Cornelius Road, and take any action necessary **(1st Reading)**.
4. **Z2025-014** - Hold a public hearing to discuss and consider a request by Alexander Trujillo for the approval of an **ordinance** for a *Specific Use Permit (SUP)* for *Minor Automotive Repair Garage* on a 2.692-acre parcel of land identified Lot 5, Block A, Platinum Storage Addition, City of Rockwall, Rockwall County, Texas, zoned Commercial (C) District, situated within the IH-30 Overlay (IH-30 OV) District, addressed as 1460 T. L. Townsend Drive, Suite 116, and take any action necessary **(1st Reading)**.
5. **Z2025-015** - Hold a public hearing to discuss and consider a request by Javier Silva of JMS Custom Homes for the approval of an **ordinance** for a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision* and a *Guest Quarters/Secondary Living Unit* on a 0.42-acre parcel of land identified as Lot 2, Block 1, Shaw Addition, City of Rockwall, Rockwall County, Texas, zoned Single-Family 7 (SF-7) District, addressed as 403B S. Clark Street, and take any action necessary **(1st Reading)**.
6. **Z2025-016** - Hold a public hearing to discuss and consider a request by Ryan Joyce for the approval of an **ordinance** for a *Specific Use Permit (SUP)* for an *Accessory Building* on a 2.71-acre parcel of land identified as Lot 6, Block B, Northgate Addition, City of Rockwall,

Rockwall County, Texas, zoned Planned Development District 88 (PD-88) [*Ordinance No. 19-26*] for Single-Family 1 (SF-1) District land uses, addressed as 2201 Sanderson Lane, and take any action necessary **(1st Reading)**.

XIV. Adjournment

This facility is wheelchair accessible and accessible parking spaces are available. Request for accommodations or interpretive services must be made 48 hours prior to this meeting. Please contact the City Secretary's Office at (972) 771-7700 or FAX (972) 771-7727 for further information.

The City of Rockwall City Council reserves the right to adjourn into executive session at any time to discuss any of the matters listed on the agenda above, as authorized by Texas Government Code ¶ 551.071 (Consultation with Attorney) ¶ 551.072 (Deliberations about Real Property) ¶ 551.074 (Personnel Matters) and ¶ 551.087 (Economic Development)

I, Kristy Teague, City Secretary for the City of Rockwall, Texas, do hereby certify that this Agenda was posted at City Hall, in a place readily accessible to the general public at all times, on the 17th day of April at 4:00 PM and remained so posted for at least 72 continuous hours preceding the scheduled time of said meeting.

Kristy Teague, City Secretary

Date Removed



City of Rockwall
The New Horizon

MEMORANDUM

TO: Mayor and Council
FROM: Joey Boyd, Assistant City Manager
DATE: April 14, 2025
SUBJECT: City Facilities Planning Study

In May 2023, Mayor Johannesen brought forward the need to conduct a study for a long-term plan to address facilities necessary to provide essential city services. In May 2024, the City hired Parkhill to perform a Facilities Planning Study to evaluate the needs, space requirements, and potential costs for capital improvements for the following facilities and related departments:

1. City Hall
2. Municipal Service Center
3. New Fire Station 1
4. Police Station
5. Downtown Parking Opportunities

Michael Howard with Parkhill will present the results of the completed study at the meeting



CITY OF ROCKWALL

PLANNING AND ZONING DEPARTMENT MEMORANDUM

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council

CC: Mary Smith, *City Manager*
Joey Boyd, *Assistant City Manager*

FROM: Ryan Miller, *Director of Planning and Zoning*

DATE: April 21, 2025

SUBJECT: Existing Conditions Report and Tapestry Segmentation Report

On December 3, 2018, the City Council approved *Ordinance No. 18-48* adopting the OURHometown Vision 2040 Comprehensive Plan. Recently, the City Council has indicated that they would like to appoint a Comprehensive Plan Advisory Committee (CPAC) to review and update the plan. In anticipation of this update, City staff has updated the *2017 Existing Conditions Report* and the *Tapestry Segmentation Report*. These documents provide the City Council, Planning and Zoning Commission, CPAC, and other various boards and commissions with updates on changes in socioeconomic demographics, the built environment, residential/commercial permitting, and public facilities in the community, and provide a good background when looking at what updates need to be addressed in the future update of the Comprehensive Plan. In addition to preparing these reports, staff will be holding a work session with the City Council on April 21, 2025 to provide a brief introduction to the report and to highlight some of the major changes observed by staff in the preparation of these documents.



JANUARY
2025



Discovery Statue
Installed 2023


City of Rockwall

While digging a well in 1851, pioneers Terry Utter, Wade Buchanan, Boynton, and William Clay Burdett discovered one of 2 million of rock wells that ran underground throughout the city. The City of Rockwall was platted and created in 1834 in honor of their finding.

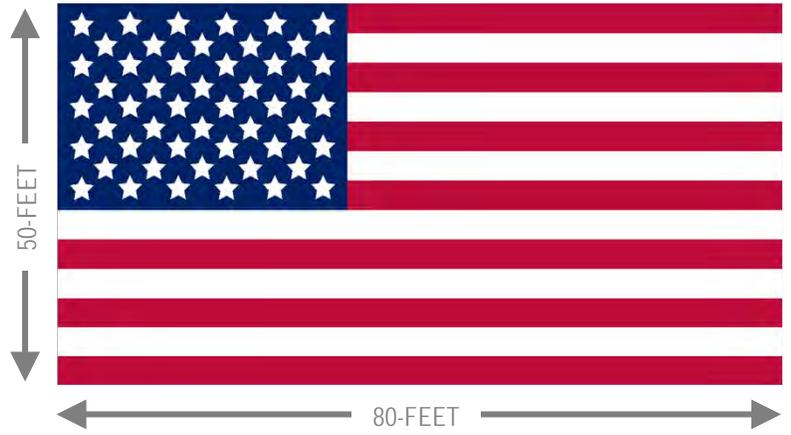
In 2004, local artist Jim Bryant designed a small sculpture to commemorate the discovery which became the model for this statue. The Rockwall City Council unanimously then installed project in 2021, which Mr. Bryant donated, donating his time and talents in the creation of this final statue.

EXISTING CONDITIONS REPORT | 2025 UPDATE

THE 2025 EXISTING CONDITIONS REPORT WAS PREPARED BY THE PLANNING AND ZONING DEPARTMENT FOR THE PURPOSE OF ASSISTING THE COMPREHENSIVE PLAN ADVISORY COMMITTEE (CPAC) IN UPDATING THE COMPREHENSIVE PLAN AND TO PROVIDE OTHER VARIOUS BOARDS AND COMMISSIONS, CITY STAFF AND THE CITIZENS OF ROCKWALL WITH A STATE OF THE CITY AS OF APRIL 1, 2025.

FLAG DATA AND CALCULATIONS

| | |
|---------------------|-----------------------------------|
| DESIGN | STARTED IN 2016 |
| FAA APPROVAL | MARCH 30, 2020 |
| TXDOT AGREEMENT | JUNE 28, 2021 |
| BUILDING PERMIT | AUGUST 2, 2022 |
| COMPLETION DATE | SEPTEMBER 21, 2022 |
| MANUFACTURER | US FLAG & FLAGPOLE SUPPLY COMPANY |
| LENGTH | 50-FEET |
| WIDTH | 80-FEET |
| SQUARE FEET | 4,000 SF |
| MATERIAL | POLYESTER |
| MAXIMUM WIND RATING | 90 MPH |
| FLAG WEIGHT | 120 LBS |
| FLAGPOLE HEIGHT | 190-FEET |
| BASE POLE SIZE | 42-INCHES |
| TOP POLE SIZE | 8%-INCHES |
| GROUND ELEVATION | 535-FEET (MSL) |



** SHOWS THE SIZE OF THE FLAG COMPARED TO CITY HALL*





ROCKWALL'S UNITED STATES FLAG ALONG IH-30

WELCOMING PEOPLE TO THE CITY OF ROCKWALL, AS THEY CROSS LAKE RAY HUBBARD, IS A HUGE UNITED STATES FLAG. THIS IMPRESSIVE FLAG IS 50 x 80-FEET IN SIZE AND IS AFFIXED TO A 190-FOOT TALL FLAG POLE. THE FLAG WEIGHS 120 POUNDS AND WAS INSTALLED IN 2022. TAKING LEAD ON GETTING THIS MONUMENT INSTALLED WAS FORMER COUNCIL MEMBER BENNIE DANIELS AND CURRENT MAYOR TRACE JOHANNESSEN ALONG WITH CITY LEADERS AND STAFF. THE FLAG IS A PROUD SYMBOL OF ROCKWALL'S PATRIOTISM AND CAN BE SEEN FROM PORTIONS OF MESQUITE, ROWLETT, GARLAND, AND DALLAS.

ACKNOWLEDGEMENTS

CITY COUNCIL

TRACE JOHANNESEN, *MAYOR*
CLARENCE JORIF, *MAYOR PRO-TEM*
TIM McCALLUM
MARK MOELLER
SEDRIC THOMAS
DENNIS LEWIS
ANNA CAMPBELL

PLANNING AND ZONING COMMISSION

JEAN CONWAY, *CHAIRMAN*
JOHN WOMBLE, *VICE-CHAIRMAN*
ROSS HUSTINGS
KYLE THOMPSON
JAY ODOM
JOHN HAGAMAN
CARIN BROCK

ADMINISTRATION

MARY SMITH, *CITY MANAGER*
JOEY BOYD, *ASSISTANT CITY MANAGER*

STAFF MEMBERS

RYAN MILLER, AICP, *DIRECTOR OF PLANNING & ZONING*
HENRY LEE, AICP, *SENIOR PLANNER*
BETHANY ROSS, *PLANNER*
ANGELICA GUEVARA, *PLANNING TECHNICIAN*
MELANIE ZAVALA, *PLANNING AND ZONING COORDINATOR*

LANCE SINGLETON, *GIS SUPERVISOR*
OLESYA POWERS, *GIS ANALYST*
CURTIS AANERUD, *GIS TECHNICIAN*
JOHN GARRETT, *GPS TECHNICIAN*

AMY WILLIAMS, PE, *CITY ENGINEER/DIRECTOR OF PUBLIC WORKS*
JONATHAN BROWNING, PE, *ASSISTANT CITY ENGINEER*
MADELYN PRICE, *CIVIL ENGINEER*

TRAVIS SALES, *DIRECTOR OF PARKS AND RECREATION*

CRAIG FOSHEE, *PLANS EXAMINER*



CITY HALL



EXECUTIVE SUMMARY

2025 EXISTING CONDITIONS REPORT UPDATE

The *2025 Existing Conditions Report Update* for the City of Rockwall, Texas, provides a comprehensive analysis of demographic, infrastructure, land use, housing trends, economic development, public facilities, and parks and open space since the original *2017 Existing Conditions Report*. This document is intended to inform City staff, elected officials, and stakeholders/citizens of key developments, current conditions, and future planning considerations as the City continues to evolve in response to population growth, development, and legislative changes.

POPULATION AND DEMOGRAPHICS

As of 2024, the City of Rockwall is home to approximately 52,882 residents, reflecting continued steady growth since 2000. Rockwall has experienced a cumulative growth of 210.16% over the past 24 years as the community has continued to be seen as a desirable place to live. Based on the current population trends, the City's population is projected to exceed 80,000 by 2040. This assumption assumes a continued 3.00% compound annual growth rate (CAGR).

Demographic trends reveal a diversifying population. The share of residents identifying as *Non-White* has grown from 8.70% in 2000 to 33.97% in 2020, with notable increases among those identifying as *Two or More Races*. The fastest growing age cohort in the City is residents aged 65 years and older, indicating a need for expanded senior services and long-term care options. Meanwhile, the percentage of the population under 35 has slightly declined; however, residents in the younger age cohorts continue to rise. This is most likely due to the desire to be in the Rockwall Independent School District (RISD), which continues to be a top school district in the State of Texas.

TRANSPORTATION AND INFRASTRUCTURE

The City of Rockwall's infrastructure system has expanded significantly to meet the demands of its growing population. Since 2017, over 77-miles of new water lines have been constructed. The City also maintains over 370-miles of water lines, nearly 300-miles of wastewater lines, and more than 150-miles of stormwater lines. Despite this growth, much of the City's infrastructure was constructed prior to 2010.

Rockwall continues to rely on the North Texas Municipal Water District (NTMWD) for water and wastewater treatment. Plans are currently underway for the construction of a new water tower near Mims Road -- *scheduled for completion by 2027* --, and improvements to wastewater basins that includes decommissioning the Squabble and Buffalo Creek Wastewater Treatment Plants in favor of treatment by the NTMWD.

The City operates 39 lift stations, an unusually high number of lift stations for a City, which is second only to Corpus Christi in the State of Texas. This is due to the natural topography enjoyed by the City. Rockwall is actively addressing remaining gaps in infrastructure, including sewer service to unserved subdivisions such as the Lake Rockwall Estates Subdivision.

LAND USE, ZONING, AND FUTURE DEVELOPMENT

The City has grown by nearly 3,000 parcels since 2017 and now encompasses approximately 19,258-acres. Of this total, 55.80% is developed, 31.40% remain vacant, and 12.80% is public right-of-way; however, after accounting for lots that have been platted or entitled, public ownership, and changes from the legislature in how City's can annex and regulate the Extraterritorial Jurisdiction (ETJ), only about 2,850 acres remain truly unentitled and vacant. This puts the City's vacant land at around 14.80% to 20.75%. Of this 2,850-acres of vacant land, 58.80% or 1,676.65-acres are designated for residential land uses, 24.06% or 686.03-acres are designated for non-residential land uses, and 24.06% or 488.11-acres are anticipated to be parks and open space.

Land use patterns have largely followed the path laid out in the OURHometown Vision 2040 Comprehensive Plan; however, legislative changes -- *most notably HB 347 (2019) and SB 2038 (2023)* -- have significantly curtailed the City's ability to plan for and regulate land within the ETJ and banned unilateral annexation. This has resulted in the City decreasing its ETJ by over 3,000 acres since 2017. These legislative changes have also affected the buildout projections and built/vacant numbers established with the OURHometown Vision 2040 Comprehensive Plan.

HOUSING AND NEIGHBORHOODS

As of 2025, Rockwall has 23,013 housing units, representing a 25.77% increase since 2017. Single-family housing continues to dominate the City's housing stock, comprising 78.08% of the total housing stock. The share of multi-family units has increased from 12.38% in 2017 to 17.41% in 2025. Most single-family homes have been constructed in the past 35 years, and values have appreciated significantly, with homes built between 2010-2019 experiencing the highest increase in value at 96.65%.

Housing growth has occurred on both sides of IH-30, though the majority of new subdivisions are now occurring south of the corridor. Remodel permit data indicates reinvestment in older neighborhoods, especially within the Downtown, Lakeside Village, and Chandler's Landing areas.

NON-RESIDENTIAL DEVELOPMENT

The City of Rockwall has added over 2.4 million square feet of new non-residential space since 2021, valued at approximately \$494,000,000.00. Commercial development, especially along the IH-30 corridor, has positioned the City as a regional retail center. Major projects include a HEB Grocery Store and an IKEA Furniture Store. At the end of 2024, retail vacancy was only 3.98%, which is far below regional, state, and national averages.

Industrial and office sectors in the City have also expanded, with major projects including STREAM Distribution Center, Seefried Distribution Center, Rayburn Electric Cooperative, and several medical office buildings. From 2017 to early 2025, Rockwall issued 160 non-residential building permits totaling more than \$880,000,000.00.

Rockwall's regional draw is supported by a robust sales tax base -- *-\$533 per capita as of 2024, the highest among comparable cities* --, and a strong labor market. The City's labor force has nearly tripled since 2000 and currently comprises roughly 28,940 residents. While the majority of this labor force works in nearby communities (*e.g. Dallas*), the Employment to Residence or ER ratio is 1.12. This confirms the City of Rockwall is a net importer of labor and a regional employment center despite also being a bedroom community of the City of Dallas.

PUBLIC FACILITIES

The City of Rockwall maintains a robust network of public service facilities. This includes 15 city buildings totaling over 160,000 square feet on 152.50-acres. Public safety infrastructure consists of four (4) fire stations, three (3) police facilities (*including the new Public Safety Center South in the Rockwall Technology Park*), and support facilities such as the Regional Firearms Training Center. Rockwall County also maintains seven (7) facilities within City's corporate limits. These include the courthouse, library, and detention center. These facilities collectively cover over 380,000 square feet of building area and 34.087-acres of land. The Rockwall Independent School District operates 15 schools and three (3) support campuses with a combined building area of over 2.25 million square feet on 489-acres in the City.

PARKS, TRAILS AND OPEN SPACE

Rockwall has demonstrated a significant commitment to public parks and recreational programs. As of 2025, the City maintains 708.13-acres of public parkland, 310.38-acres of future parkland, and over 74-acres of private parks. The system includes community parks, neighborhood parks, greenways, mini-parks, sports complexes, specialty parks, and school parks. Notable developments include the dedication of the Hero's Memorial Park in 2024. The City also maintains 259 linear miles of sidewalks, 12 linear miles of park trails, and nearly six (6) miles of mountain bike trails. A major trail corridor along John King Boulevard is approximately 38.00% complete and will eventually total nearly 14 linear miles. Open space and floodplain areas total ~3,445-acres, with 1,964-acres dedicated to conservation/open space. The City is currently updating its Parks, Recreation and Open Space Master Plan, which will help guide planning efforts in the future.

Community engagement and entertainment remain a priority for the City of Rockwall, with the City earning the distinction of the *Free Live Music Capital of North Texas*. The City currently hosts over 100 free live music events annually and a variety of festivals, family events, and celebrations that reinforce its sense of community and identity.

CONCLUSION

The *2025 Existing Conditions Report* highlights Rockwall's ongoing growth and its emergence as a key destination for commerce, residential development, and regional employment. The City's investments in infrastructure, public services, housing, and recreational amenities have positioned it to effectively manage continued development while maintaining a high quality of life for its residents.

As Rockwall prepares for future growth, this report offers a factual basis for decision-making by City leadership, boards and commissions, and community stakeholders. The insights provided herein will support thoughtful planning, guide land use and development strategies, and assist in prioritizing capital improvements. The findings of this report affirm Rockwall's status as a dynamic and attractive community, and will inform ongoing efforts to enhance livability, expand opportunity, and preserve the characteristics that define the City's identity.



COMMUNITY QUICK FACTS

| | | | | | | | | | |
|--|--|--|---|--|---|--|--|--|---|
|  52,882 RESIDENTS |  25,280 (49.52%) MALE RESIDENTS |  25,868 (50.48%) FEMALE RESIDENTS |  8.80% FOREIGN BORN RESIDENTS |  2,467 VETERAN POPULATION |  \$71,932.00 COST OF LIVING |  \$128,501.00 MEDIAN COST OF LIVING |  \$127,548.00 HOUSEHOLD INCOME |  \$115,086.00 AVG. DISP. INCOME |  40.4 MEDIAN AGE |
|  370.80 LM WATER LINES |  298.63 LM WASTEWATER LINES |  150.56 LM STORMWATER LINES |  3 WATER TOWERS |  2 WATER PUMP STATIONS |  39 LIFT STATIONS |  274.64 LM STREETS |  63.75 LM ALLEYWAYS |  259.06 LM SIDEWALKS |  12.28 LM PARK TRAILS |
|  30.09 SQ. MI. CITY AREA |  12% ↑ DAYTIME POPULATION |  4.11% 2020-2024 CAGR POPULATION GROWTH |  BETWEEN 14.80% - 20.75% VACANT LAND |  23,013 HOUSING UNITS |  17,696 SINGLE-FAMILY HOMES |  4,007 MULTI-FAMILY UNITS |  22 MULTI-FAMILY DEVELOPMENTS |  937 ASSISTED LIVING UNITS |  100 GOVERNMENT UNITS |
|  62% RESIDENTS LIVE 10 MINS OF A PARK |  24.745¢ PROPERTY TAX RATE |  99.7% HIGH SCHOOL GRAD RATE |  61.3% COLLEGE EDUCATION |  2,625 TOTAL BUSINESSES |  608.80-ACRE TECHNOLOGY PARK |  2500 EXPECTED TECH PARK EMPLOYEES |  \$533.26 PER CAPITA SALES TAX |  2038-2040 ESTIMATED BUILDOUT DATE |  78-948 – 84,243 ESTIMATED BUILD OUT POPULATION |
|  170.27-ACRES OF VACANT LAND ALONG IH-30 |  11.78 LINEAR MILES OF SHORELINE |  1 HOSPITAL |  2 EMERGENCY ROOMS |  14 OUTDOOR SIRENS |  8 CITY BUILDINGS |  4 FIRE STATIONS |  4 POLICE / COURT FACILITIES |  3 AMPHITHEATER / STAGES |  2 CITY-OWNED CEMETERIES |
|  36 EXISTING PARK DESTINATIONS |  100+ LIVE MUSIC EVENTS PER YEAR |  13 CITY BASEBALL FIELDS |  2 PICKLEBALL COMPLEXES |  2 CITY SWIMMING POOLS |  1 PUBLIC GOLF COURSES |  ~708.13 ACRES CITY PARKLAND |  11 RENTABLE PAVILIONS |  3 BOAT RAMPS |  3 CHILD SPRAY GROUNDS |
|  13 VETERINARY FACILITIES |  3 HARDWARE STORES |  12 GROCERY STORES |  21 CHILD DAYCARES |  ~178 RESTAURANTS |  ~34 CHURCHES |  10 HOTELS |  21 GAS STATIONS |  7 EV CHARGING STATIONS |  ~42 BANKING/ATM LOCATIONS |
|  ~285 AVERAGE SINGLE-FAMILY PERMITS PER YEAR |  ~17 AVERAGE COMMERCIAL PERMITS PER YEAR |  5 YEAR AVERAGE ~3,655 FIRE CALLS PER YEAR |  70% LOWER VIOLENT CRIME THAN TX AND US |  276 HISTORIC DISTRICT PROPERTIES |  33.39 MINUTES AVERAGE TRAVEL TIME TO WORK |  66° F AVERAGE TEMPERATURE |  54.4% AVERAGE HUMIDITY |  34.65" AVERAGE PRECIPITATION |  0" AVERAGE SNOWFALL |
|  #1 CITY IN STATE INCOME GROWTH |  A+ OVERALL CITY GRADE NICHE.COM |  SILVER MEDAL 'CONCERT BY THE LAKE' SERIES - 2024 BEST OF DFW AWARD |  'BEST SMALL CITY IN TX - 2024' WALLETHUB.COM |  TOP 25 MOST PAYCHECK-FRIENDLY IN US |  #3 PURCHASING POWER - 2024' SMARTASSET.COM |  #2-2024 FASTEST COUNTY GROWTH USNEWS.COM |  45 MILES TO INTERNATIONAL AIRPORT (DFW) |  527 WOMEN-OWNED BUSINESSES |  3,925 FIRE HYDRANTS |

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1 POPULATION AND DEMOGRAPHICS





INDEPENDENCE DAY AT HARRY MYERS PARK

THE ANNUAL INDEPENDENCE DAY CELEBRATION ON JULY 4TH IS HELD AT HARRY MYERS PARK AND INCLUDES LIVE MUSIC, FOOD TRUCKS, AND FAMILY ENTERTAINMENT. CONCLUDING THE CELEBRATION IS A FIREWORKS DISPLAY SET TO MUSIC. LAST YEAR'S DISPLAY LASTED ~23-MINUTES AND INVOLVED 1,100-POUNDS OF 412 UNIQUE FIREWORKS WITH SHELLS RANGING IN SIZE FROM THREE (3) TO SIX (6) INCHES.

DEMOGRAPHICS

Demographics are statistical data that relate to the population of a particular group within a geographic area. Understanding the demographic makeup of a city is helpful when writing regulatory policies and making policy decisions that affect the distribution of resources, development priorities, and community planning efforts. This data aids in ensuring that policies align with the needs and characteristics of diverse population groups, supporting equitable growth and fostering a community that is responsive to its residents' demographic profiles.

POPULATION

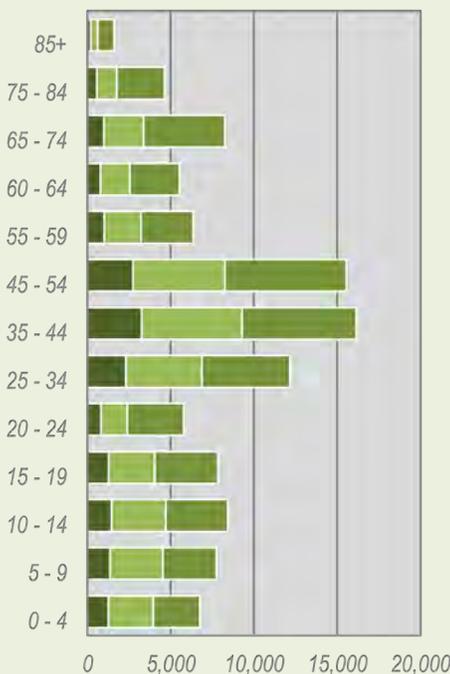
Population is a count of all inhabitants living within the corporate boundaries of a town, city, county, state, or country. Population counts within these jurisdictions are essential for forecasting infrastructure, public service, housing, and economic needs to ensure effective resource allocation and sustainable growth.

POPULATION GROWTH & PROJECTIONS

As of 2024, the City of Rockwall is home to approximately 52,882 residents, accounting for 40.32% of Rockwall County's total estimated population of 131,172 residents (North Central Texas Council of Governments [NCTCOG]).

FIGURE 1.4: POPULATION BY AGE

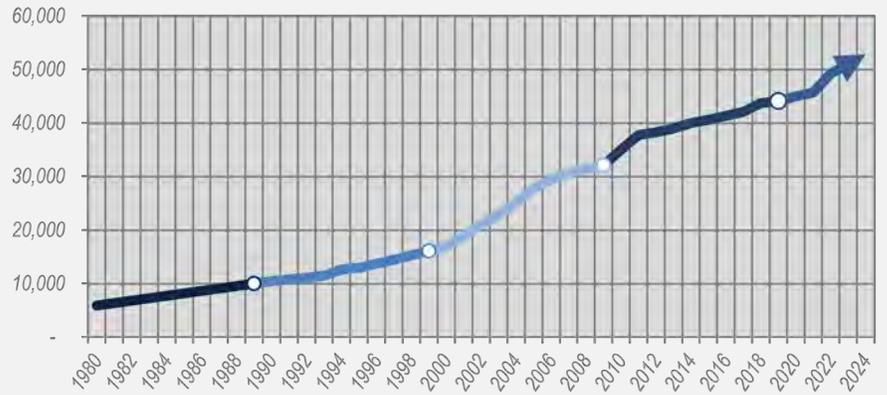
CHART KEY: DARK GREEN = 2000 CENSUS; LIGHT GREEN = 2010 CENSUS; GREEN = 2020 CENSUS



SOURCE: 2000, 2010, & 2020 US CENSUS

FIGURE 1.1: POPULATION GROWTH RATES 1980 – 2024

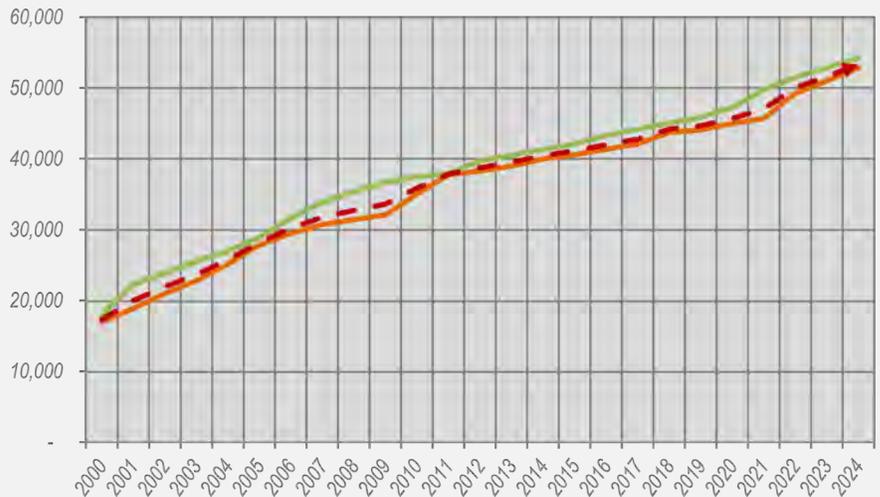
GROWTH RATES: 1980 – 1989 = 76.56% OR A CAGR OF 5.85%; 1990 – 1999 = 62.60% OR A CAGR OF 4.98%; 2000 – 2009 = 105.57% OR A CAGR OF 7.47%; 2010 – 2019 = 28.42% OR A CAGR OF 2.53%; 2020 – 2024 = 17.49% OR A CAGR OF 4.11%; [CAGR = COMPOUND ANNUAL GROWTH RATE]



SOURCE: CITY OF ROCKWALL PLANNING & ZONING DEPARTMENT

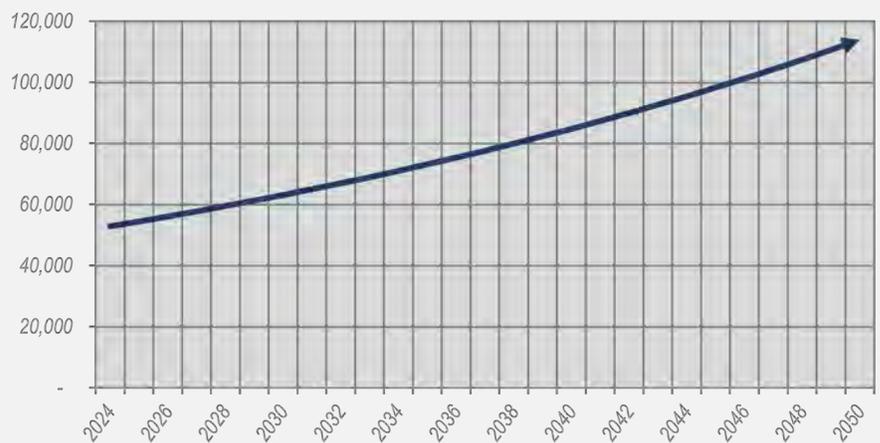
FIGURE 1.2: POPULATION BY AGENCY 2000 - 2024

CHART KEY: ORANGE = NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (NCTCOG); GREEN = US CENSUS BUREAU; RED = TREND LINE



SOURCES: LAND USE ASSUMPTIONS FOR IMPACT FEES REPORT (2024); US CENSUS BUREAU; NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (NCTCOG); CITY OF ROCKWALL PLANNING & ZONING DEPARTMENT

FIGURE 1.3: POPULATION PROJECTION ASSUMING A 3% COMPOUND ANNUAL GROWTH RATE



SOURCES: LAND USE ASSUMPTIONS FOR IMPACT FEES REPORT (2024); CITY OF ROCKWALL PLANNING & ZONING DEPARTMENT

Rockwall has experienced significant growth over the past 24 years, with the population increasing from 17,050 residents in 2000 to 52,882 residents in 2024 (refer to Figure 1.1). Based on a 3.00% Compound Annual Growth Rate (CAGR) -- as established in the 2024 Land Use Assumptions for Impact Fees Report --, projections indicate that the City of Rockwall could surpass a population of 80,000 residents by 2040 (see Figures 1.2 and 1.3); however, this growth trajectory may be influenced by future growth trends, the City and State's policies, the accessibility of infrastructure, and the availability of undeveloped land within the City (see Chapter 3, Current Land Use, Zoning & Future Land Use).

AGE DISTRIBUTION OF THE POPULATION

The age distribution of the population has remained relatively consistent since 2000 (see Figure 1.4), with the largest portion of the population being between the ages of 35-54 years old. The two (2) age cohorts (i.e. 35-44 years old and 45-54 years old) that make up this age distribution represent 27.61% of the total population; however, there has been a decreasing trend in this age range over the past three (3) US Census (see Table 1.1). In 2000, this age group represented 33.24% of the population, or 5,976 of the 17,976 people in the City. This decreased to 30.85% of the population in 2010, or 11,565 of the 37,490 people in the City. As of the 2020 US Census, the percentage of this group has further decreased to 27.61% of the population, or 14,149 of the 51,248 people in the City. With this being said, the overall growth of this age group has increased from 2000-2020 by 136.76% or an additional 8,173 people aging into or moving into the City from this age group.

The second largest segment of the population is between the ages of 0-19 years old. This group is represented by four (4) age cohorts (i.e. 0-4 years old, 5-9 years old, 10-14 years old, and 15-19 years old), and has also seen a decreasing trend since 2010 (see Table 1.1). In 2010, this segment of the population represented 31.64% of the population, or 11,863 of the 37,490 people in the City. In 2020, this decreased to 26.39% of the population or 13,525 of the 51,248 people in the City. Overall, this age group has increased 152.71% from 2000-2020 or an additional 8,173 people moving into or being born into this group.

The third -- and fastest growing -- segment of the population is people 65 years or older,

which has seen an increase of 403.51% growth from 2000-2010 or an additional 7,017 people aging into or moving into the City from this age group. This group is represented by three (3) age cohorts (i.e. 65-74 years old, 75-84 years old, and 85+ years old), and has seen an increasing trend between 2000-2020 growing from 9.67% of the population in 2000 to 17.09% of the population in 2020 (see Table 1.1).

The two (2) age groups with the smallest change in percentage of the population were the 20-34 years old age group and the 55-64 years old age group. The 20-34 years old group is composed of two (2) age cohorts (i.e. 20-24 years old and 25-34 years old), and has seen a decreasing trend with the 2000 US Census showing this age group at 17.44%. Inversely, the age group composed of residents 55-64 years old is showing an increasing trend, with the 2000 US Census showing this age group to be at 9.87% of the population and growing to 11.96% of the population by the 2020 US Census. This age group is composed of two (2) age cohorts (i.e. 55-59 years old and 60-64 years old). In addition, the 55-64 age group is the second fastest growing age group behind the age group consisting of people 65 years or older, growing at 245.55% from 2000-2020.

POPULATION BY GENDER

According to the 2020 US Census, the division of the population along gender lines is nearly equal with 49.52% of the population being male and 50.48% of the population being female (see Figure 1.5). These numbers have remained consistent between the 2010 US Census and 2020 US Census.

TABLE 1.2: POPULATION BY GENDER AND PERCENT CHANGE, 2000-2010

| GENDER | 2010 | 2020 | 2010-2020 |
|--------|--------|--------|-----------|
| MALE | 18,333 | 25,380 | 38.44% |
| FEMALE | 19,157 | 25,868 | 35.03% |
| | 37,490 | 51,248 | |

POPULATION BY RACE/ETHNICITY

ESRI'S Business Analyst shows that the ethnic composition of the City has continued to diversify from the 2000 US Census (see Figure 1.6). Over the past 20 years, all major ethnic divisions identified in the US Census have grown at a rate of over 500.00% with the exception

TABLE 1.1: TRENDS IN POPULATION BY AGE
KEY: RED = DECREASING TREND; BLUE = INCREASING TREND; GREEN = FASTEST GROWING COHORTS

| AGE | 2000 CENSUS | 2010 CENSUS | 2020 CENSUS | % CHANGE 2000-2020 |
|--------|-------------|-------------|-------------|--------------------|
| 0-19 | 29.77% | 31.64% | 26.39% | 152.71% |
| 20-34 | 17.44% | 16.31% | 16.95% | 177.13% |
| 35-54 | 33.24% | 30.85% | 27.61% | 136.76% |
| 55-64 | 9.87% | 10.58% | 11.96% | 245.55% |
| 65-85+ | 9.67% | 10.61% | 17.09% | 403.51% |

SOURCE: 2000, 2010, & 2020 US CENSUS

FIGURE 1.5: POPULATION BY GENDER

KEY: MALE = 25,380 (49.52%); FEMALE = 25,868 (50.48%)

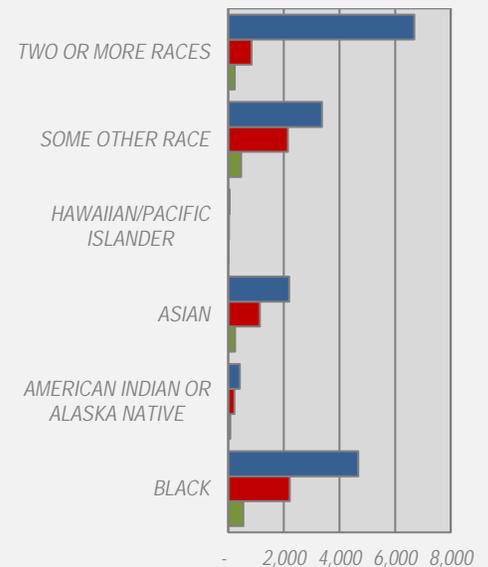


SOURCE: ESRI BUSINESS ANALYST & 2020 CENSUS

FIGURE 1.6: POPULATION BY RACE/ETHNICITY

NOTE: THE CHART EXCLUDES WHITE WHICH MADE UP 91.3% (16,421) OF THE POPULATION IN 2000, 82.4% (30,909) OF THE POPULATION IN 2010, AND 66.03% (33,834) OF THE POPULATION IN 2020.

KEY: GREEN = 2000 CENSUS; RED = 2010 CENSUS; BLUE = 2024 ESRI BUSINESS ANALYST



SOURCE: 2000, 2010, & ESRI BUSINESS ANALYST

08.8% PERCENT OF THE POPULATION THAT IS FOREIGN BORN

NUMBER OF VETERANS LOCATED WITHIN THE CITY 2,467

of the *White* population, which grew at a rate of 106.04%. The fastest growing ethnic division in Rockwall's population was the group that identified as *Two or More Races*, which grew at a rate of 2,810.87% between 2000 and 2020. The following table shows the percent change in ethnic divisions from 2000-2010 and 2010-2020:

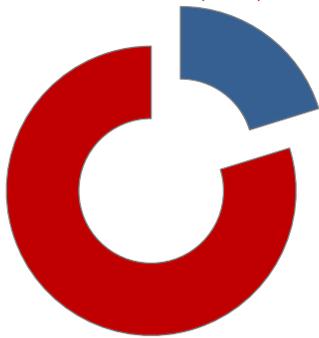
TABLE 1.3: % CHANGE FROM 2000-2010 & 2010-2020

| ETHNIC CATEGORY | 2000-2010 | 2010-2020 |
|----------------------------------|-----------|-----------|
| WHITE | 88.23% | 9.46% |
| BLACK | 310.95% | 111.11% |
| AMERICAN INDIAN OR ALASKA NATIVE | 218.84% | 90.91% |
| ASIAN | 356.63% | 92.70% |
| HAWAIIAN/PACIFIC ISLANDER | 316.67% | 128.00% |
| SOME OTHER RACE | 364.07% | 57.04% |
| TWO OR MORE RACES | 265.22% | 697.02% |

SOURCE: 2000 CENSUS, 2010 CENSUS, AND ESRI BUSINESS ANALYST

In addition, according to ESRI's *Business Analyst*, approximately 20.23% (i.e. 10,352 residents) of the overall population identified as *Hispanic or Latino* (see Figure 1.7) in 2024. This is up from 16.60% (i.e. 6,214) in the 2010 US Census.

FIGURE 1.7: HISPANIC OR LATINO IN 2024
HISPANIC OR LATINO: 20.23% (10,352); NOT HISPANIC OR LATINO: 79.77% (40,832)



SOURCE: ESRI BUSINESS ANALYST

In addition, the 2024 *American Community Survey* showed that the people identifying as *Hispanic or Latino* further identified as follows:

TABLE 1.4: HISPANIC OR LATINO BY RACE, 2024

| ETHNIC CATEGORY | 2024 |
|--------------------------|--------|
| MEXICAN | 71.48% |
| PUERTO RICAN | 4.44% |
| CUBAN | 1.92% |
| OTHER HISPANIC OR LATINO | 22.17% |

SOURCE: ESRI BUSINESS ANALYST

POPULATION THROUGH TAPESTRY SEGMENTATION

ESRI -- a *Geographic Information Systems (GIS) Company that specializes in software development and demographic data* -- has developed a tool that breaks down the entire US population into 67 unique population segments based on demographic and socioeconomic characteristics. These segments can also be grouped into 14 *LifeMode* categories that describe each group's lifestyle and life stage, and six (6) *Urbanization* groups that describe the geographic and physical features of the groups. This tool has been utilized throughout various industries to identify the customers being served in various geographic areas. For cities, this tool can be used to make generalizations concerning their populations. The following is a breakdown of the City of Rockwall's 2016 population versus the 2024 population by *Tapestry Segments*, *LifeMode* groups, and *Urbanization* groups.

TABLE 1.5: TAPESTRY SEGMENTS

KEY: RED = LARGE DECREASE; BLUE = LARGE INCREASE; GREEN = FASTEST GROWING SEGMENTS; HH = HOUSEHOLD

| TAPESTRY SEGMENT | % of HH | |
|---|---------|--------|
| | 2016 | 2024 |
| UP AND COMING FAMILIES | 20.82% | 16.77% |
| BOOMBURBS | 16.57% | 14.90% |
| SAVVY SUBURBANITES | 4.93% | 11.41% |
| RETIREMENT COMMUNITIES | 2.43% | 10.78% |
| SOCCER MOMS/WORK DAY DRIVE ¹ | 12.32% | 9.30% |
| IN STYLE | 9.66% | 9.27% |
| COMFORTABLE EMPTY NESTERS | 10.85% | 8.66% |
| BRIGHT YOUNG PROFESSIONALS | 6.72% | 6.46% |
| GREEN ACRES | 6.54% | 6.38% |
| DOWN THE ROAD | 3.44% | 3.05% |
| PROFESSIONAL PRIDE | 2.45% | 2.21% |
| EXURBANITES | 0.71% | 0.81% |
| SOUTHWESTERN FAMILIES | 2.55% | 0.00% |

NOTES:

¹: IN JUNE OF 2021 THE SOCCER MOMS TAPESTRY SEGMENT WAS RENAMED TO WORKDAY DRIVE.

According to Table 1.5, over 53.00% of Rockwall's population is split between four (4) *Tapestry Segments*. These segments are [1] *Up and Coming Families*, [2] *Boomburbs*, [3] *Savvy Suburbanites*, and [4] *Retirement*

Communities. The *National* household profiles for these *Tapestry Segments* are summarized as follows:

UP AND COMING FAMILIES (16.77%) ▼

The *Up and Coming Families* segment is described as residents that are younger, more mobile, diverse, ambitious, and more optimistic than previous generations. The median age of this segment is 31.40, and the average household size is 3.10. They live in new suburban neighborhoods, primarily in single-family homes valued at \$194,400.00, with high homeownership rates at around 73.90%. Typically, people in this segment are considered to be well educated, with 67.00% having at least some college education. This segment is also described as being willing to accept a longer than average commute for better/affordable housing opportunities. They are hardworking, tech-savvy, and value-conscious, often shopping online and favoring practical vehicles. Their leisure revolves around family activities, fitness, and home entertainment.

BOOMBURBS (14.90%) ▼

People in the *Boomburbs* segment are described as affluent, family-focused 30-somethings with high incomes and financial means. They have a median income of \$113,400.00, median age of 34, high rate of homeownership at 84% and an average household size of 3.25. Members of the *Boomburbs* segment live in new, single-family homes -- with a median value of \$350,000.00 --, and are considered to be well-educated, young professionals with 55.00% having college degrees. Their households typically have multiple earners and a high labor force participation at 71.30%. They are well connected, typically own the latest devices and are willing to trade longer commute times for the amenities of the suburbs. They tend to blend active lifestyles with financial planning and community involvement.

SAVVY SUBURBANITES (11.41%) ▲

The *Savvy Suburbanites* segment is composed of affluent, well educated households with a median age of 45.1, an average household size of 2.85, and a median income of \$108,700.00. People in this segment predominately live in established suburban neighborhoods, in spacious single-family homes that have a median value of \$362,900.00. They have a high rate of homeownership at 91.00%, with around 66.00% having mortgages. They value quality and research purchases, enjoy DIY projects like gardening or remodeling, and prioritize

fitness and active lifestyles. They are tech-savvy and financially informed, invest heavily in their homes, vehicles, and personal well-being. They tend to blend suburban charm with a focus on culture, health, and leisure.

RETIREMENT COMMUNITIES (10.78%) ▲

Retirement Communities are senior-oriented neighborhoods with a median age of 53.9, an average household size of 1.88 people, and a household income of \$40,800.00. Housing includes a mix of single-family homes and multi-unit structures, with 55.00% of people in this group renting. These communities often feature assisted living and senior care facilities. Residents value fiscal responsibility, prefer traditional media (e.g. newspapers), and enjoy low-key leisure activities like fishing, card games, and dining out. Brand loyalty and health-conscious habits -- such as using name-brand medications -- are common. Residents of these communities look for modest lifestyles focused on convenience and simplicity.

TABLE 1.6: LIFEMODE GROUPS

KEY: RED = LARGE DECREASE; BLUE = LARGE INCREASE; GREEN = FASTEST GROWING GROUPS; HH = HOUSEHOLD

| LIFEMODE GROUPS | % of HH | |
|---|---------|--------|
| | 2016 | 2024 |
| AFFLUENT ESTATES | 24.66% | 29.33% |
| GENXURBAN | 20.51% | 17.93% |
| ETHNIC ENCLAVE/SPROUTING EXPLORERS ¹ | 23.37% | 16.77% |
| SENIOR STYLES | 2.43% | 10.78% |
| FAMILY LANDSCAPE | 12.32% | 9.30% |
| MIDDLE GROUND | 6.72% | 6.46% |
| COZY COUNTRY LIVING | 6.54% | 6.38% |
| RUSTIC OUTPOST | 3.44% | 3.05% |

NOTES:

¹: IN JUNE OF 2021 THE ETHNIC ENCLAVE LIFEMODE GROUP WAS RENAMED TO SPROUTING EXPLORERS.

Tapestry Segmentations can be grouped into *LifeMode* groups, which represent markets that share a common experience (e.g. born in the same generation, immigration from another county, and etcetera) or a significant

demographic trait like affluence. Based on Table 1.6, over 64.00% of Rockwall's population is in three (3) *LifeMode* Groups. These groups are [1] *Affluent Estates*, [2] *GenXUrban*, and [3] *Sprouting Explorers*. The *National* profiles for each of these *LifeMode* Groups are as follows:

AFFLUENT ESTATES (29.33%) ▲

People in the *Affluent Estates* group are typically married couples that are described as having established wealth, being educated, and well-traveled. They have children that range in age from grade school to college. They have high levels of homeownership at ~90.00%, most with mortgages at ~65.20%. This group expects quality and invests in time-saving services, they participate actively in their communities, and are active in sports and travel.

GENXURBAN (17.93%) ▼

This is the second-largest *LifeMode* Group nationwide and is composed of both *Gen X* married couples and a growing population of retirees. This group is typically middle-aged - with about a fifth of residents over the age of 65 -- and about a fourth of households having retirement income. This group tends to own older single-family homes in urban areas, have one (1) to two (2) vehicles, and live and work in the same county, creating shorter commute times. They invest wisely, are well insured, and are comfortable banking online or in person. This group are news enthusiasts and enjoy reading, renting movies, playing board games, going to museums and concerts, dining out and walking for exercise.

SPROUTING EXPLORERS (17.93%) ▼

The *Sprouting Explorers* group is composed of multilingual and multigenerational households with children who represent second, third, or fourth generation Hispanic families. They are young homeowners with families in owner occupied, single-family neighborhoods that are at the City's edge, primarily built after 1980. People in this group are described as being hardworking and optimistic, with most being over the age of 25 years old and having a high school diploma and some college education. They enjoy shopping and leisure activities, typically focus on their children, buying baby and children's products, and trips to theme parks, water parks, and/or the zoo. Their children enjoy playing video games on personal computers, or handheld and console devices. Many of these households can also be expected to have dogs for domestic pets.

TABLE 1.7: URBANIZATION GROUPS

KEY: RED = LARGE DECREASE; BLUE = LARGE INCREASE; GREEN = FASTEST GROWING GROUPS; HH = HOUSEHOLD

| URBANIZATION GROUPS | % of HH | |
|---------------------|---------|--------|
| | 2016 | 2024 |
| SUBURBAN PERIPHERY | 68.65% | 64.07% |
| METRO CITIES | 12.09% | 20.05% |
| URBAN PERIPHERY | 9.27% | 6.46% |
| RURAL | 6.54% | 6.38% |
| SEMI-RURAL | 3.44% | 3.05% |

LifeMode Groups can be further grouped into *Urbanization Groups*, which are markets that share similar locales. As depicted in Table 1.7, the City of Rockwall heavily falls into the *Suburban Periphery* group; however, this group has been slightly declining while the *Metro Cities* group continues to increase. These *Urbanization Groups* are summarized as follows:

SUBURBAN PERIPHERY (64.07%) ▼

This area is characterized by urban expansion, affluence in the suburbs or city-by-commute households. It is the most populous and fastest-growing among all the *Urbanization Groups* and includes one-third of the nation's population. Commuters in this group value low-density living, but demand proximity to jobs, entertainment, and amenities of an urban center. People in this group are well-educated, typically coming from two (2) income households who accept longer commute times to raise their children in family-friendly neighborhoods. Many are heavily mortgaged in newly built, single-family homes. Older households have either retired in place, downsized, or purchased a seasonal home. Suburbanites are the most affluent group, working hard to lead bright, fulfilled lives. Residents often invest in their future and ensure themselves against unforeseen circumstances but also enjoy the results of their hard work.

METRO CITIES (20.05%) ▲

The *Metro Cities* group is characterized by affordable city life, including smaller metropolitan cities or satellite cities that feature a mix of single-family and multi-unit housing. Single householders in this group typically seek affordable living in the form of multi-unit buildings that range from mid-rise to high-rise apartments with average rents and home value below the US average. The typical consumer in this group includes college students, affluent Gen X couples, and retirement communities. People in this group typically have student loan debt, and use debit

cards more than credit cards. They also share an interest in City life and its amenities, and participate in activities like dancing, clubbing, concerts, and going to museums. They tend to rely on the internet for entertainment (e.g. streaming music and movies, and playing online video games) and as a useful resource.

NOTE: For more information on Rockwall's various Tapestry Segments and Groups, see the 2024 Tapestry Segmentation Report provided with this document.

COST OF LIVING AND INCOME

According to a study performed by smartasset™ -- a personal finance and technology company --, Rockwall County is third in the State of Texas and 11th nationally with regard to its Purchasing Power Index in terms of Cost of Living (i.e. 88.63)². The Purchasing Power Index for Cost of Living is a measure of a community's purchasing power established by the City's median income relative to its cost of living. The study utilized data from the US Census Bureau's 2022 American Community Survey, the MIT Living Wage Study, and the Bureau of Labor Statistics' 2022 Consumer Expenditure Survey. These studies indicate that the weighted median income for the City of Rockwall as of 2024 is \$121,303.00 and its cost of living is \$71,932.00 (see Table 1.8 & Figure 1.8). This represents a growth of 31.90% in median income from 2015-2024 or a Compound Annual Growth Rate (CAGR) of 3.06%; however, this growth is being outpaced by the growth in cost of living which increased by 66.46% or a CAGR of 5.83% over the same period. Regardless of this fact, the City of Rockwall has a much higher disposable income than the national and state averages. The estimated per capita income of the City is \$56,146.00, compared to the State per capita income of \$39,971 and the National per capita income of \$43,829 [ESRI Business Analyst 2024, Data Axel 2024].

In addition, the 2024 American Community Survey estimates that the City's poverty rate (i.e. 3.04%) is far less than the State average (i.e. 14.00%) and the National average (i.e. 11.50%) (see Figure 1.9). These facts also identify the City's 2024 median household income as \$127,548.00, which is well above the State and National household incomes, with the State's median household income being \$77,169.00 and the National median household income being \$79,068.00. Rockwall County had a slightly higher median household income of \$128,501.00. Extrapolating the median household income

TABLE 1.8: PURCHASING POWER INDEX AND STATE AND NATIONAL RANKING

KEY: RED = ESTIMATED DUE TO NO AVAILABLE INFORMATION; ▲ / ▼ = INCREASING/DECREASING TREND

| YEAR | COST OF LIVING | MEDIAN INCOME | PURCHASING POWER INDEX | STATE RANK | NATIONAL RANK |
|------|----------------|---------------|------------------------|--------------|---------------|
| 2015 | \$ 43,213.00 | \$ 92,466.00 | 100.00 | 1 | 1 |
| 2016 | \$ 42,990.00 | \$ 86,597.00 | 96.96 ▼ | 2 | 6 |
| 2017 | \$ 43,209.00 | \$ 87,524.00 | 91.72 ▼ | 2 | 5 |
| 2018 | \$ 43,444.00 | \$ 89,161.00 | 96.79 ▲ | 2 | 3 |
| 2019 | \$ 44,512.00 | \$ 93,269.00 | 92.33 ▼ | 1 | 4 |
| 2020 | \$ 44,539.00 | \$ 93,269.00 | 89.18 ▼ | 1 | 4 |
| 2021 | \$ 48,033.00 | \$ 100,920.00 | 86.58 ▼ | 3 | 4 |
| 2022 | \$ 55,999.33 | \$ 107,714.33 | | NO DATA 2022 | |
| 2023 | \$ 63,965.67 | \$ 114,508.67 | | NO DATA 2023 | |
| 2024 | \$ 71,932.00 | \$ 121,303.00 | 88.63 ▲ | 3 | 11 |

SOURCE: SMARTASSET

FIGURE 1.8: COST OF LIVING VS. MEDIAN INCOME, 2015-2024

KEY: RED = MEDIAN INCOME; GREEN = COST OF LIVING

NOTE: THERE WAS NO AVAILABLE DATA FOR 2022 & 2023



SOURCE: SMARTASSET

FIGURE 1.9: POVERTY RATES, 2020-2024

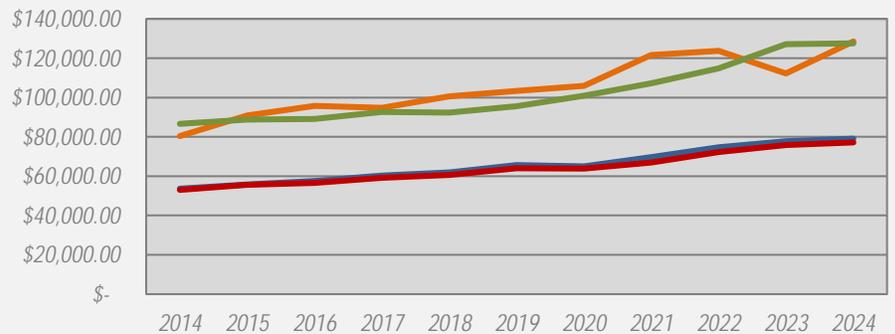
KEY: BLUE = UNITED STATES; RED = STATE OF TEXAS; GREEN = CITY OF ROCKWALL



SOURCE: AMERICAN COMMUNITY SURVEY; TEXAS DEMOGRAPHIC CENTER

FIGURE 1.10: MEDIAN HOUSEHOLD INCOME, 2014-2024

KEY: BLUE = UNITED STATES; RED = STATE OF TEXAS; GREEN = CITY OF ROCKWALL; ORANGE = ROCKWALL COUNTY



SOURCE: AMERICAN COMMUNITY SURVEY; TEXAS DEMOGRAPHIC CENTER

over a ten (10) year period, from 2014-2024, the City showed a Compound Annual Growth Rate (CAGR) of 3.94% slightly below the National CAGR of 3.95%, but higher than the State CAGR of 3.82%. Rockwall County showed the highest CAGR of 4.80%.

NOTE: See the Non-Residential section of this document for more detailed job and salary information.

EDUCATIONAL ACHIEVEMENT

The City of Rockwall has a high educational attainment. According to the *US Census Bureau's American Community Survey* the percentage of Rockwall's population with a high school diploma is 95.20%, which exceeds the County (i.e. 91.70%), State (i.e. 82.45%), and National (86.30%) averages. This also holds true for the percentage of the population with a college education, which was estimated to be at 61.30% compared to the County, State, and National averages of 57.50%, 43.10%, and 45.70% (see Figure 1.11). In addition, the educational attainment of the residents has increased in the City since the original 2017 *Existing Conditions Report* was drafted, with the number of people in the City with a college education increasing by 23.10%.

Looking at the 14,902 students enrolled in the Rockwall Independent School District (RISD) as of 2024, 56.90% were *White*, 24.20% were *Hispanic*, 10.50% were *Black*, 4.80% were of *Two (2) or More Races*, 3.00% were *Asian*, 0.60% were *Native American*, and 0.20% were *Pacific Islander* (see Figure 1.12). In addition, 22.80% of these students (i.e. ~3,398 students) are estimated to be considered *economically disadvantaged students*; however, this is far below the State's average of 60.70% of all students being considered *economically disadvantaged students* [Texas Education Agency]. Overall, the RISD continues to have an outstanding graduation rate at 99.70% in 2023 (see Figure 1.13). According to Niche -- a website that focuses on narrow topics including school districts --, the RISD achieves an overall grade of an 'A' (see Figure 1.14), and identifies it as ranking #31 best school district in the state of Texas out of 883 school districts. It also ranked #202 of 912 in the best places to teach in Texas³.

FIGURE 1.14: RISD RANKINGS ON NICHE



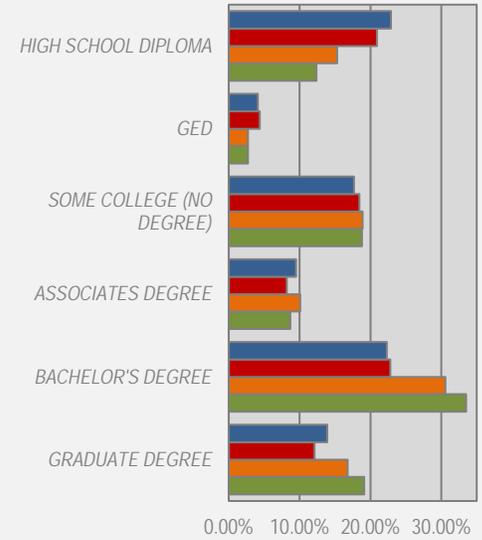
PROJECTED TRENDS

With the exception of the abnormal growth experienced by the community after the COVID-19 pandemic (i.e. 7.78% in 2022), Rockwall has consistently grown at a rate between 0.82%-3.87% since 2012. Looking forward at the City's population -- assuming a conservative Compound Annual Growth Rate (CAGR) of 3.00% -- it is anticipated that the City will reach 80,000 residents by 2040. With this being said, this growth projection is heavily influenced by the changing land use policies of the State of Texas, the continued in-migration trends from northern states and California to southern states (which is being experienced nationally), and the remaining available undeveloped land within the community.

Regardless of these trends, it is anticipated that the City of Rockwall's population will remain predominately between the ages of 35-54 years old, and that the City -- while being an employment center in the region -- will continue to be primarily a bedroom community of the City of Dallas. With this being said, the recent trends in the City's older age cohorts cannot be ignored. It is anticipated that moving forward over the next 20-years, as the *Baby Boomer* generation phases out of the work force and the majority of *Generation X* starts to reach a retirement age, the age cohorts between 55-74 years old will continue to increase. This also holds true for the 75-85+ age cohorts. As people's life spans continue to increase, due to advances in health care and a continued shift towards *aging in place*, these age cohorts will continue to see significant increases. The 0-19 years old age cohorts have seen consistent growth between 26.39%-31.64% over the last three (3) US Census, and have remained consistent with the percentage growth experienced in the 35-44 years old age cohorts. This is expected to continue moving forward. The growth reflected in both the 0-19 and 35-44 age cohorts is indicative of young families seeking out a suburban atmosphere with a highly rated school system. The relatively flat growth of the 20-24 age cohort is not a new trend in the City of Rockwall. This is most likely attributed to the Rockwall Independent School District's (RISD's) high graduation and college attendance rates (i.e. the majority of this age cohort is attending college outside of the City).

FIGURE 1.11: HIGHEST EDUCATIONAL ACHIEVEMENT

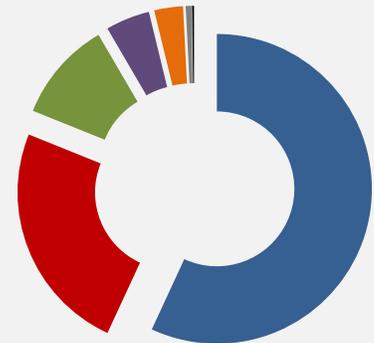
KEY: BLUE = UNITED STATES; RED = STATE OF TEXAS; GREEN = CITY OF ROCKWALL; ORANGE = ROCKWALL COUNTY



SOURCE: AMERICAN COMMUNITY SURVEY, ESRI BUSINESS ANALYST; DATA AXEL

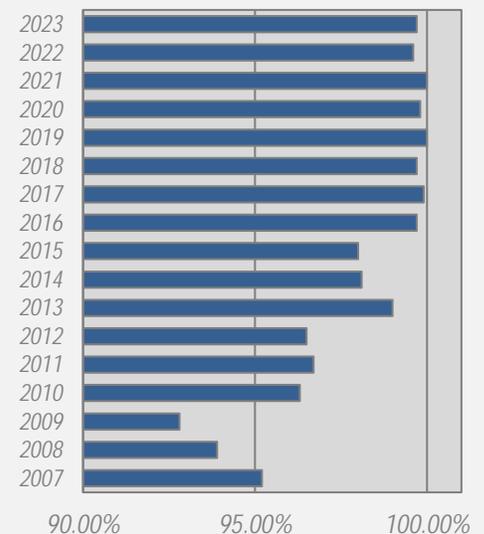
FIGURE 1.12: ENROLLMENT BY ETHNICITY

KEY: BLUE = WHITE (56.90%); RED = HISPANIC (24.20%); GREEN = BLACK (10.50%); PURPLE = TWO OR MORE RACES (4.60%); ORANGE = ASIAN (3.00%); GREY = NATIVE AMERICAN (0.60%); BLACK = PACIFIC ISLANDER (0.20%)



SOURCE: ROCKWALL INDEPENDENT SCHOOL DISTRICT

FIGURE 1.13: GRADUATION RATE, 2007-2023



SOURCE: ROCKWALL INDEPENDENT SCHOOL DISTRICT

From a gender standpoint, the community has remained evenly divided through the last three (3) *US Census*. Based on this, it is not anticipated that the gender composition of the City will see significant changes over the next 20-years, and the community will continue to be roughly half male and half female.

In 2017, it was anticipated that the City's ethnic populations would continue to grow in the same relative consistency as the trends that were observed from 2000-2010, but that the *Hispanic or Latino* populations were expected to outpace their current trends. This appears to be an accurate assumption with the City seeing the same relative changes between 2010-2020; however, the *Hispanic or Latino* population only grew by 3.63% or 4,138 residents. The fastest growing segment of the population during this time period was the *Two or More Races*, which grew at a staggering 697.02% or 5,855 residents. This trend may be due to how the *US Census Bureau* classified *Hispanic or Latino* in 2010 versus 2020. During the *2010 US Census*, the *Hispanic or Latino* category was incorporated into the *White* category; however, in the *2020 US Census* this category was broken out with the majority of the people being in the *Hispanic or Latino* category ending up in the *Two or More Races* category. This may also partially explain the sharp decrease in the *White* category, which shrank from 82.40% in 2010 to 66.03% in 2020 and only growing by around 2,925 residents during this time period. With all of this being said, the predications from 2017 -- *while not completely verifiable* -- do appear to be correct, and based on the observations from 2000-2020, it can be assumed that by the *2030 US Census* the community's population will continue to become more diverse.

The trends identified above relating to the changes in age and the ethnic composition of the City, are further supported by the *Tapestry Segmentation Report*. The fastest growing segment of the *Tapestry Segmentation Report* between 2017-2024 was the *Retired Communities* segment, which grew at a rate of 8.35%. A possible reason for this growth was the addition of more age restricted communities (*i.e. the Alders, the Ladera Subdivision, and the Standard Subdivision*). The other fastest growing segment of the *Tapestry Segmentation Report* was the *Savvy Suburbanites* segment, which grew at a rate of 6.48%. Since this group tends to be older suburbanites -- *with a median age around 45.1 years old* --, if this trend continues, the 45-54

age cohort may eventually surpass the 35-44 age cohort as the predominate age cohort in the community.

Looking at the City's *Cost of Living and Income*, the City of Rockwall continues to be an affluent suburb with a high purchasing power in comparison to other cities at both the State and National levels; however, a concerning trend that was identified in this analysis was the shrinking gap between the City's cost of living and its median income. This gap peaked in 2021 at \$52,887.00 and has shrunk to \$49,371.00. While this isn't as low as the level experienced in 2016 at \$43,607.00, it does appear to be a continuing downward trend (see *Table 1.9*).

TABLE 1.9: GAP BETWEEN COST OF LIVING AND MEDIAN INCOME 2015-2024

KEY: RED = SMALLEST GAP; BLUE = ESTIMATED DUE TO NO INFORMATION PROVIDED; ▲ / ▼ = INCREASING/DECREASING TREND

| YEAR | DIFFERENCE IN COST OF LIVING VS. MEDIAN INCOME | | TREND |
|------|--|--|-------|
| | | | |
| 2015 | \$49,253.00 | | |
| 2016 | \$43,607.00 | | ▼ |
| 2017 | \$44,315.00 | | ▲ |
| 2018 | \$45,717.00 | | ▲ |
| 2019 | \$48,757.00 | | ▲ |
| 2020 | \$48,730.00 | | ▼ |
| 2021 | \$52,887.00 | | ▲ |
| 2022 | \$51,715.00 | | ▼ |
| 2023 | \$50,543.00 | | ▼ |
| 2024 | \$49,371.00 | | ▼ |

SOURCE: SMARTASSET

As predicted in 2017, the Rockwall Independent School District (RISD) continues to be a highly regarded and sought-after school district in the State of Texas, and this has also played a role in fueling the growth experienced by the City of Rockwall. Moving forward, it is not anticipated that -- *at least in the short-term* -- this will change, and the school district will continue to support high graduation and college attendance rates; however, it is worth mentioning the recent failure of the 2024 Comprehensive Bond Election, and the outside possibility that this could change or effect the school district's rankings compared to other school districts in the State in the future.

ENDNOTES

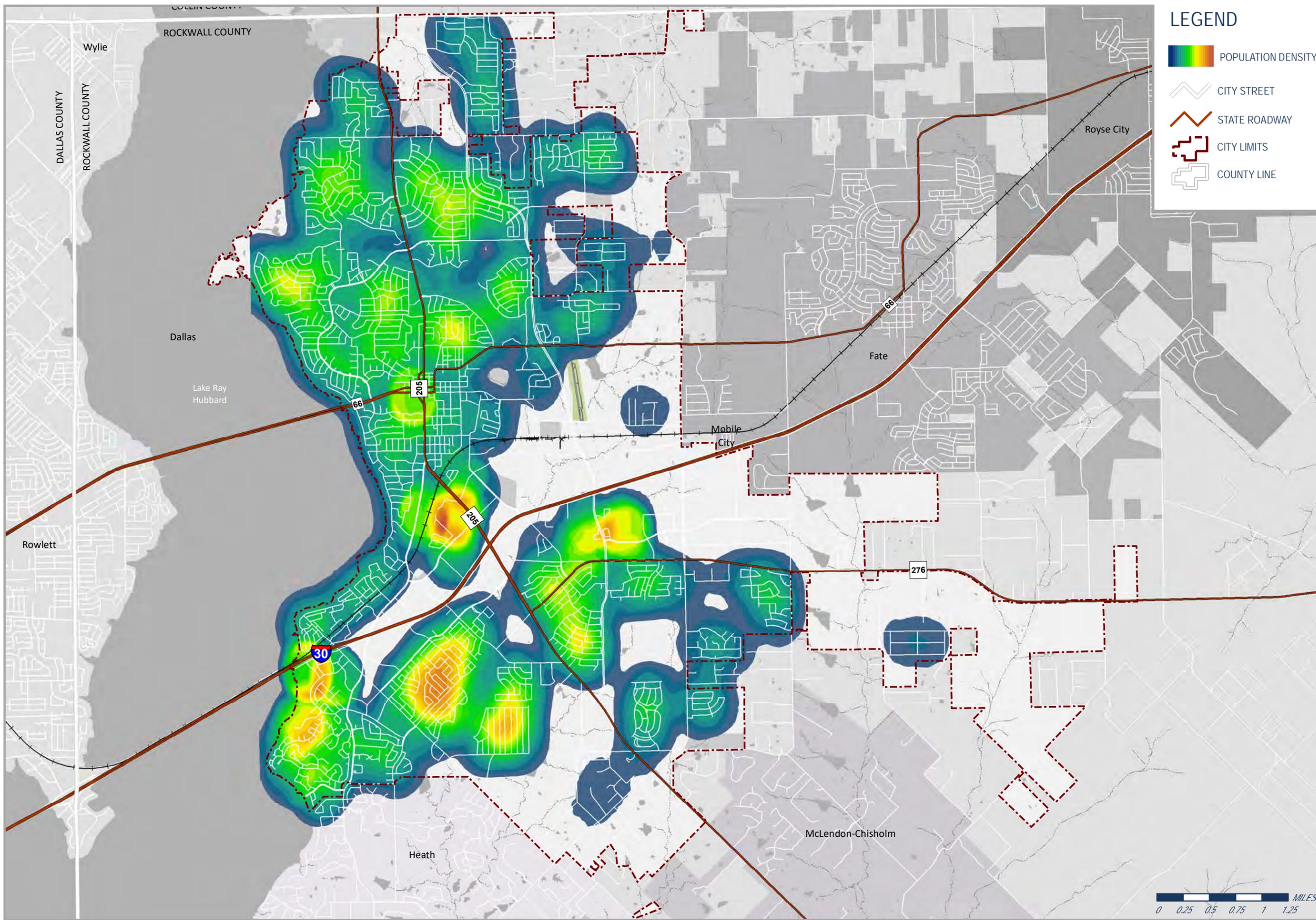
- Tapestry Segmentation (2024)*. Retrieved November 19, 2024, from <http://www.esri.com/landing-pages/tapestry>
- Places with the Most Favorable Cost of Living (2017-2024)*. Retrieved November

19, 2024, from <https://smartasset.com/mortgage/cost-of-living-calculator#texas>

- School Rankings: Rockwall Independent School District*. Retrieved December 3, 2024, from <https://www.niche.com/k12/d/rockwall-independent-school-district-tx/>

MAP INDEX

- MAP 1.1: MAP OF POPULATION DENSITY



LEGEND

- POPULATION DENSITY
- CITY STREET
- STATE ROADWAY
- CITY LIMITS
- COUNTY LINE

POPULATION DENSITY

CH.1: POPULATION AND DEMOGRAPHICS

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1.1



2 TRANSPORTATION AND INFRASTRUCTURE





DISMANTLING OF THE BOYDSTON WATER TOWER

THE ABOVE IMAGES SHOW THE DISMANTLING OF THE CITY'S WATER TOWER ALONG BOYDSTON AVENUE (ADJACENT TO FIRE STATION #1). THE WATER TOWER -- ALSO KNOWN AS "OLD RUSTY" -- WAS ORIGINALLY CONSTRUCTED IN 1978, AND WAS DECOMMISSIONED IN 2008. THE DISMANTLING PROCESS SHOWN ABOVE TOOK 27 DAYS STARTING ON AUGUST 4, 2022 AND ENDING ON AUGUST 31, 2022.

INFRASTRUCTURE

A City's infrastructure is primarily composed of its streets, water, stormwater, and wastewater systems. These facilities are necessary for cities to serve their residential and non-residential customers.

WATER AND WASTEWATER SYSTEMS

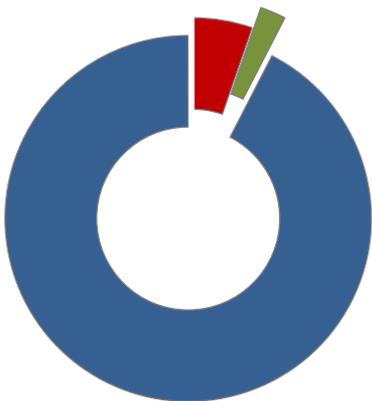
The City of Rockwall contracts with the North Texas Municipal Water District (NTMWD) to supply water and wastewater services. The City's responsibility is water distribution and wastewater collection, while the NTMWD is responsible for the provision of water and the treatment of wastewater.

WATER FACILITIES

In its corporate boundaries, the City of Rockwall has 370.18 linear miles (i.e. 1,954,527.13 linear feet) of City owned water lines, 9.11 linear miles (i.e. 48,097.45 linear feet) of private water lines or water lines owned by Water Supply Corporations with *Certificates of Convenience and Necessity* (CCN), and 21.19 linear miles (i.e. 111,870.90 linear feet) of water lines owned by the North Texas Municipal Water District (NTMWD) (see Figure 2.1). Of these totals, 77.63 linear miles of water lines have been constructed since 2017; however, the majority of the City's water lines (i.e. 68.10% or 252.07 linear miles) were constructed more than 14 years ago (i.e. prior to 2010), with 27.83% of all water lines being constructed more than 25-years ago. See Figure 2.2 for a breakdown of the City's water lines by construction material and see Figure 2.3 for a breakdown of the City's water lines by age.

FIGURE 2.1: WATER LINES BY ENTITY IN THE CITY'S CORPORATE LIMITS

KEY: BLUE = CITY OF ROCKWALL; RED = NORTH TEXAS MUNICIPAL WATER DISTRICT (NTMWD); GREEN = PRIVATE WATER SUPPLY CORPORATIONS



SOURCE: CITY OF ROCKWALL GIS DIVISION

In addition, the City has three (3) active water towers (i.e. North Country, Southside, and Springer). Recently, the City removed the decommissioned water tower adjacent to Fire Station #1 (as depicted on the previous page), and began planning for a new water tower site adjacent to Mims Road. This water tower is currently in its preliminary planning stage, but is anticipated to be operational in Q4 of 2026 or Q1 of 2027 (see Maps 2.1 & 2.4 for all current and future water facilities).

NORTH COUNTRY: 2.0 MG CAPACITY



SOUTHSIDE: 1.0 MG CAPACITY



SPRINGER: 2.0 MG CAPACITY

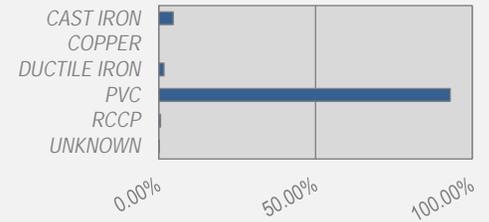


MG = MILLION GALLON

WASTEWATER FACILITIES

Situated within the City's corporate boundaries, there are 298.63 linear miles (i.e. 1,576,742.08 linear feet) of City owned wastewater lines, 10.34 linear miles (i.e. 54,600.56 linear feet) of private wastewater lines/private service lines, and 3.84 linear miles (i.e. 20,286.28 linear feet) of wastewater lines owned by the North Texas Municipal Water District (NTMWD) (see Figure 2.6). The wastewater flows generated by these lines are generally found in one (1) of the two

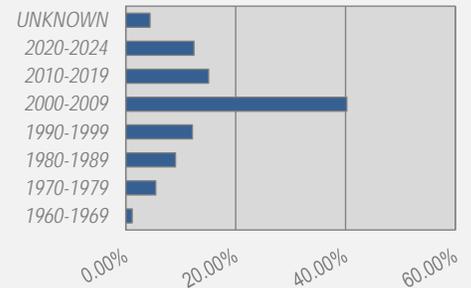
FIGURE 2.2: CITY WATER LINES BY MATERIAL
CAST IRON: 89,887.94 LF (04.60%); COPPER: 0 LF (00.00%); DUCTILE IRON: 32,752.62 LF (01.68%); PVC: 1,817,496.70 LF (92.99%); RCCP: 10,607.73 LF (00.54%); UNKNOWN: 3,782.15 LF (00.19%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.3: WATER LINES BY AGE

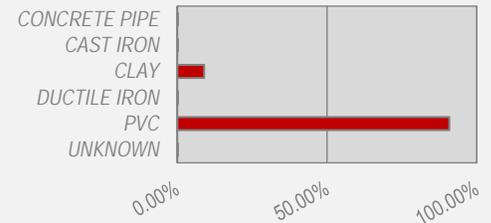
1960-1969: 22,440.64 LF (01.15%); 1970-1979: 107,044.00 LF (05.48%); 1980-1989: 177,246.89 LF (09.07%); 1990-1999: 237,179.40 LF (12.13%); 2000-2009: 787,036.32 LF (40.27%); 2010-2019: 294,491.03 LF (15.07%); 2020-2024: 242,356.21 LF (12.40%); UNKNOWN DATE: 166,240 LF (11.00%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.4: WASTEWATER LINES BY MATERIAL

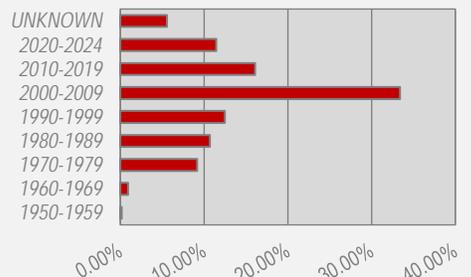
CAST IRON: 28.36 LF (0.002%); CLAY: 140,891.42 LF (08.94%); DUCTILE IRON: 2,118.43 LF (00.13%); PVC: 1,432,034.98 LF (90.82%); CONCRETE PIPE: 378.04 LF (00.02%); UNKNOWN: 1,290.85 LF (00.08%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.5: WASTEWATER LINES BY AGE

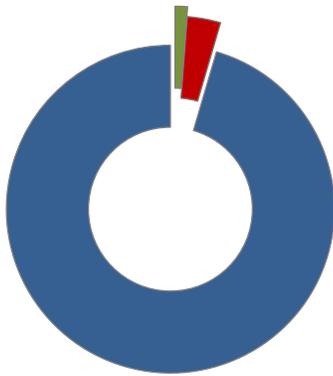
1950-1959: 2,435 LF (00.15%); 1960-1969: 14,491.94 LF (00.92%); 1970-1979: 144,940.62 LF (09.19%); 1980-1989: 168,759.50 LF (10.70%); 1990-1999: 197,124.38 LF (12.50%); 2000-2009: 526,565.91 LF (33.40%); 2010-2019: 253,586.76 LF (16.08%); 2020-2024: 180,846.60 LF (11.47%); UNKNOWN: 87,991.81 LF (05.58%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

(2) major basins located within the City (i.e. Squabble Creek and Buffalo Creek). The Squabble Creek basin currently flows to a treatment plant behind Lakeview Summit; however, this plant is in the process of being taken off-line and removed from service, and will send its effluent to the NTMWD treatment plant in south Mesquite in the future. The Buffalo Creek basin is treated at the Buffalo Creek Wastewater Treatment Plant with overflows being pumped to the NTMWD treatment plant in south Mesquite. See Figure 2.4 for a breakdown of the City's wastewater lines by material and see Figure 2.5 for a breakdown by age.

FIGURE 2.6: WASTEWATER LINES BY ENTITY IN THE CITY'S CORPORATE LIMITS
 KEY: BLUE = CITY OF ROCKWALL; RED = NORTH TEXAS MUNICIPAL WATER DISTRICT (NTMWD); GREEN = PRIVATE



SOURCE: CITY OF ROCKWALL GIS DIVISION

In addition, the City of Rockwall currently maintains 39 lift stations, which is second in the State behind Corpus Christi for the most lift stations maintained by a municipality. A lift station or pump station is a facility that is used to pump wastewater from a lower elevation to a higher elevation, particularly where the elevation of the source is not sufficient to gravity flow the lines. The large number of lift stations in the City of Rockwall -- which is atypical for cities in the North Texas region -- is due to the topography of the terrain (see Maps 2.2 & 2.5 for all current and future wastewater facilities).

EXAMPLE OF A LIFT STATION



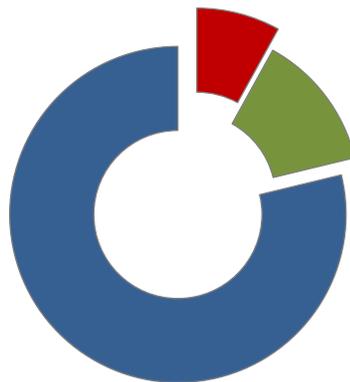
STONEY HOLLOW LIFT STATION ON JANUARY 9, 2025

It should also be noted that there are currently 722 single-family homes or lots in the City that are not attached to the City's wastewater network. These homes have On-Site Sewage Facilities (OSSF's) or a septic system. Taking this into consideration, the City of Rockwall is currently in the process of designing and constructing sanitary sewer service mains that will connect the eastern side of the Lake Rockwall Estates Subdivision to the City's wastewater collection system. This should decrease the number of homes utilizing OSSF or septic systems by approximately 150 homes.

STORMWATER FACILITIES

There are approximately 150.56 linear miles (i.e. 794,941.88 linear feet) of City maintained stormwater lines in the City of Rockwall. In addition, there are approximately 25.19 linear miles (i.e. 132,982.82 linear feet) of privately maintained stormwater lines, and 15.36 linear miles of stormwater lines maintained by the Texas Department of Transportation (TXDOT) (see Figure 2.7). This equals a total of 191.10 linear miles or 1,008,999.54 linear feet of stormwater lines in the City's corporate limits. As with water and wastewater lines in the City, the majority of the stormwater lines (i.e. 65.37% or 519,631.77 linear feet) were installed more than 14 years ago. The majority of all stormwater lines (94.89%) are constructed of Reinforced Concrete Pipe (RCP). See Figure 2.8 for a complete breakdown of the City's stormwater lines by construction material and see Figure 2.9 for a breakdown by age.

FIGURE 2.7: STORMWATER LINES BY ENTITY IN THE CITY'S CORPORATE LIMITS
 KEY: BLUE = CITY OF ROCKWALL; RED = TEXAS DEPARTMENT OF TRANSPORTATION (TXDOT); GREEN = PRIVATE



SOURCE: CITY OF ROCKWALL GIS DIVISION

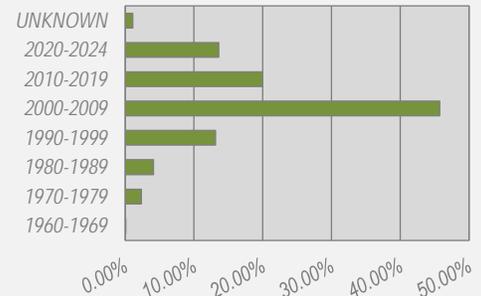
FIGURE 2.8: STORMWATER LINES BY MATERIAL
 ABS: 121 LF (0.04%); ADS: 2,362 LF (0.32%); CGM: 237 LF (0.04%); CIP: 329 LF (0.06%); CMP: 2,911 LF (0.26%); HDPE: 19,370 LF (2.69%); PVC: 5,267 LF (0.57%); RCB: 35,917 LF (3.62%); RCP: 689,248 LF (92.01%); UNKNOWN: 2,322 LF (0.39%)



KEY:
 CMP: CORRUGATED METAL PIPE
 PVC: POLYVINYL CHLORIDE PIPE
 RCB: REINFORCED CONCRETE BOX
 RCP: REINFORCED CONCRETE PIPE

SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.9: STORMWATER LINES BY AGE
 1960-1969: 595 LF (0.07%); 1970-1979: 18,648.00 LF (02.35%); 1980-1989: 32,503.32 LF (04.09%); 1990-1999: 104,393.55 LF (13.13%); 2000-2009: 363,491.90 LF (55.16%); 2010-2019: 158,710.68 LF (19.97%); 2020-2024: 107,882.66 (13.57%); UNKNOWN: 8,716.58 LF (01.10%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

CITY MAINTAINED INFRASTRUCTURE AT A GLANCE



TRANSPORTATION SYSTEM

The transportation network of a city consists of the infrastructure necessary to move people and commodities from one place to another. The City of Rockwall's transportation network consists of highways, roadways, railroads, public transit agreements, sidewalks and the City's hike/bike and trail systems.

ROADWAY FACILITIES

The City of Rockwall has approximately 358.15 linear miles of roadways consisting of 65.50 linear miles of State Highways, 274.64 linear miles of City streets, and 18.01-miles of private streets. In addition, to these transportation facilities, the City also has 63.75 linear miles of alleyways. *Figures 2.11, 2.12 & 2.13* depict a breakdown of the roadway materials used for each entity's roadways.

FUTURE ROADWAY FACILITIES

As the City of Rockwall's population grows, so does the need to expand the transportation facilities serving the population. This also directly correlates to Rockwall's ability to attract and retain commercial and retail development. The City accounts for this need through the City's Master Thoroughfare Plan (see *Map 2.7*) and the Capital Improvements Plan (CIP) for roadways (see *Map 2.8*).

MASTER THOROUGHFARE PLAN (MTP)

A *Master Thoroughfare Plan* is intended to be a long-range plan that identifies the roadway facilities necessary for a City at ultimate buildout. More specifically, it is a right-of-way preservation plan that is intended to facilitate the orderly development of the City's transportation network through City or private development funding.

CAPITAL IMPROVEMENTS PLAN (CIP)

The *Capital Improvements Plan* is a short-range plan (e.g. *four [4] to ten [10] years*) intended to identify and plan for capital projects. These plans generally identify the method for financing the capital projects and the schedule for construction.

FUTURE ROADWAY PROJECTS

The following is a comprehensive list of all the future roadway projects planned inside the City's corporate boundaries by the City of Rockwall and the Texas Department of Transportation (TXDOT).

CITY OF ROCKWALL PROJECTS

- ☑ N. Lakeshore Drive from SH-66 to Master Boulevard (*Construction Start Date: Late 2025/Early 2026*)

TEXAS DEPARTMENT OF TRANSPORTATION (TXDOT) PROJECTS¹

- ☑ FM-552 Between SH-205 and SH-66 (*Ready to Let Date*: July 2026*)
- ☑ SH-276 Between FM-549 and CR2472 in Hunt County (*Ready to Let Date*: June 2026*)
- ☑ FM-549 South Between SH-276 and SH-205 (*Let Date*: November 2024; Ready for Construction*)
- ☑ Future SH-205 Middle (i.e. *Current John King Boulevard*) from SH-205 North to SH-205 South (*Estimated Ready to Let Date*: December 2026*)
- ☑ SH-205 from John King Boulevard to the Southern City Limits (*Estimated Let Date*: December 2025*)
- ☑ FM-1141 from SH-66 to FM-552 (*No Let Date* Established; Rockwall County Postponed the Authorization for Engineering Services*)
- ☑ FM-3097 from Tubbs Road to FM-549 (*No Let Date* Established; Waiting on TXDOT Environmental Clearance*)
- ☑ Outer Loop from FM-2755 to SH-205 (*No Let Date* Established; Working on Ultimate Alignment*)
- ☑ Horizon Road from FM-740 to IH-30 (*No Estimated Let Date*; Design Engineer has been Identified but not Awarded by Rockwall County*)
- ☑ Village Drive from Laguna Drive to Marina Drive (*No Estimated Let Date**)
- ☑ FM-549 from FM-740 to SH-205 (*No Estimated Let Date*; Engineering Contract being Negotiated*)
- ☑ IH-30 from Dalrock Road to SH-205 (*Currently Under Construction ; Estimated Completion Date: July 2027*)
- ☑ IH-30 from SH-205 to the Hunt County Line (*Currently Under Construction; Estimated Completion Date: December 2027*)

*: The Let Date is the date by which bids on a project are opened. These dates are subject to change.

ALTERNATIVE MODES OF TRANSIT

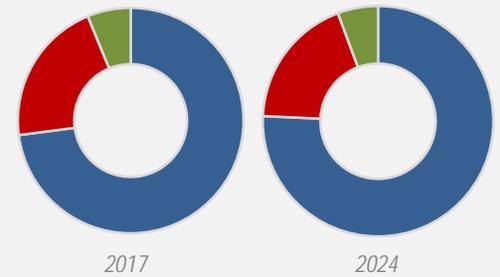
In addition to the City's roadway facilities, Rockwall also offers public transit, hike/bike trails, pedestrian trails and sidewalks throughout the City.

SIDEWALKS AND HIKE/BIKE TRAILS

The City currently has 259.06 linear miles (i.e. 1,367,836.80 linear feet) of on-street

FIGURE 2.10: CHANGE IN LINEAR MILES OF ROADWAY BY ENTITY, 2017-2024

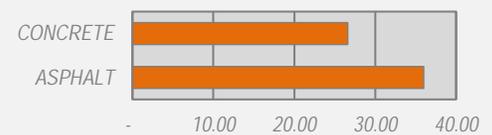
KEY: BLUE = CITY OF ROCKWALL (2017: 209.51 LM; 2024: 254.52 LM); RED = TEXAS DEPARTMENT OF TRANSPORTATION (TXDOT) (2017: 59.90 LM; 2024: 62.62 LM); GREEN = PRIVATE (2017: 17.81 LM; 2024: 19.01 LM)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.11: STATE HIGHWAY ROAD SURFACES IN LINEAR MILES

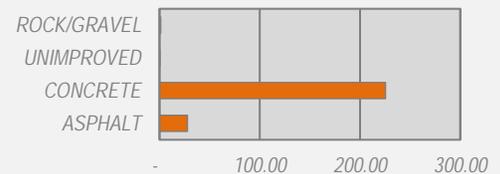
CONCRETE: 26.62 LM (42.51%); ASPHALT: 36.00 LM (57.49%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.12: CITY ROAD SURFACES IN LINEAR MILES

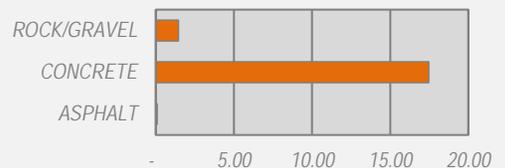
CONCRETE: 225.32 LM (88.53%); ASPHALT: 27.94 LM (10.98%); ROCK/GRAVEL: 1.14 LM (0.45%); UNIMPROVED: 0.12 LM (0.05%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.13: PRIVATE ROAD SURFACES IN LINEAR MILES

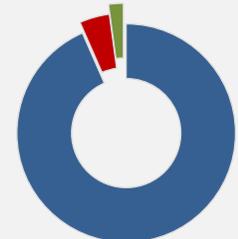
CONCRETE: 17.47 LM (91.91%); ASPHALT: 0.09 LM (0.47%); ROCK/GRAVEL: 1.45 LM (7.63%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 2.14: PEDESTRIAN SIDEWALK AND TRAILS

KEY: BLUE = SIDEWALKS (259.06 LM); RED = PARK TRAILS (12.28 LM); GREEN = MOUNTAIN BIKE TRAILS (5.95 LM)



SOURCE: CITY OF ROCKWALL GIS DIVISION

sidewalks, 12.28 linear miles (i.e. 64,838.40 linear feet) of park trails, and 5.95 linear miles (i.e. 31,416.00 linear feet) of mountain bike trails. In addition, the City has been working on a ten (10) foot hike and bike trail that will run along the entire length of John King Boulevard. When finished, this trail will span 13.87 linear miles (i.e. 6.93 linear miles on each side of the road) or 73,223.00 linear feet. Currently, the trail is about 38.08% constructed with 3.94 linear miles of ten (10) foot concrete trail and 1.34 linear miles of eight (8) foot concrete trail. There is about 8.13 linear miles (i.e. 42,943.00 linear feet) left to be developed and 0.45 linear miles (i.e. 2,396.00 linear feet) of four (4) foot concrete sidewalk that needs to be redeveloped. Despite the City's robust sidewalk and trail system, it currently provides more of a recreational amenity as opposed to a true transit option; however, the City continues to look for opportunities to create connections between subdivisions, businesses, and recreational/park amenities in the community.

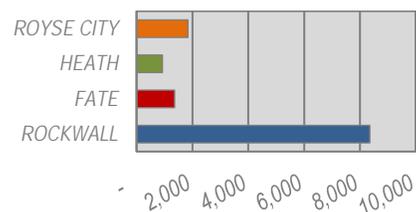
NOTE: For more information on Rockwall's trail systems, see the Parks, Trails and Open Space section of this report.

PUBLIC TRANSIT

Public transit options in the City of Rockwall are provided by the City's contracted provider, STAR Transit, and provide a demand responsive form of public transit/paratransit. According to information provided by STAR Transit, this service provided 12,483 total trips in Rockwall County in fiscal year 2024 (i.e. September 2023 to August 2024) (see Figure 2.15 for a breakdown of trips generated by City in Rockwall County). Of these trips, approximately 8,351 or 66.90% of all trips were generated by the City of Rockwall (see Figure 2.15). In addition, 10,054 or 80.54% of the total trips taken in Rockwall County during this fiscal year were associated with elderly or disabled people.

FIGURE 2.15: STAR TRANSIT TRIPS GENERATED BY CITY

KEY: BLUE = CITY OF ROCKWALL (8,351 TRIPS); RED = CITY OF FATE (1,364 TRIPS); GREEN = CITY OF HEATH (927 TRIPS); ORANGE = CITY OF ROYSE CITY (1,841 TRIPS)



SOURCE: CITY OF ROCKWALL GIS DIVISION

RAILROADS

Currently, there are 4.79 linear miles (i.e. 25,289.00 linear feet) of railroad tracks in the City of Rockwall. All of these rail lines are operated and maintained by the Union Pacific/Dallas, Garland & Northeastern Railroad, which is a short-line railroad headquartered in Garland, Texas. The rail lines that run through the City of Rockwall are a part of the eastern segment that connects Trenton, Greenville, and Garland. They are primarily commercial rail lines that transport aggregates, agricultural and food products, lumber, paper, scrap paper, scrap metals, and auto parts. There are currently several companies in the City that make use of these rail lines through rail spurs, including: Whitmore Manufacturing Company, SPR Packaging, Graham Packaging PET Technologies Incorporated, and Channell Commercial Corporation.

TRAVEL TRENDS

According to the US Census Bureau's 2023 American Community Survey, the average commute time -- for a worker 16-years and older -- in the City of Rockwall is 31.00-minutes, which represents an increase of 0.80-minutes from the 2015 American Community Survey (see Table 2.1 for a summary of travel times from the 2023 American Community Survey). In addition, the number of workers that commute outside of Rockwall County for work decreased by 10.90%, from 60.00% in 2015 to 49.10% in 2023. The number of people that worked in Rockwall County increased from 39.20% in 2015 to 50.10% in 2023, and the number of people in the work force that work outside of the State of Texas increased from 0.06% in 2015 to 0.80% in 2023. It can be assumed that much of this change is attributable to the shift the community experienced during the COVID-19 pandemic.

Looking at modes of travel, commuters who drove alone decreased from 81.90% to 69.20% from 2015 to 2023. The number of people commuting by carpool also decreased from 9.70% to 9.00% over the same time period (see Table 2.2). Regardless of these decreases, the number of households in Rockwall that have two (2) or more vehicles increased from 84.00% to 87.50% over the same time period. All of these statistics indicate that the City of Rockwall's transportation system continues to be auto-centric and that most residents are auto-dependent. This is typical of most suburbs, which act as bedroom communities to larger cities in a region; however, the decreases in commuter numbers show that this trend may

be changing. At this point, it is unclear if this trend will continue moving forward or if it is just the result of a singular event.

TABLE 2.1: TRAVEL TIMES TO WORK IN MINUTES

KEY: BLUE = INCREASE; RED = DECREASE

| TRAVEL TIME | 2015 | 2023 | CHANGE |
|-------------|--------|--------|---------|
| 0-9 | 13.66% | 12.50% | -1.16% |
| 10-14 | 12.78% | 13.20% | 0.42% |
| 15-19 | 12.63% | 15.20% | 2.57% |
| 20-24 | 9.54% | 14.20% | 4.66% |
| 25-29 | 3.67% | 7.00% | 3.33% |
| 30-34 | 19.82% | 13.60% | -6.22% |
| 35-44 | 17.62% | 7.30% | -10.32% |
| 45-59 | 8.08% | 8.10% | 0.02% |
| 60-90 | 2.20% | 8.90% | 6.70% |

SOURCE: 2017 EXISTING CONDITIONS REPORT & 2023 AMERICAN COMMUNITY SURVEY

TABLE 2.2: MEANS OF TRANSPORTATION TO WORK

KEY: BLUE = INCREASE; RED = DECREASE

| MODE OF TRAVEL | 2015 | 2023 | CHANGE |
|------------------------------------|--------|--------|---------|
| DRIVE ALONE | 81.90% | 69.20% | -12.70% |
| CARPOLED | 9.70% | 9.00% | -0.70% |
| WORKED FROM HOME | 5.70% | 13.80% | 8.10% |
| TAXICAB, MOTORCYCLE OR OTHER MEANS | 1.80% | 1.50% | -0.30% |
| PUBLIC TRANSIT | 0.70% | 3.50% | 2.80% |
| WALKED | 0.30% | 2.40% | 2.10% |
| BICYCLE | 0.00% | 0.50% | 0.50% |

SOURCE: 2017 EXISTING CONDITIONS REPORT & 2023 AMERICAN COMMUNITY SURVEY

INFRASTRUCTURE PLANNING

An emerging concern for municipalities across the country is the high cost of maintaining existing infrastructure. For the City of Rockwall, the majority of the existing infrastructure is relatively new with an estimated 27.83% of the City's water lines, 33.47% of the City's wastewater lines, and 19.64% of the City's stormwater lines being installed prior to 2000; however, this could cause future issues with repair/replacement, considering a large share of the City's infrastructure was also constructed during the 2000-2009 time period (i.e. 40.27% of the City's water lines, 33.40% of the City's wastewater lines, and 45.73% of the City's stormwater lines). These facilities typically carry an approximate life cycle of 50-years (with stormwater lines being approximately 75-years), which means the City may be looking at repair and/or replacement of these lines starting in 2050. Looking at the City's roadways, only 10.80% of the City maintained roadways are currently asphalt; however, these roadways require repair annually and only have a usable life cycle of five (5) to ten (10) years. This is drastically different than the life cycle of a concrete roadway, which

typically has a life cycle of 30 - 50 years. This is a major reason the City of Rockwall requires that all new streets and parking areas be of concrete construction.

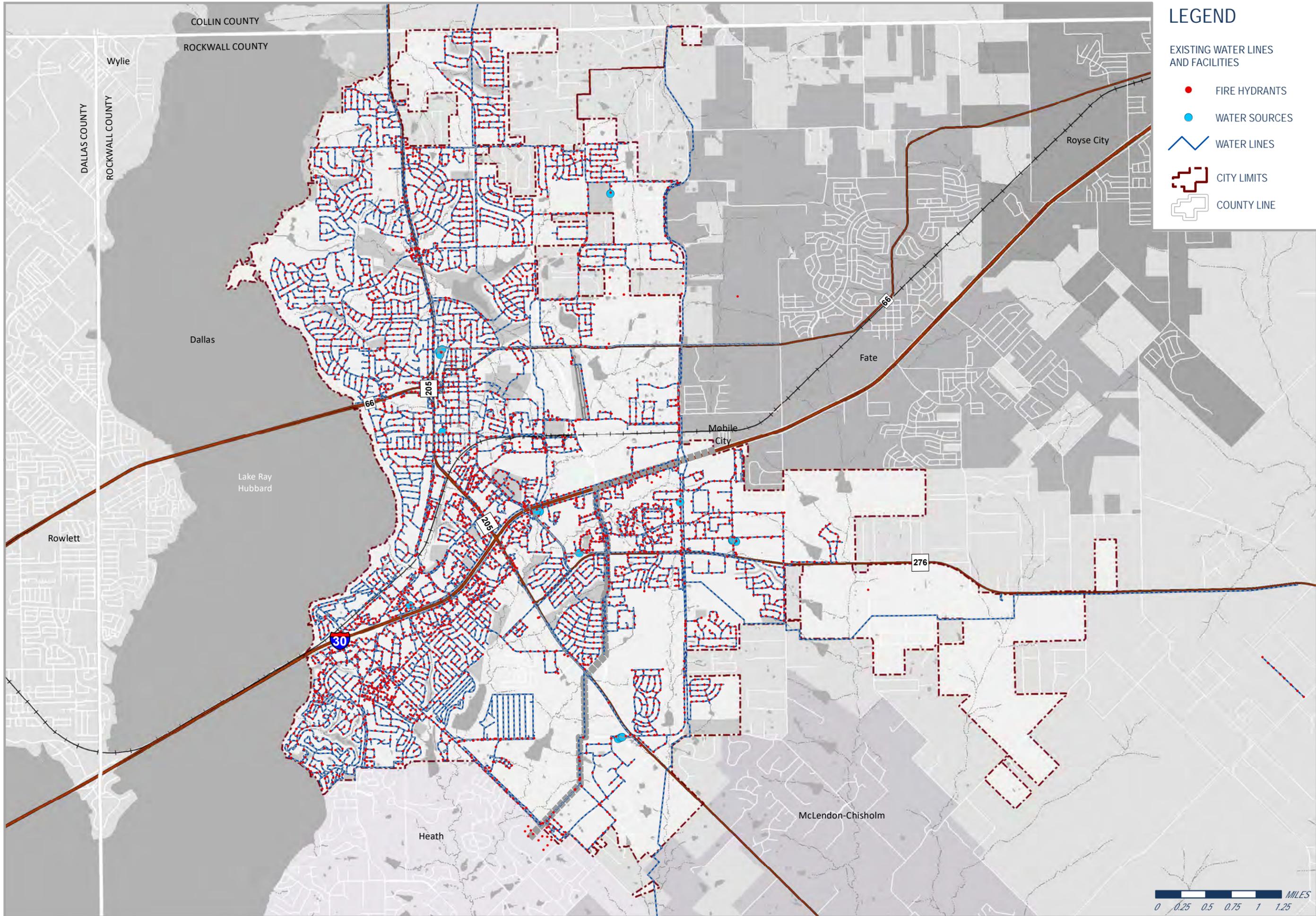
To address some of the concerns about the aging infrastructure, the City's Engineering and Public Works Departments started a comprehensive paving assessment of all the existing streets and alleyways in the City in 2016-2017. This assessment is updated every five (5) years, and was updated in 2022. In addition, the City logs, monitors, and maintains all its infrastructure assets in a geospatial format through the City's Geographic Information Systems software, and implemented an asset management system in 2018 to help track the City's various infrastructure assets and their life cycle. The City also uses WinCan, a CCTV software, that stores videos of the inside of the City wastewater lines. These efforts continue to help the City target current infrastructure needs, coordinate the repair and replacement of infrastructure assets, and balance future infrastructure spending.

ENDNOTES

¹ *Rockwall County Planning Consortium Report (January 2025). Rockwall County Consortium. Accessed April 2025.*

MAP INDEX

- (1) *MAP 2.1: MAP OF EXISTING WATER LINES & FACILITIES*
 - (2) *MAP 2.2: MAP OF EXISTING WASTEWATER LINES & FACILITIES*
 - (3) *MAP 2.3: MAP OF EXISTING STORMWATER LINES & FACILITIES*
 - (4) *MAP 2.4: MASTER WATER PLAN MAP*
 - (5) *MAP 2.5: MASTER WASTEWATER PLAN MAP*
 - (6) *MAP 2.6: EXISTING STREET MAP*
 - (7) *MAP 2.7: MASTER TRANSPORTATION PLAN (MTP) MAP*
 - (8) *MAP 2.8: CAPITAL IMPROVEMENTS PLAN (CIP) MAP*
-



LEGEND

EXISTING WATER LINES AND FACILITIES

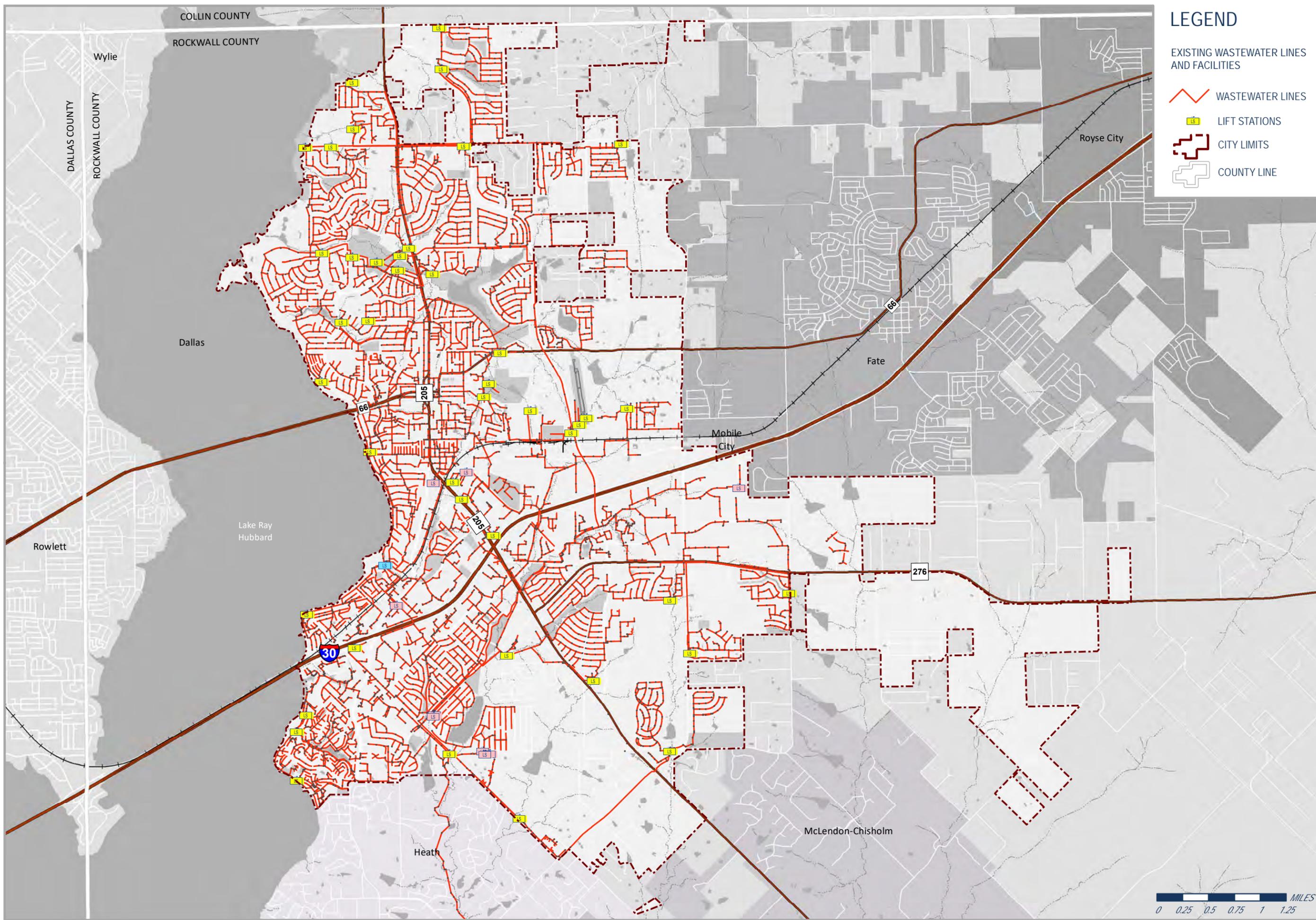
- FIRE HYDRANTS
- WATER SOURCES
- WATER LINES
- CITY LIMITS
- COUNTY LINE



EXISTING WATER LINES AND FACILITIES

CH.2: TRANSPORTATION AND INFRASTRUCTURE





LEGEND

EXISTING WASTEWATER LINES AND FACILITIES

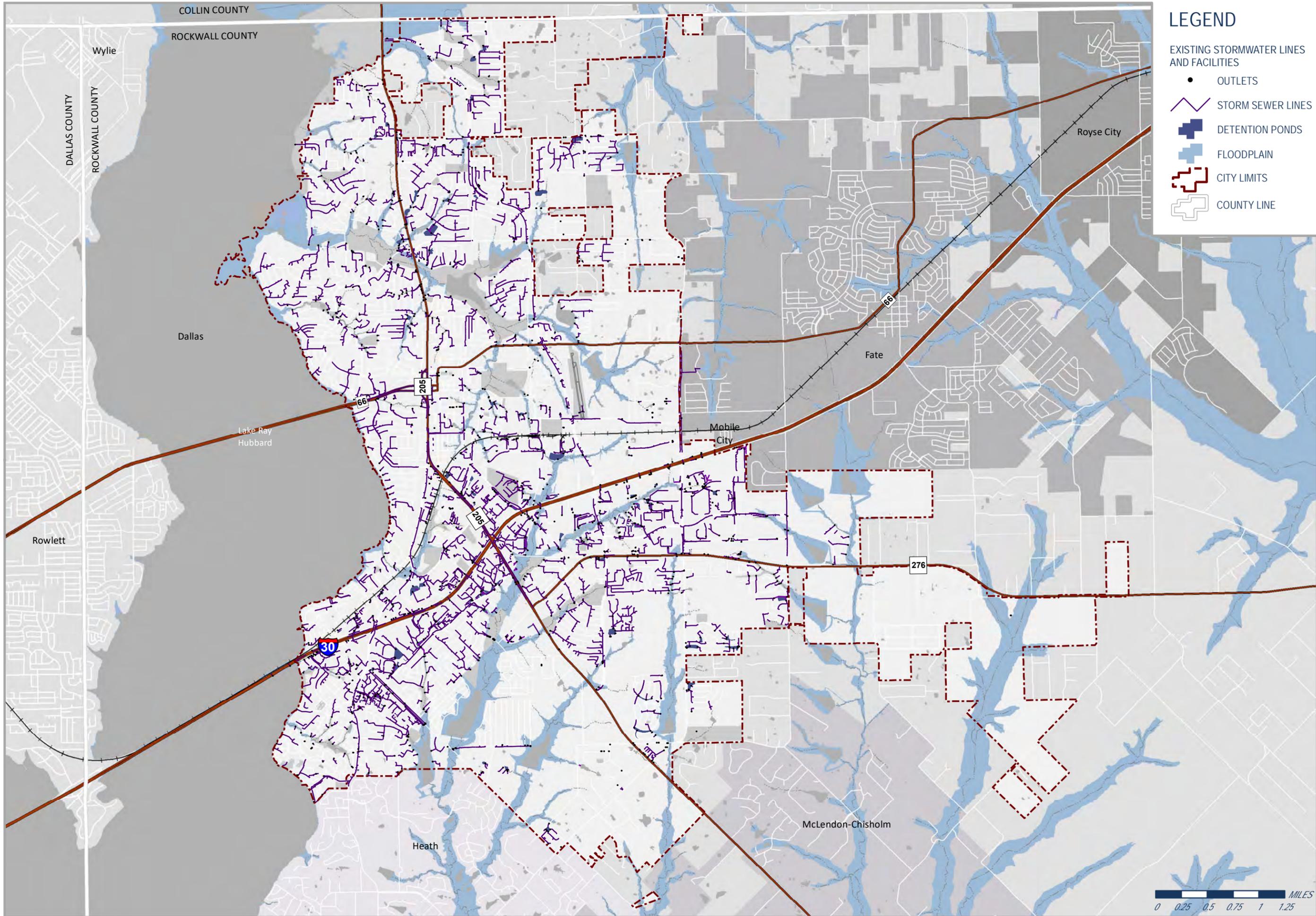
-  WASTEWATER LINES
-  LIFT STATIONS
-  CITY LIMITS
-  COUNTY LINE



EXISTING WASTEWATER LINES AND FACILITIES

CH.2: TRANSPORTATION AND INFRASTRUCTURE





LEGEND

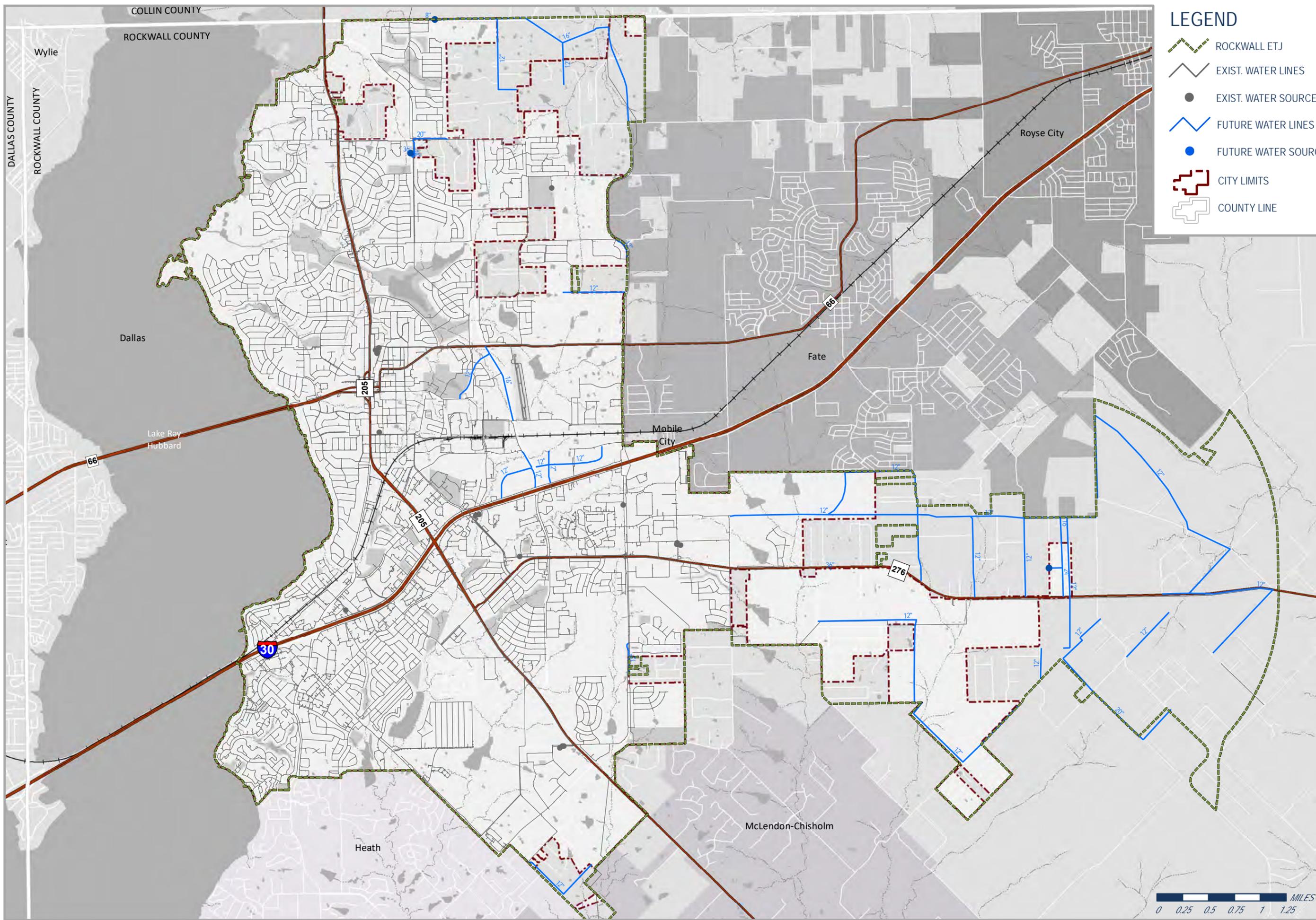
EXISTING STORMWATER LINES AND FACILITIES

- OUTLETS
- STORM SEWER LINES
- DETENTION PONDS
- FLOODPLAIN
- - - CITY LIMITS
- COUNTY LINE

EXISTING STORMWATER LINES AND FACILITIES

CH.2: TRANSPORTATION AND INFRASTRUCTURE

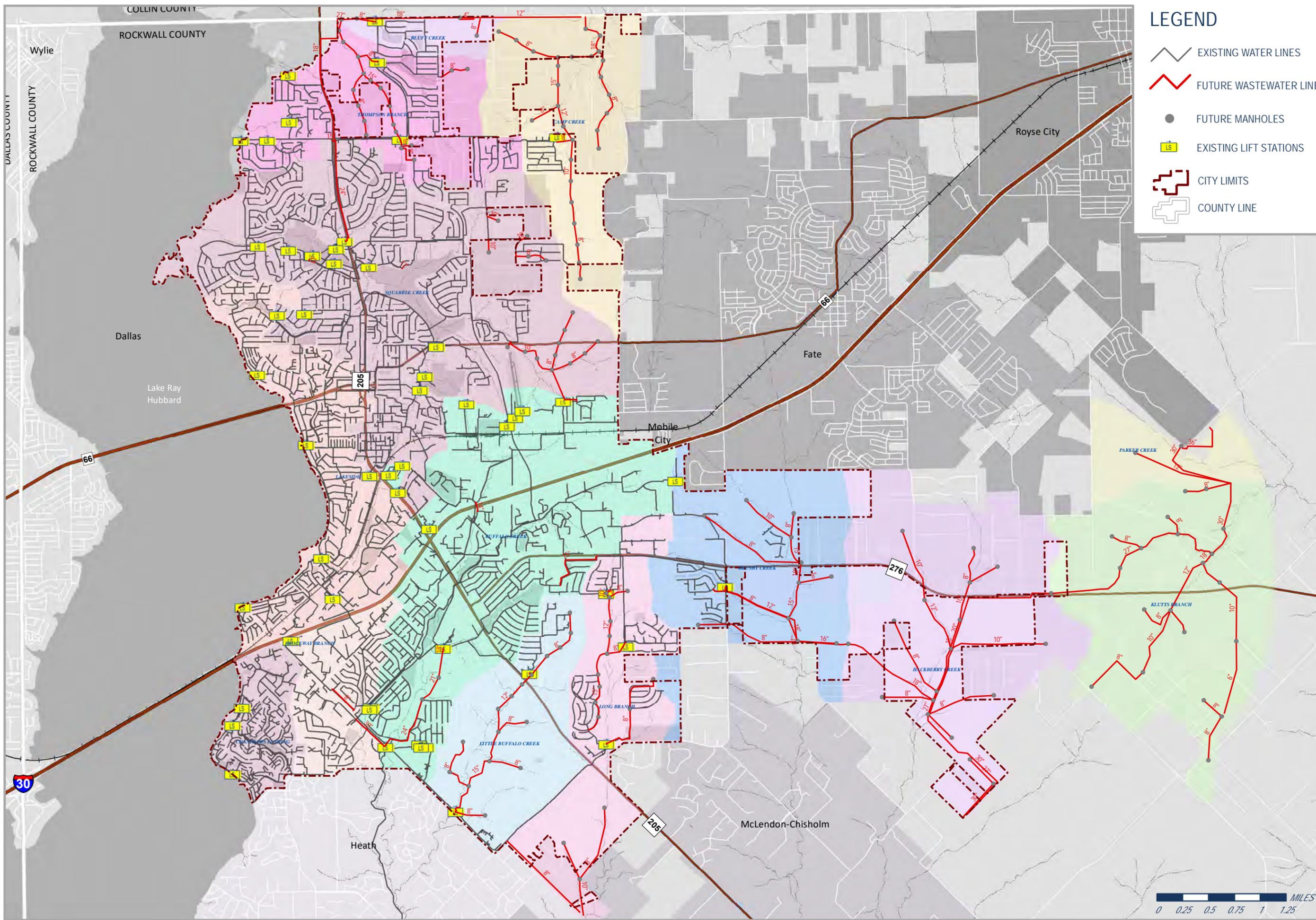




- LEGEND**
- ROCKWALL ETJ
 - EXIST. WATER LINES
 - EXIST. WATER SOURCES
 - FUTURE WATER LINES
 - FUTURE WATER SOURCES
 - CITY LIMITS
 - COUNTY LINE

MASTER WATER PLAN MAP 2.4

CH.2: TRANSPORTATION AND INFRASTRUCTURE



LEGEND

- EXISTING WATER LINES
- FUTURE WASTEWATER LINES
- FUTURE MANHOLES
- EXISTING LIFT STATIONS
- CITY LIMITS
- COUNTY LINE

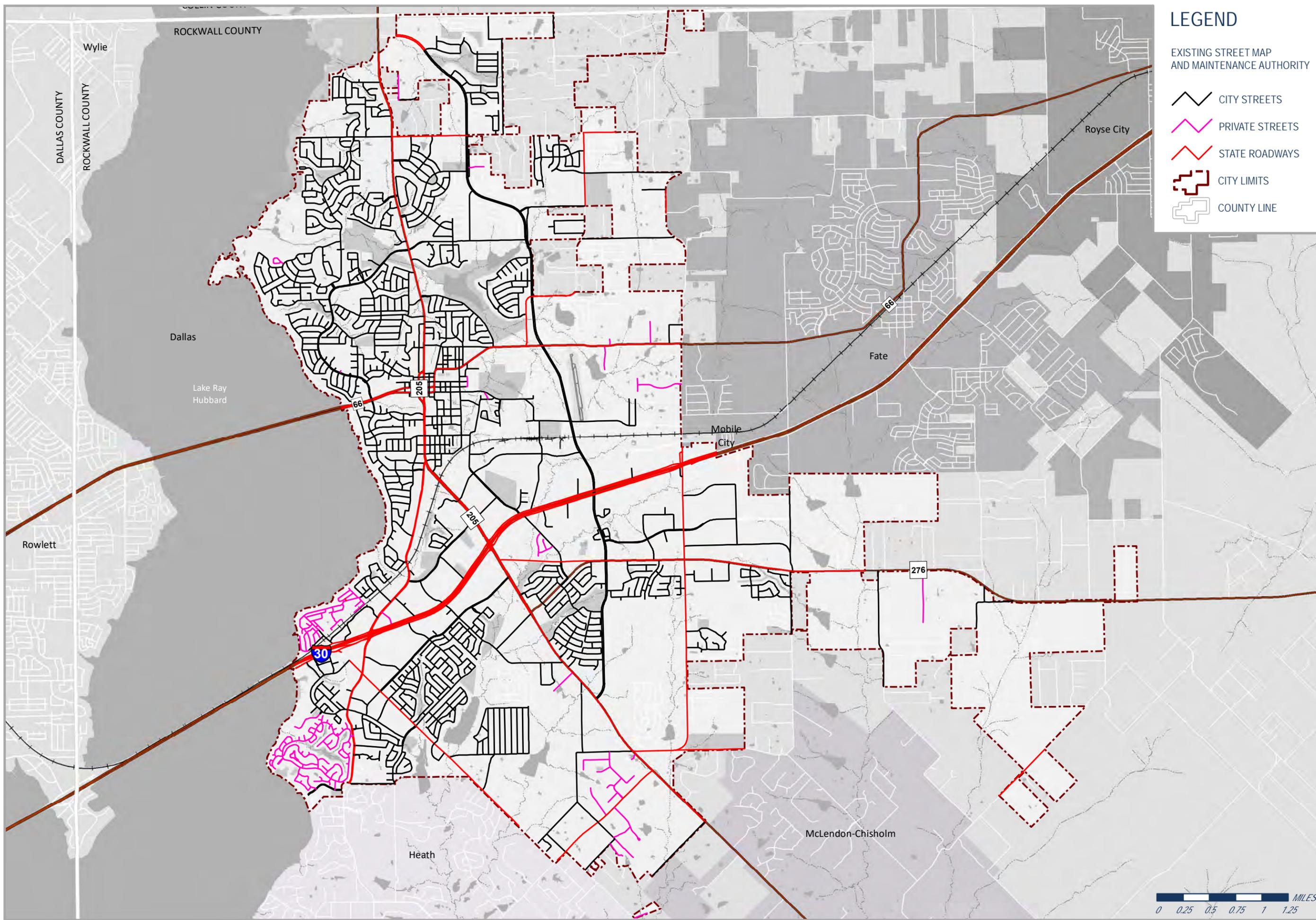


**MASTER WASTEWATER
PLAN MAP**

2.5

**CH.2: TRANSPORTATION
AND INFRASTRUCTURE**





LEGEND

EXISTING STREET MAP AND MAINTENANCE AUTHORITY

-  CITY STREETS
-  PRIVATE STREETS
-  STATE ROADWAYS
-  CITY LIMITS
-  COUNTY LINE

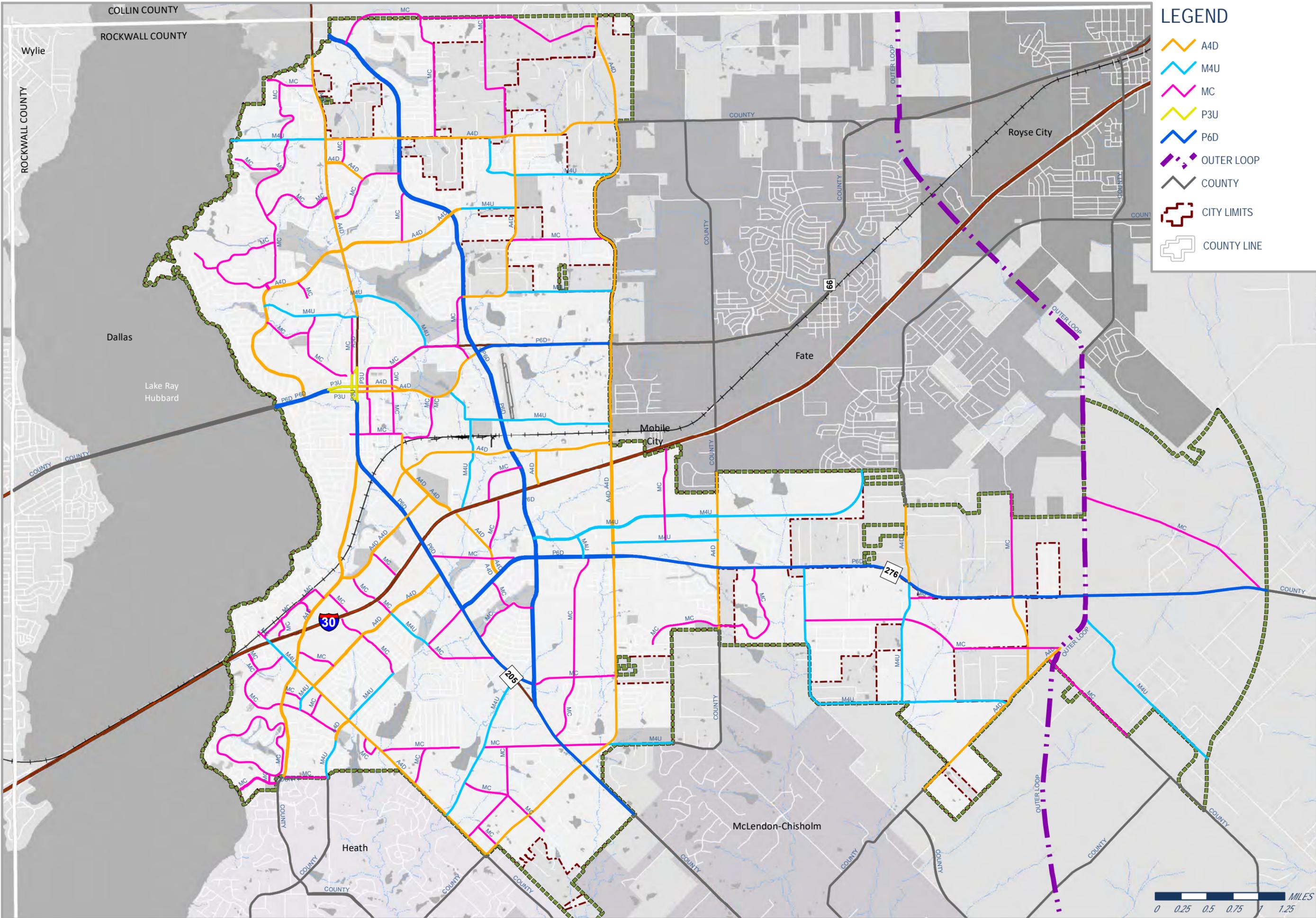


EXISTING STREET MAP

CH.2: TRANSPORTATION AND INFRASTRUCTURE



2.6



LEGEND

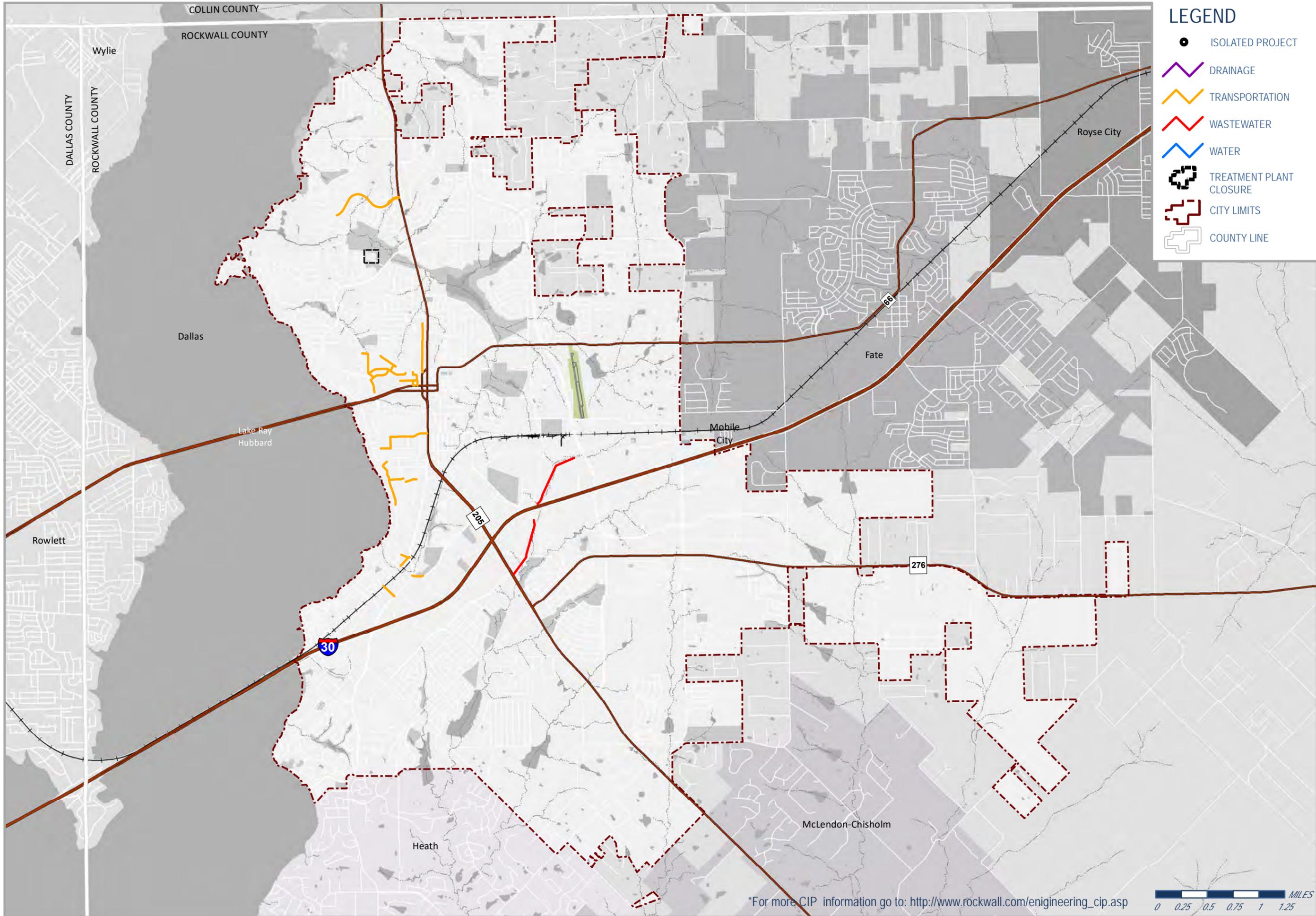
- A4D
- M4U
- MC
- P3U
- P6D
- OUTER LOOP
- COUNTY
- CITY LIMITS
- COUNTY LINE



MASTER TRANSPORTATION PLAN
2.7

CH.2: TRANSPORTATION AND INFRASTRUCTURE





LEGEND

- ISOLATED PROJECT
- ~ DRAINAGE
- ~ TRANSPORTATION
- ~ WASTEWATER
- ~ WATER
- ⊗ TREATMENT PLANT CLOSURE
- ⊞ CITY LIMITS
- ⊞ COUNTY LINE

*For more CIP information go to: http://www.rockwall.com/engineering_cip.asp



**CAPITAL IMPROVEMENT
PLAN MAP**

2.8

**CH.2: TRANSPORTATION
AND INFRASTRUCTURE**



3 LAND USE AND BUILD OUT





THE HORIZON BRIDGE AND IH-30 IMPROVEMENTS

THE PICTURE ABOVE SHOWS AN IMAGE OF THE HORIZON BRIDGE AND IH-30 IMPROVEMENTS AT NIGHT. LOOKING WEST ON THE SOUTHSIDE OF THE BRIDGE OVER LAKE RAY HUBBARD YOU CAN SEE THE BEGINNINGS OF THE NEW SERVICE ROADS THAT WILL RUN PARALLEL TO THE MAIN LANES THAT ARE CURRENTLY IN EXISTENCE. THIS AREA FOR IH-30 IMPROVEMENTS BEGAN IN MARCH 2023 AND IS ANTICIPATED TO CONCLUDE IN JULY 2027. FOLLOWING IH-30 WEST TOWARDS ROWLETT YOU CAN SEE THE LIGHTS OF THE DALLAS SKYLINE. ALSO PICTURED IN THIS IMAGE SOUTH OF IH-30 IS THE HARBOR DISTRICT.

LAND USE

In its corporate boundaries the City of Rockwall currently has 20,610 parcels or tracts of land (i.e. an increase of 2,988 since 2017), which total 19,258.44-acres or 30.09-square miles (i.e. an increase of 178.75-acres or an increase of 0.28-square miles since 2017). Each of these parcels carries three (3) designations: [1] a current land use designation, [2] a zoning designation, and [3] a future land use designation. In addition, the City of Rockwall's Extraterritorial Jurisdiction (ETJ) totals 7,393.45-acres or 11.55-square miles (i.e. a decrease of 3,078.95-acres or 4.81-square miles since 2017), and contains 1,973 parcels or tracts of land (i.e. a decrease of 200 since 2017). These parcels carry both a current land use and a future land use designation. As opposed to the parcels inside the City's corporate boundaries, parcels in a City's ETJ are not subject to zoning requirements. This is due to the fact that counties are not granted this authority through the Texas Zoning Enabling Act, which was adopted in 1927. Each of the designations for these parcels -- both inside the City's corporate boundaries and in the ETJ -- play an important role in the land use planning process. See Map 3.1 for a depiction of the City's corporate boundaries and its ETJ.

ANNEXATION AND THE EXTRATERRITORIAL JURISDICTION (ETJ)

Since the original 2017 Existing Conditions Report was drafted, several important pieces of legislation have been adopted by the Texas Legislature during the 86th, 87th, & 88th Legislative Sessions that effect a City's ability to annex and regulate property within the Extraterritorial Jurisdiction (ETJ). The two (2) bills with the greatest impact to annexation and regulation within the ETJ for the City of Rockwall were HB347 -- which was adopted in 2019 as part of the 86th Legislative Session --, and SB2038 -- which was adopted in 2023 as part of the 88th Legislative Session.

HB347 -- which became effective on May 24, 2019 -- effectively ended most unilateral annexation actions for municipalities in Texas. Specifically, this bill ended the Tier 1 and Tier 2 status for municipalities created by SB6 in 2015, and requires an election of the property owners in the proposed annexed area in order to initiate annexation. Based on this bill municipal annexation has been relegated to a voluntary process initiated by the property owner of a particular property. After HB347 was adopted, the City of Rockwall has only had two (2) annexation requests in the past six

(6) years, both of which were initiated by the property owners.

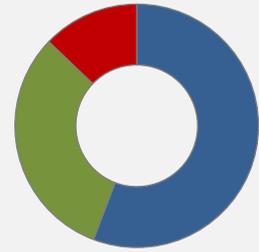
SB2038 -- which was passed by the Texas Legislature on May 19, 2023 and went into effect on September 1, 2023 -- allows for property owners in a City's Extraterritorial Jurisdiction (ETJ) to disincorporate or release themselves from the ETJ through a petition or election; however, in most cases this is done by providing the municipality with a simple letter and legal description of the property. This legislation makes it extremely difficult for cities to plan for, manage, and regulate growth in the ETJ that effects citizens within the corporate limits. It also puts an unnecessary strain on County governments, because once a property has removed itself from a City's Extraterritorial Jurisdiction (ETJ) it is no longer subject to any interlocal agreements between the City and County. Since its adoption, the City of Rockwall has had nine (9) petitions for release that have accounted for the loss of 84.36-acres of land (see Map 3.2). This trend is expected to continue as more property owners are made aware of SB2038.

As this document was being prepared, the Texas Legislature began the 89th Legislative Session, and -- as with the previous three (3) Legislative Sessions -- several bills relating to annexation and a city's ability to regulate property within the ETJ have been filed. These bills all have restrictive standards that, if passed, could further restrict a municipality's ability to annex and regulate development within the ETJ and potentially property within the City's corporate boundaries. These bills ultimately have the possibility of further inhibiting cities from protecting their citizens from uncontrolled growth and inconsistent development within their jurisdictional boundaries.

BUILT/VACANT LAND

Currently, the City of Rockwall has ~10,743.38-acres of developed land (i.e. 55.79%), ~6,043.82-acres of undeveloped land (i.e. ~31.38%), and ~2,471.24-acres of right-of-way (i.e. 12.83%) (see Figure 3.1 and Map 3.3). This, however, is only the raw built/vacant numbers for the City. To get a more complete picture of the built/vacant percentage of the City's remaining land, several additional factors have to be considered. For example, of the 6,043.82-acres of raw undeveloped

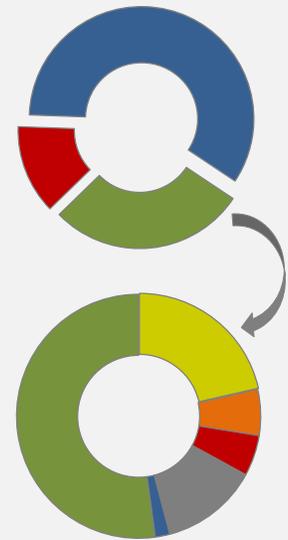
FIGURE 3.1: RAW BUILT/VACANT LAND
 KEY: BLUE = BUILT (55.79% OR 10,743.38-ACRES); GREEN = VACANT (31.38% OR 6,043.82-ACRES); RED = RIGHT-OF-WAY (12.83% OR 2,471.24-ACRES)



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 3.2: VACANT LAND BREAKDOWN
 NOTE: 580.28-ACRES REMOVED DUE TO BEING PLATTED LOTS IN ACTIVELY PERMITTING SUBDIVISIONS

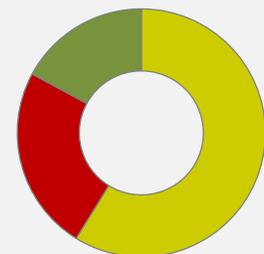
KEY (CHART 1: ADJUSTED BUILT/VACANT): BLUE = BUILT (58.80% OR 11,323.66-ACRES); GREEN = VACANT (28.37% OR 5,463.54-ACRES); RED = RIGHT-OF-WAY (12.83% OR 2,471.24-ACRES)



KEY (CHART 2: VACANT LAND BREAKDOWN): YELLOW = RESIDENTIAL LAND WITH ACTIVE ENTITLEMENTS (21.43% OR 1,170.96-ACRES); ORANGE = RESIDENTIAL LAND WITH DORMANT ENTITLEMENTS (333.71-ACRES OR 6.11%); RED = NON-RESIDENTIAL LAND WITH ACTIVE ENTITLEMENTS (296.44-ACRES OR 5.43%); GREY = NON-RESIDENTIAL LAND WITH DORMANT ENTITLEMENTS (705.96-ACRES OR 12.92%); BLUE = PUBLIC OR QUASI-PUBLIC VACANT LAND (106.55-ACRES OR 1.95%); GREEN = VACANT LAND (2,849.93-ACRES OR 52.16%)

SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 3.3: UNENTITLED VACANT LAND BREAKDOWN
 KEY (CHART 1): YELLOW = RESIDENTIAL (1,676.65-ACRES OR 58.81%); RED = NON-RESIDENTIAL (686.03-ACRES OR 24.06%); GREEN = PARKS AND OPENSOURCE (488.11-ACRES OR 17.12%)



SOURCE: CITY OF ROCKWALL GIS DIVISION

land, 580.28-acres of this land includes parcels that have been platted as part of residential subdivisions that are actively under construction. In addition, another 2,507.06-acres of land is currently undeveloped, but has been entitled or zoned. Another 106.55-acres of this land is owned by public or quasi-public agencies. This leaves only 2,849.93-acres of land that is vacant and not currently entitled (see Figure 3.2 and Map 3.4). Figure 3.3 shows the breakdown of the projected future land uses of the remaining vacant land in the City based on the land use designation on the Future Land Use Plan (also see Map 3.5). Approximately 58.81% or 1,676.65-acres of this land is projected to be developed with residential land uses and 24.06% or 686.03-acres of this land is projected to be developed with non-residential land uses. The remaining 24.06% or 488.11-acres is anticipated to be parks and open space (see Figure 3.3).

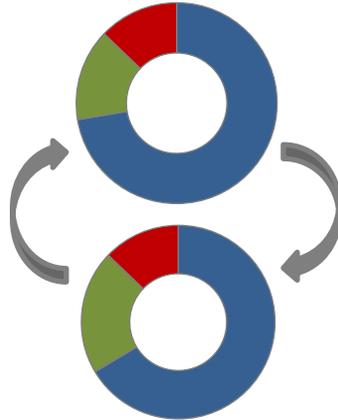
Of the ~2,507.06-acres of vacant land that is currently entitled or zoned, 1,467.39-acres has active entitlements (i.e. entitlements that are currently working through the development process or that have been granted within the last five [5] years). This means that the remaining 1,039.67-acres are considered to be dormant entitlements or entitlements which were granted more than five (5) years ago and which are not currently working through the development process.

From this information, two (2) scenarios for the City's built/vacant percentage emerge. The first is the built/vacant percentage that just includes the vacant, unentitled land, and the

second is the built/vacant percentage that includes the vacant, unentitled land plus the vacant land that is entitled with dormant entitlements. This puts the City's true built/vacant percentage between 14.80% (i.e. 2,850.79-acres) and 20.75% (i.e. 3,996.14-acres) (see Figure 3.4).

FIGURE 3.4: BUILT/VACANT SCENARIOS
KEY: BLUE = BUILT; GREEN = VACANT; RED = RIGHT-OF-WAY (ROW)

SCENARIO 1: 72.37% BUILT; 14.80% VACANT; 12.83% ROW



SCENARIO 2: 66.42% BUILT; 20.75% VACANT; 12.83% ROW

It should be noted that of the 1,676.65-acres of all vacant land (i.e. land that is vacant and unentitled or vacant and entitled) that is designated for residential land uses in Figure 3.3, 931.571-acres (or 55.56%) is situated east of Rochelle Road. The reason that this location is significant, is that the City's wastewater collection system is not currently available in this area, and any development of

this area would have to make use of On-Site Sewage Facilities (OSSF's) or septic systems. In addition, the City's Municipal Code of Ordinances stipulates that all properties that make use of an OSSF system be a minimum of 1½-acres in size. It should be noted that the City Council has granted reductions to the 1½-acre size requirement, but has *not* typically allowed OSSF systems on lots less than one (1) acre in size. This also means that under the current Future Land Use Plan, only 745.08-acres or 44.44% of the remaining vacant residential land is currently capable of being developed in accordance with the densities depicted in the plan. This includes 582.93-acres of land that is designated for Low Density Residential land uses (which allows densities of two [2] to 2½ units per acre), and 162.15-acres of land that is designated for Medium Density Residential land uses (which allows densities of up to 2½ to three [3] units per acre).

CURRENT LAND USE

Current Land Use describes the present utilization of a parcel of land. The City of Rockwall's current land use -- broken down by land use category -- is depicted in Table 3.1 and in Map 3.6. Based on the City's current land use breakdown, the City of Rockwall has developed as a traditional suburban community with single-family development comprising nearly 30.00% of all development within the City (i.e. 29.48% or 5,676.77-acres). This represents an increase of 14.69% or an additional 727.20-acres of single-family development from 2017. Another common characteristic of suburban development is a

TABLE 3.1: CURRENT LAND USE BY LAND USE CATEGORY, 2017-2025

| ZONING DISTRICT | 2017 | | 2025 | | CHANGE FROM 2017-2025 | |
|-----------------------|-----------|------------|-----------|------------|-----------------------|----------------|
| | ACRES | PERCENTAGE | ACRES | PERCENTAGE | ACRES | PERCENT CHANGE |
| TWO FAMILY/DUPLEX | 28.55 | 0.15% | 32.02 | 0.17% | 3.47 | 12.15% |
| AGRICULTURAL/VACANT | 5,799.51 | 30.40% | 4,476.35 | 23.24% | (1,323.16) | (22.82%) |
| COMMERCIAL | 1,407.97 | 7.38% | 1,506.22 | 7.82% | 98.25 | 6.98% |
| CEMETERY | 59.57 | 0.31% | 79.15 | 0.41% | 19.58 | 32.87% |
| DOWNTOWN | 34.73 | 0.18% | 34.78 | 0.18% | 0.05 | 0.14% |
| GENERAL RETAIL | 365.33 | 1.91% | 466.23 | 2.42% | 100.90 | 27.62% |
| HEAVY COMMERCIAL | 114.45 | 0.60% | 149.26 | 0.78% | 34.81 | 30.42% |
| LIGHT INDUSTRIAL | 1,623.44 | 8.51% | 1,144.96 | 5.95% | (478.48) | (29.47%) |
| MULTI-FAMILY | 165.25 | 0.87% | 209.86 | 1.09% | 44.61 | 27.00% |
| NEIGHBORHOOD SERVICES | 10.28 | 0.05% | 5.62 | 0.03% | (4.66) | (45.33%) |
| OPEN SPACE/FLOODPLAIN | 1,687.15 | 8.84% | 2,015.07 | 10.46% | 327.92 | 19.44% |
| PUBLIC | 502.58 | 2.63% | 754.84 | 3.92% | 252.26 | 50.19% |
| QUASI-PUBLIC | 140.29 | 0.74% | 200.94 | 1.04% | 60.65 | 43.23% |
| RESIDENTIAL-OFFICE | 49.74 | 0.26% | 35.13 | 0.18% | (14.61) | (29.37%) |
| RIGHT-OF-WAY | 2,141.27 | 11.22% | 2,471.24 | 12.83% | 329.97 | 15.41% |
| SINGLE-FAMILY | 4,949.57 | 25.94% | 5,676.77 | 29.48% | 727.20 | 14.69% |
| TOTALS | 19,079.68 | 100.00% | 19,258.44 | 100.00% | 178.76 | 0.94% |

SOURCE: CITY OF ROCKWALL GIS DIVISION

high percentage of roads and alleyways. Currently, the City of Rockwall's roads and alleyways makeup 12.83% or 2,471.24-acres of all current land uses. This represents an increase of 15.41% or an additional 329.97-acres of roadways and alleyways being added between 2017-2025.

Looking at the City's current non-residential land uses *Commercial* and *General Retail* land uses make up a combined 10.24% (i.e. 7.82% or 1,506.22-acres of *Commercial* and 2.42% or 466.23-acres of *General Retail*). In addition, both of these land use categories saw increases from 2017-2025 (i.e. with *Commercial* growing by 6.98% or 98.25-acres and *General Retail* growing by 27.62% or 100.90-acres). The next largest non-residential current land use is the *Industrial* land use, which is currently composed of 5.95% or 1,144.96-acres of all land uses. This land use has seen a decrease from 2017-2025 of 29.47% or a reduction of 478.48-acres. The majority of this reduction is the zoning change for the Discovery Lakes Subdivision, which took place in 2015 and changed approximately 316.315-acres of land zoned Light Industrial (LI) District and changed it to a Planned Development District for General Retail (GR) District and Single-Family 10 (SF-10) District. Despite being rezoned in 2015, it was erroneously counted as Light Industrial (LI) District in the 2017 Existing Conditions Report.

Currently, the City of Rockwall has a large rural reserve with *Agricultural/Vacant* land making up 23.24% or 4,476.35-acres of all current land uses; however, this percentage

did decrease by 22.82% or 1,323.16-acres between 2017-2025. It is important to note that while this does include vacant land, a large majority of this acreage is residential estate lots that were annexed into the City and are not projected to change in the future.

ZONING

Municipalities use *zoning* to control and direct development within their corporate boundaries. A property's zoning designation not only drives what a property can be used for, but also the development standards for that property. Depicted in *Table 3.2: Land by Zoning Classification* is the City of Rockwall's zoning map broken down by each zoning district's acreage (see *Map 3.7*). The zoning district that contains the largest acreage within the City (i.e. 47.67% or 9,179.97-acres) is the Planned Development District designation. A Planned Development District is an aggregation of several different uses/zoning classifications and development standards. It is a specialized form of zoning that allows more flexibility than traditional zoning by accommodating project specific regulations (i.e. *tailoring the zoning regulations to a project or concept plan*). Currently, the City of Rockwall has 103 Planned Development Districts within its boundaries, which represents an increase of 21 new Planned Development Districts being added over the last seven (7) years. The majority of these Planned Development Districts are geared towards residential land uses; however, each of these districts has its own set of permissible land uses and development standards.

The second largest zoning classification in terms of land area is the *Agricultural (AG) District*, which is at 22.94% or 4,418.27-acres; however, this district has decreased by 33.57% or 2,232.95-acres from 2017 to 2025. The *Agricultural (AG) District* prior to 2019 was used as a *placeholder* for properties that were annexed into the City, but that did not have a defined use or a use that conformed to the Future Land Use Map. With the adoption of the new annexation laws the City is no longer able to unilaterally annex property, which is why this zoning district is seeing a decreasing trend.

The largest non-residential zoning districts are the *Light Industrial (LI) District* at 8.02% (i.e. 1,544.01-acres) and the *Commercial (C) District* at 7.02%; however, neither of these zoning districts have seen much growth over the last seven (7) years, with the *Commercial (C) District* decreasing by 1.18-acres (or 0.09%) and the *Light Industrial (LI) District* increasing by 5.20-acres (or 0.34%). The remainder of the City's zoning districts are evenly dispersed with the majority being associated with single-family residential land use.

FUTURE LAND USE

On December 3, 2018, the City Council adopted *Ordinance No. 18-48*, which implemented the OURHometown Vision 2040 Comprehensive Plan. This adoption was the culmination of a two (2) year planning effort between the Comprehensive Plan Advisory Committee (CPAC) and the City's Planning and Zoning Department. As part of this

TABLE 3.2: LAND BY ZONING CLASSIFICATION, 2017-2025

| ZONING DISTRICT | 2017 | | 2025 | | CHANGE FROM 2017-2025 | |
|------------------------------------|-----------|------------|-----------|------------|-----------------------|----------------|
| | ACRES | PERCENTAGE | ACRES | PERCENTAGE | ACRES | PERCENT CHANGE |
| TWO FAMILY (2F) | 33.96 | 0.18% | 15.58 | 0.08% | (18.38) | (54.12%) |
| AGRICULTURAL (AG) | 6,651.22 | 34.86% | 4,418.27 | 22.94% | (2,232.95) | (33.57%) |
| COMMERCIAL (C) | 1,352.60 | 7.09% | 1,351.42 | 7.02% | (1.18) | (0.09%) |
| CEMETERY (CEM) | 25.70 | 0.13% | 25.70 | 0.13% | - | 0.00% |
| DOWNTOWN (DT) | 69.84 | 0.37% | 70.10 | 0.36% | 0.26 | 0.37% |
| GENERAL RETAIL (GR) | 107.00 | 0.56% | 134.68 | 0.70% | 27.68 | 25.87% |
| HEAVY COMMERCIAL (HC) | 108.72 | 0.57% | 65.40 | 0.34% | (43.32) | (39.85%) |
| LIGHT INDUSTRIAL (LI) | 1,538.81 | 8.07% | 1,544.01 | 8.02% | 5.20 | 0.34% |
| MULTI-FAMILY 14 (MF-14) | 64.79 | 0.34% | 64.79 | 0.34% | - | 0.00% |
| NEIGHBORHOOD SERVICES (NS) | 24.37 | 0.13% | 5.28 | 0.03% | (19.09) | (78.33%) |
| PLANNED DEVELOPMENT (PD) | 6,703.21 | 35.13% | 9,179.97 | 47.67% | 2,476.76 | 36.95% |
| RESIDENTIAL-OFFICE (RO) | 6.01 | 0.03% | 7.89 | 0.04% | 1.88 | 31.28% |
| SINGLE-FAMILY 1 (SF-1) | 2.27 | 0.01% | 22.34 | 0.12% | 20.07 | 884.14% |
| SINGLE-FAMILY 10 (SF-10) | 1,010.37 | 5.30% | 1,008.00 | 5.23% | (2.37) | (0.23%) |
| SINGLE-FAMILY 16 (SF-16) | 310.87 | 1.63% | 205.55 | 1.07% | (105.32) | (33.88%) |
| SINGLE-FAMILY 7 (SF-7) | 540.93 | 2.84% | 540.93 | 2.81% | - | 0.00% |
| SINGLE-FAMILY ESTATE 1.5 (SFE-1.5) | 268.56 | 1.41% | 316.27 | 1.64% | 47.71 | 17.77% |
| SINGLE-FAMILY ESTATE 2.0 (SFE-2.0) | 43.78 | 0.23% | 63.96 | 0.33% | 20.18 | 46.09% |
| SINGLE-FAMILY ESTATE 4.0 (SFE-4.0) | 212.90 | 1.12% | 214.53 | 1.11% | 1.63 | 0.77% |
| ZERO LOT LINE (ZL-5) | 3.77 | 0.02% | 3.77 | 0.02% | - | 0.00% |
| TOTALS | 19,079.68 | 100.00% | 19,258.44 | 100.00% | 178.76 | 0.94% |

SOURCE: CITY OF ROCKWALL GIS DIVISION

process, a new *Future Land Use Plan* was adopted segmenting the City into 18 districts (*originally 20 with two [2] districts being removed through the disincorporation of land within the Extraterritorial Jurisdiction [ETJ]*) and redefining the *Future Land Use* categories for each property within the City.

A *Future Land Use* designation is the expected use of a property based on the policies and guidelines established within the City's Comprehensive Plan. A *Future Land Use Map* can be thought of as a community's visual guide or roadmap to its future. It should be noted that a *Future Land Use Map* takes into account all land within the City's corporate boundaries and its Extraterritorial Jurisdiction (ETJ).

When a property owner requests to rezone a property, the *Future Land Use* designation of that property plays a major role in the City Council's decision-making process. The City of Rockwall's *Future Land Use Map (depicted in Map 3.8)* is broken down into 15 land use categories that correspond to each of the City's zoning districts. *Table 3.3* shows each of these designations and how much of the City is set aside for each use. The largest designation is the *Low Density Residential* designation, which takes up 7,459.68-acres or 27.99% of the City at buildout. The next highest designation is the *Medium Density Residential* designation, which is reserved for 6,413.49-acres or 24.06% of the City of Rockwall. In all, the City is projected to be 53.14% residential land uses, 10.59% non-residential land uses, 3.74% public/quasi-public land uses, 14.96% parks and open space, and 17.56% right-of-way.

TABLE 3.3: LAND BY FUTURE LAND USE DESIGNATION, 2025

| FUTURE LAND USE DESIGNATION | ACRES | % |
|-----------------------------|-----------|---------|
| RIGHT-OF-WAY | 4,681.12 | 17.56% |
| CEMETERY | 63.82 | 0.24% |
| COMMERCIAL/RETAIL | 177.63 | 0.67% |
| COMMERCIAL/INDUSTRIAL | 126.32 | 0.47% |
| DOWNTOWN | 37.67 | 0.14% |
| BUSINESS CENTERS | 369.15 | 1.39% |
| LIVE/WORK | 67.54 | 0.25% |
| HIGH DENSITY RESIDENTIAL | 290.58 | 1.09% |
| MEDIUM DENSITY RESIDENTIAL | 6,413.49 | 24.06% |
| LOW DENSITY RESIDENTIAL | 7,459.68 | 27.99% |
| MIXED USE | 364.67 | 1.37% |
| PARKS AND OPEN SPACE | 3,987.27 | 14.96% |
| PUBLIC USES | 557.90 | 2.09% |
| QUASI PUBLIC USES | 374.73 | 1.41% |
| SPECIAL COMMERCIAL CORRIDOR | 515.97 | 1.94% |
| TECHNOLOGY/EMPLOYMENT | 1,164.35 | 4.37% |
| TOTALS | 26,651.89 | 100.00% |

SOURCE: CITY OF ROCKWALL GIS DIVISION

BUILD OUT ANALYSIS

Build Out is the estimated date at which all developable land within the City and its Extraterritorial Jurisdiction (ETJ) has been fully developed; however, with the adoption of HB347 in 2019 and SB2038 in 2023, the City no longer has the same controls or certainty with regard to land within its ETJ. More specifically, the City cannot unilaterally annex land within the ETJ as a result of HB347, and the area within the ETJ continues to erode or disincorporate due to SB2038. Due to this, it is difficult for the City of Rockwall to incorporate these areas into its planning efforts, and even more difficult to assume that they will ever be incorporated into the corporate limits of the City.

Based on these changes, the City's *Buildout Analysis* can no longer simply look at the ETJ as being eventually incorporated into its future corporate limits. In addition, there are a number of other factors (*e.g. existing development patterns, availability to utilities, and etcetera*) that need to be taken into consideration when looking at how the City may eventually develop. To best provide all possible *buildout* outcomes, it became apparent that various scenarios needed to be created to look at how the City may develop in the future under various circumstances. Based on this the following lays out the methodology used to create a *Scenario Based Buildout Analysis*.

SCENARIO BASED BUILDOUT ANALYSIS

The following are the scenarios that were used in this *Buildout Analysis*:

- (1) Scenario 1. This scenario assumes that the City's ETJ will *not* be incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *without* wastewater at 1½-units per acre.
- (2) Scenario 2. This scenario assumes that the City's ETJ will *not* be incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *without* wastewater at one (1) unit per acre.
- (3) Scenario 3. This scenario assumes that the City's ETJ will *not* be incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *with* wastewater at their maximum density.
- (4) Scenario 4. This scenario assumes that the City's ETJ *will* eventually be

incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *without* wastewater at 1½-units per acre.

- (5) Scenario 5. This scenario assumes that the City's ETJ *will* eventually be incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *without* wastewater at one (1) unit per acre.
- (6) Scenario 6. This scenario assumes that the City's ETJ *will* eventually be incorporated into the corporate limits at *Buildout*, and the areas east of Rochelle Road will develop *with* wastewater at their maximum density.

ASSUMPTIONS FOR BUILDOUT ANALYSIS

In addition to the above scenarios, a number of assumptions were needed to determine how the City would develop. These assumptions are as follows:

- ☑ Entitled Vacant Land [Applicable to All Scenarios]. All entitled vacant land will develop with the maximum number of housing units permitted for the existing entitlement. For example, if a vacant property is zoned Single-Family 16 (SF-16) District, the maximum number of housing units permitted within this zoning district is 2.7 dwelling units per gross acre. This means that the maximum carrying capacity for this example would be the acreage of the property zoned Single-Family 16 (SF-16) District multiplied by 2.7 dwelling units per acre.
- ☑ Unentitled Vacant Land [Applicable to All Scenarios]. All unentitled vacant land or land zoned Agricultural (AG) District will develop with the maximum number of housing units as determined by the lands' Future Land Use designation. For example, if a property is zoned Agricultural (AG) District and is designated for Low Density Residential (LDR) land uses on the City's Future Land Use Map, the property is assumed to develop at a maximum of two (2) units per gross acre. This would be determined by multiplying the acreage of the property zoned Agricultural (AG) District by two (2) housing units per acre to determine the gross carrying capacity of the land.
- ☑ Density Factors for Population [Applicable to All Scenarios]. The density factor (*also known as the people per*

household) for each Census Tract -- as depicted in Map 3.9 -- is used to determine the population of each household.

☑ ETJ Boundaries [Applicable to Scenarios 4, 5 & 6]. The City's ETJ is fixed and will not increase in the future.

☑ Land within the ETJ [Applicable to Scenarios 4, 5 & 6]. All property within the ETJ that is east of Rochelle Road is assumed to be vacant and will be developed in conformance with the development scenarios. All property within the ETJ that is west of Rochelle Road that is vacant is assumed to develop in accordance with the Future Land Use Map at the maximum density permitted by the Comprehensive Plan.

BUILDOUT CALCULATIONS

Using the assumptions listed above, the following calculations were compiled by the City of Rockwall's Geographic Information Systems (GIS) Division using ESRI's Geographical Information Systems Software:

KEY:
 AC: ACREAGE
 HH: HOUSEHOLDS
 POP: POPULATION
 FLU: FUTURE LAND USE

TABLES 3.3: CALCULATIONS FOR THE CORPORATE LIMITS WEST OF ROCHELLE ROAD

| CATEGORY | AC | HH | POP |
|-------------------|----------|--------|--------|
| BUILT | 5,657.78 | 20,319 | 55,487 |
| VACANT ENTITLED | 1,031.00 | 2,137 | 6,172 |
| VACANT UNENTITLED | 2,173.99 | 4,163 | 11,719 |
| TOTAL | 8,862.77 | 26,619 | 73,378 |

TABLES 3.4: CALCULATIONS FOR THE CORPORATE LIMITS EAST OF ROCHELLE ROAD

| CATEGORY | AC | HH | POP |
|-----------------|--------|-----|-----|
| BUILT | 198.62 | 37 | 111 |
| VACANT ENTITLED | 258.36 | 142 | 425 |
| TOTAL | 456.98 | 179 | 536 |

TABLES 3.5: DEVELOPMENT SCENARIOS FOR THE VACANT UNENTITLED LAND IN THE CORPORATE LIMITS EAST OF ROCHELLE ROAD

| CATEGORY | AC | HH | POP |
|----------------|----------|-------|--------|
| 1.5-ACRE LOTS | 1,765.81 | 1,705 | 5,034 |
| 1.00-ACRE LOTS | 1,765.81 | 2,119 | 6,333 |
| FLU DENSITY | 1,765.81 | 3,432 | 10,329 |

TABLES 3.6: BUILT CALCULATIONS FOR THE ETJ

| CATEGORY | AC | HH | POP |
|------------------|----------|-------|-------|
| WEST ROCHELLE RD | 1,207.00 | 552 | 1,463 |
| EAST ROCHELLE RD | 1,596.00 | 722 | 2,184 |
| TOTAL | 2,803.00 | 1,274 | 3,647 |

TABLES 3.7: DEVELOPMENT SCENARIOS FOR VACANT UNENTITLED LAND IN THE ETJ WEST OF ROCHELLE ROAD

| CATEGORY | AC | HH | POP |
|-------------|--------|-------|-------|
| FLU DENSITY | 651.00 | 1,626 | 4,229 |
| TOTAL | 651.00 | 1,626 | 4,229 |

TABLES 3.8: DEVELOPMENT SCENARIOS FOR THE VACANT UNENTITLED LAND IN THE ETJ EAST OF ROCHELLE ROAD

| CATEGORY | AC | HH | POP |
|----------------|----------|-------|--------|
| 1.5-ACRE LOTS | 1,323.00 | 882 | 2,683 |
| 1.00-ACRE LOTS | 1,323.00 | 1,323 | 4,024 |
| FLU DENSITY | 1,323.00 | 4,543 | 13,817 |

SCENARIOS

Based on the above calculations, the final households and populations for each of the six (6) scenarios is as follows:

TABLES 3.9: BUILDOUT SCENARIOS
KEY: HH: HOUSEHOLDS; POP: POPULATION

| SCENARIO | HH | POP |
|----------|--------|---------|
| 1 | 28,503 | 78,948 |
| 2 | 28,917 | 80,247 |
| 3 | 30,230 | 84,243 |
| 4 | 32,285 | 89,507 |
| 5 | 33,140 | 92,147 |
| 6 | 37,673 | 105,936 |

BUILDOUT YEAR BASED ON SCENARIOS

After the *buildout population* for each of the *Buildout Scenarios* was calculated, the population for 2025 (i.e. 55,487) was projected forward using a three (3) percent Compound Annual Growth Rate (CAGR) [this figure was adopted with the Land Use Assumptions approved by the City Council in 2024 -- see the 2024 Land Use Assumptions for Impact Fees Report] (see Table 3.10).

CONCLUSIONS FOR BUILDOUT ANALYSIS

As previously stated, the impacts of HB347 and SB2038 have made using any calculations that take into account the City's Extraterritorial Jurisdiction (ETJ) difficult to consider. This would exclude Scenarios 4, 5 & 6; however, these were provided to show how these legislative items have affected the *buildout* numbers that were established in previous planning documents. The more realistic scenarios (i.e. Scenarios 1, 2 & 3) show that the City's anticipated *buildout* will be between 2038-2040, and that the City will have

between 28,503-30,230 households and a population between 78,948-84,243 residents.

TABLES 3.10: BUILDOUT YEAR BY SCENARIO

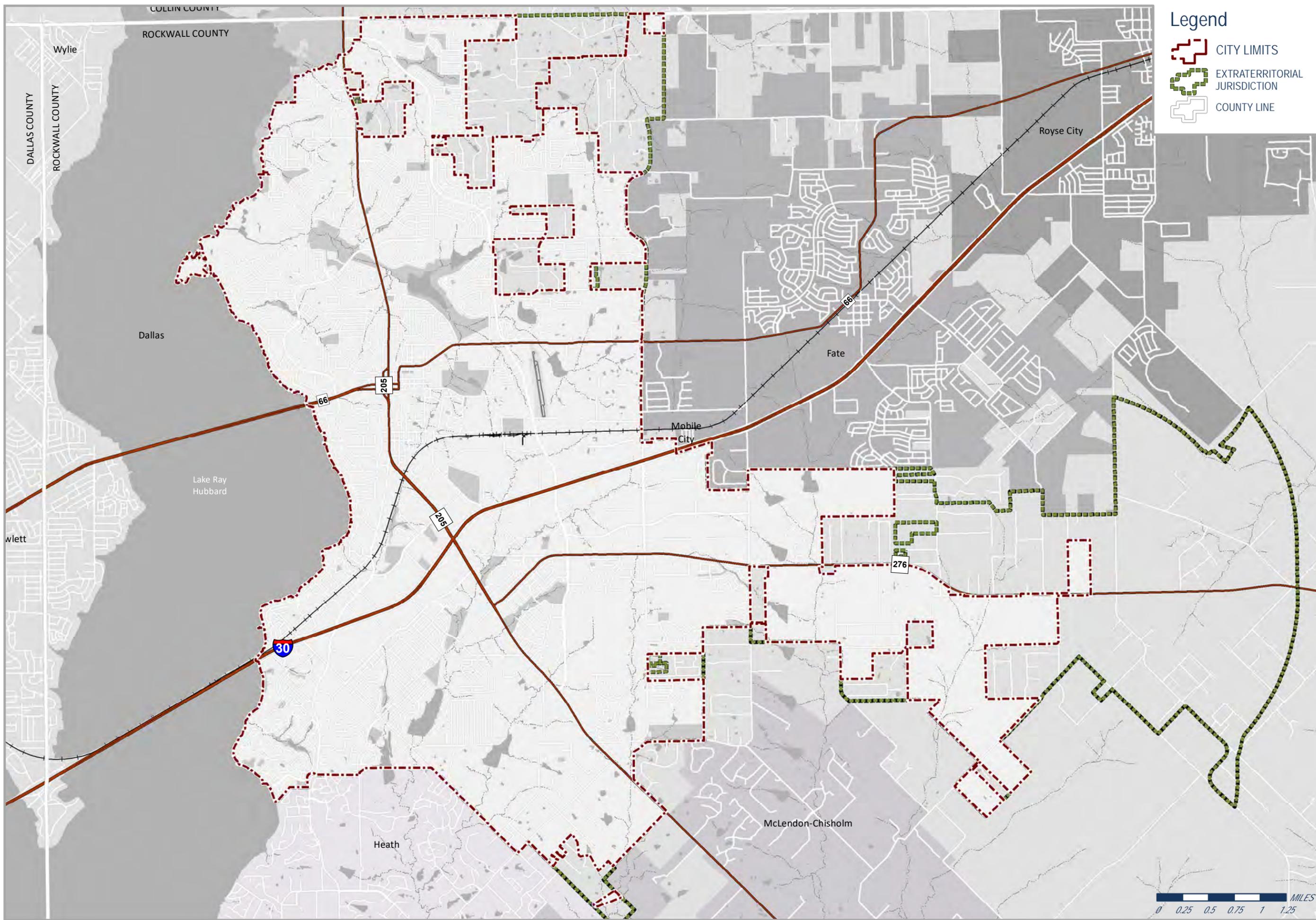
| YEAR | POPULATION | |
|------|------------|---|
| 2025 | 55,487 | |
| 2026 | 57,152 | |
| 2027 | 58,866 | |
| 2028 | 60,632 | |
| 2029 | 62,451 | |
| 2030 | 64,325 | |
| 2031 | 66,254 | |
| 2032 | 68,242 | |
| 2033 | 70,289 | |
| 2034 | 72,398 | |
| 2035 | 74,570 | |
| 2036 | 76,807 | |
| 2037 | 79,111 | |
| 2038 | 81,485 | SCENARIO 1: 79,948 & SCENARIO 2: 80,247 |
| 2039 | 83,929 | |
| 2040 | 86,447 | SCENARIO 3: 84,243 |
| 2041 | 89,040 | |
| 2042 | 91,712 | SCENARIO 4: 89,507 |
| 2043 | 94,463 | SCENARIO 5: 92,147 |
| 2044 | 97,297 | |
| 2045 | 100,216 | |
| 2046 | 103,222 | |
| 2047 | 106,319 | SCENARIO 6: 105,936 |

ESTIMATED BUILDOUT

YEAR: 2038 – 2040
HOUSEHOLDS: 28,503 – 30,230
POPULATION: 78,948 – 84,243

MAP INDEX

- (1) MAP 3.1: MAP OF THE CITY LIMITS AND ETJ
- (2) MAP 3.2: MAP OF CHANGES TO THE ETJ
- (3) MAP 3.3: RAW BUILT/VACANT MAP
- (4) MAP 3.4: VACANT LAND BREAKDOWN
- (5) MAP 3.5: UNENTITLED VACANT LAND BY FUTURE LAND USE MAP
- (6) MAP 3.6: CURRENT LAND USE MAP
- (7) MAP 3.7: ZONING MAP
- (8) MAP 3.8: FUTURE LAND USE MAP
- (9) MAP 3.9: PEOPLE PER HOUSEHOLD BY CENSUS TRACT



Legend

-  CITY LIMITS
-  EXTRATERRITORIAL JURISDICTION
-  COUNTY LINE

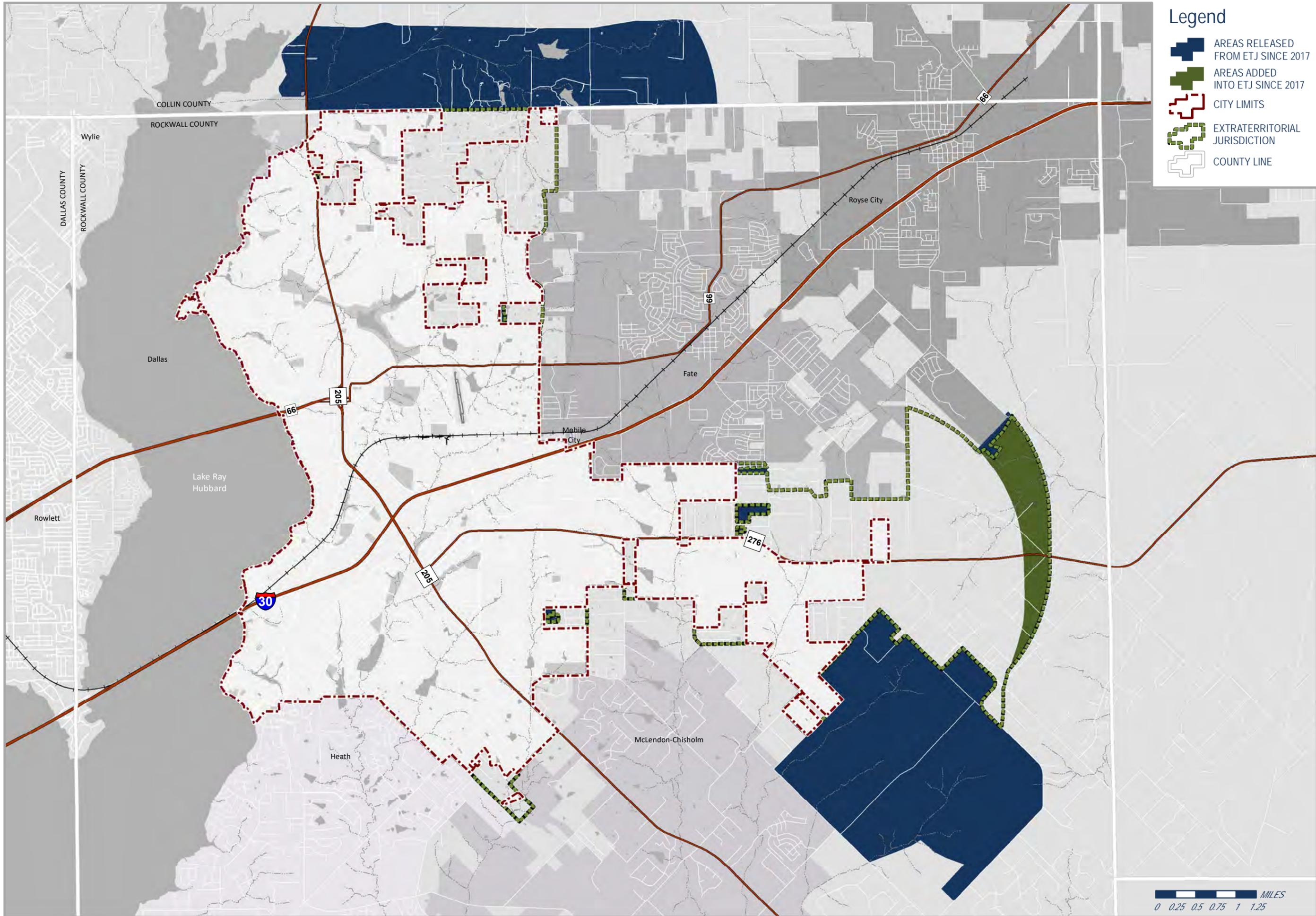


CITY LIMITS AND ETJ

CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE



Page 56 of 382



Legend

- AREAS RELEASED FROM ETJ SINCE 2017
- AREAS ADDED INTO ETJ SINCE 2017
- CITY LIMITS
- EXTRATERRITORIAL JURISDICTION
- COUNTY LINE

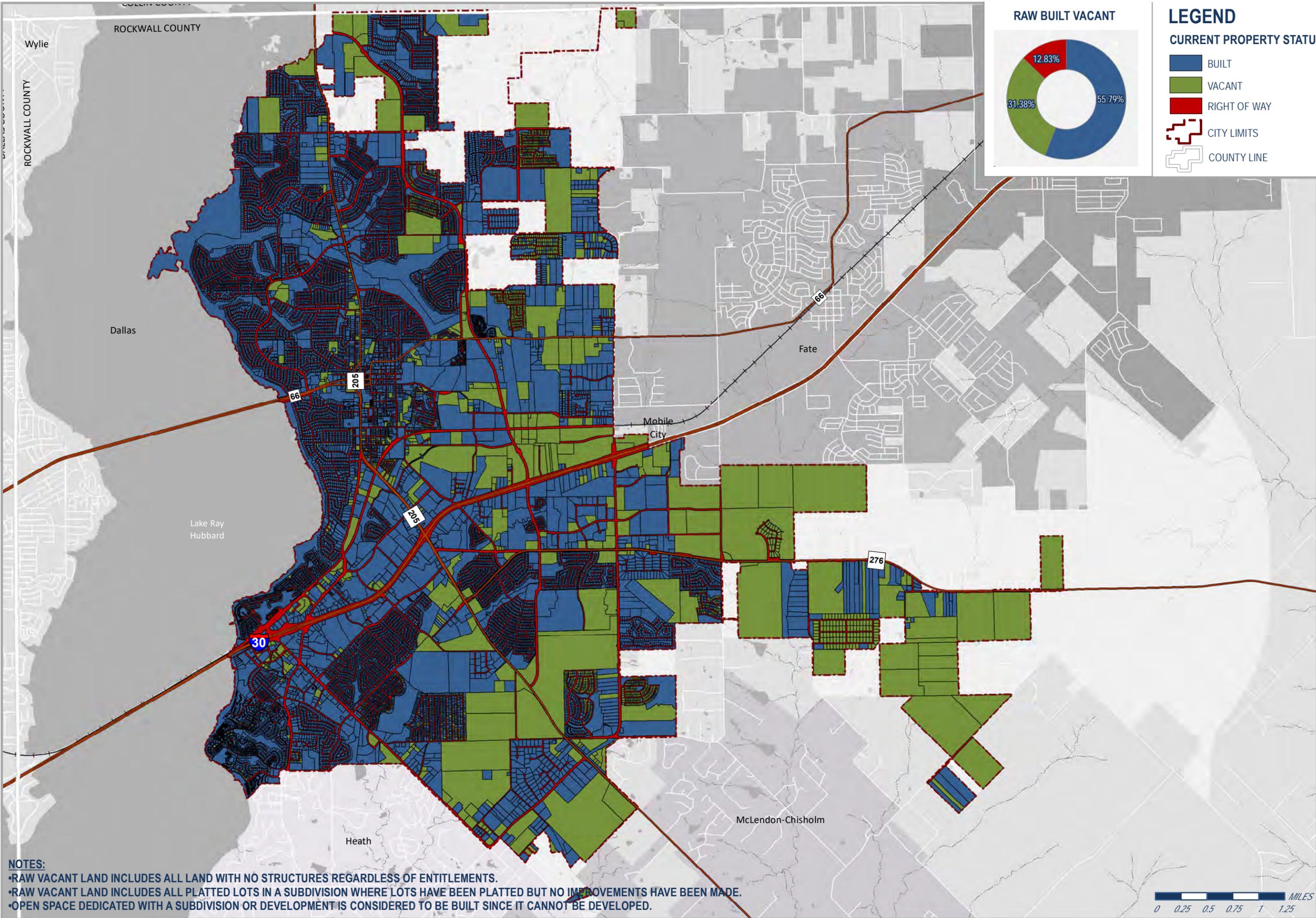


CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE

CHANGES TO THE EXTRA-TERRITORIAL JURISDICTION (ETJ)

3.2





RAW BUILT / VACANT

MAP



VACANT LAND BREAKDOWN

CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE

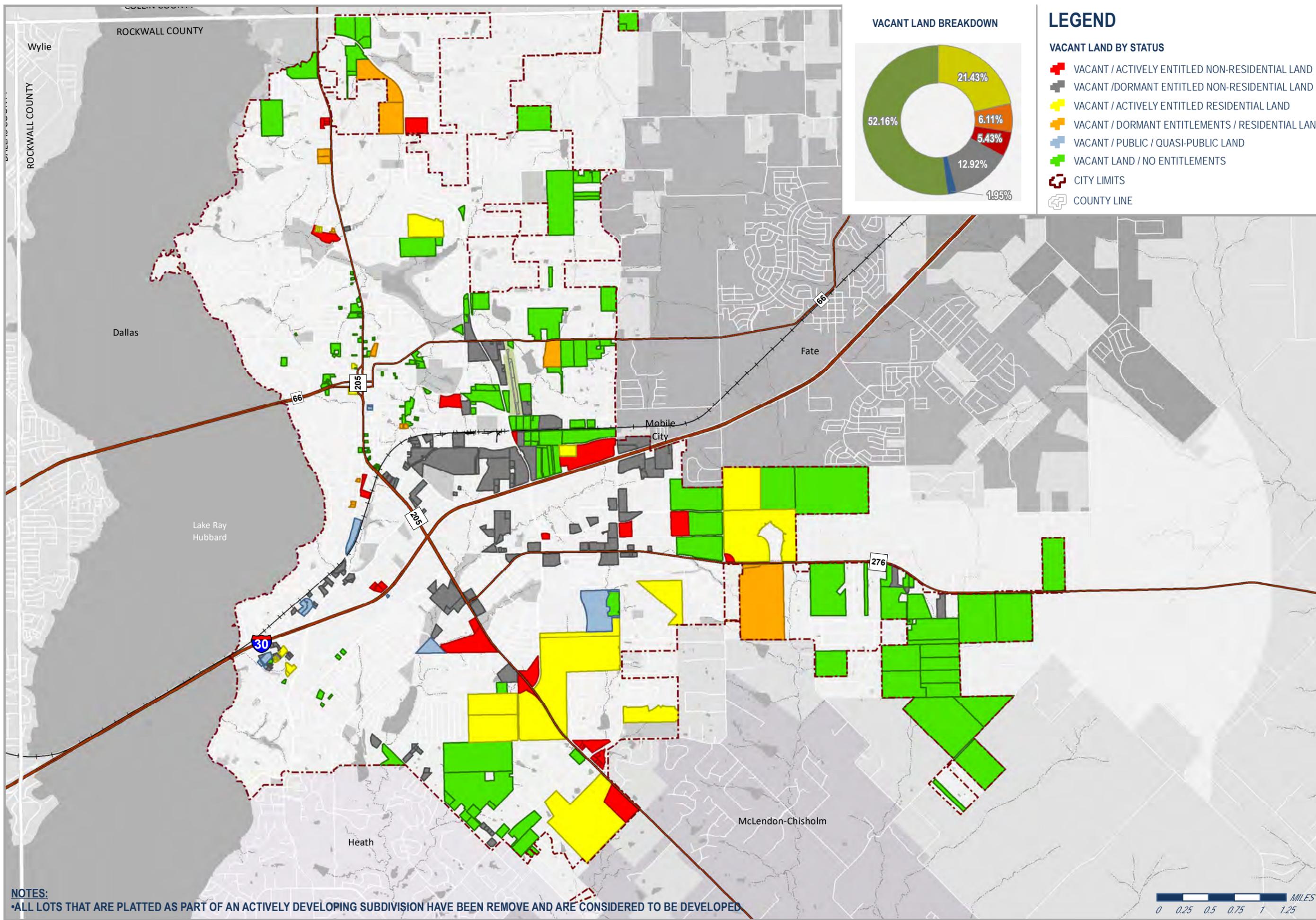
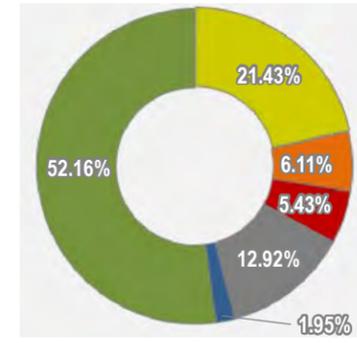


LEGEND

VACANT LAND BY STATUS

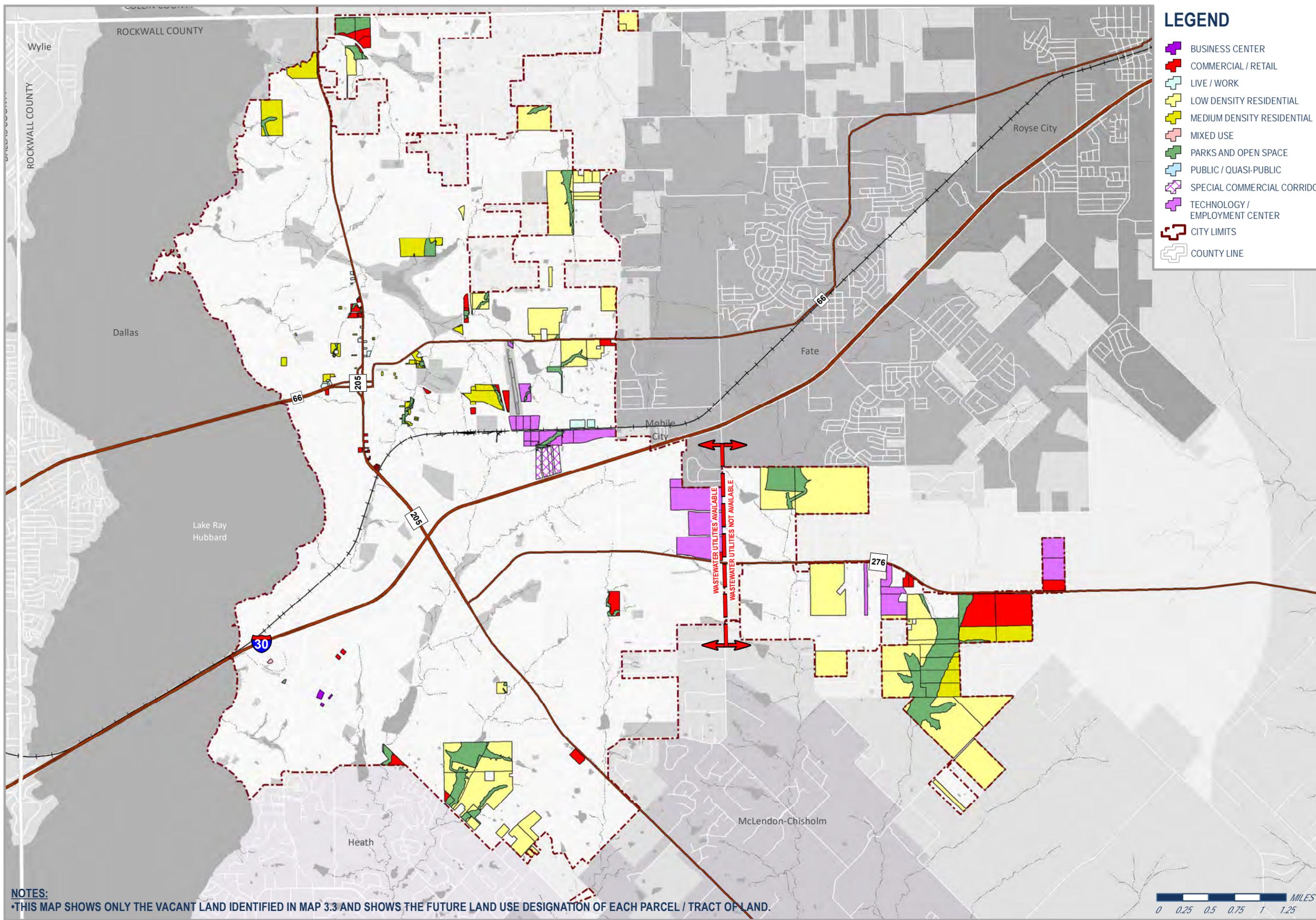
- VACANT / ACTIVELY ENTITLED NON-RESIDENTIAL LAND
- VACANT / DORMANT ENTITLED NON-RESIDENTIAL LAND
- VACANT / ACTIVELY ENTITLED RESIDENTIAL LAND
- VACANT / DORMANT ENTITLEMENTS / RESIDENTIAL LAND
- VACANT / PUBLIC / QUASI-PUBLIC LAND
- VACANT LAND / NO ENTITLEMENTS
- CITY LIMITS
- COUNTY LINE

VACANT LAND BREAKDOWN



NOTES:
 • ALL LOTS THAT ARE PLATTED AS PART OF AN ACTIVELY DEVELOPING SUBDIVISION HAVE BEEN REMOVE AND ARE CONSIDERED TO BE DEVELOPED.





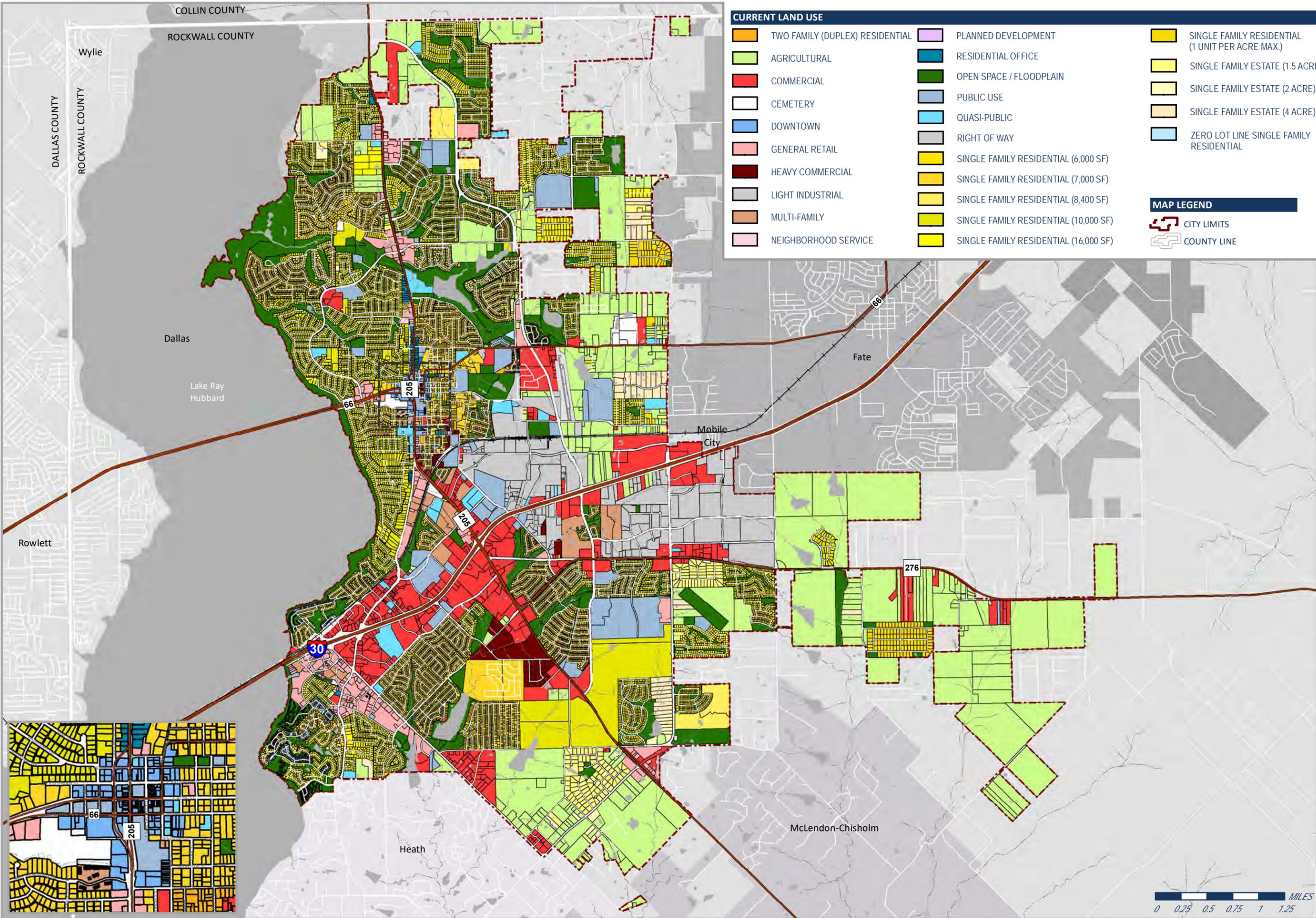
- LEGEND**
- BUSINESS CENTER
 - COMMERCIAL / RETAIL
 - LIVE / WORK
 - LOW DENSITY RESIDENTIAL
 - MEDIUM DENSITY RESIDENTIAL
 - MIXED USE
 - PARKS AND OPEN SPACE
 - PUBLIC / QUASI-PUBLIC
 - SPECIAL COMMERCIAL CORRIDOR
 - TECHNOLOGY / EMPLOYMENT CENTER
 - CITY LIMITS
 - COUNTY LINE

NOTES:
 •THIS MAP SHOWS ONLY THE VACANT LAND IDENTIFIED IN MAP 3.3 AND SHOWS THE FUTURE LAND USE DESIGNATION OF EACH PARCEL / TRACT OF LAND.



3.5 UNENTITLED VACANT LAND BY FUTURE LAND USE

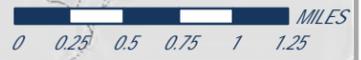
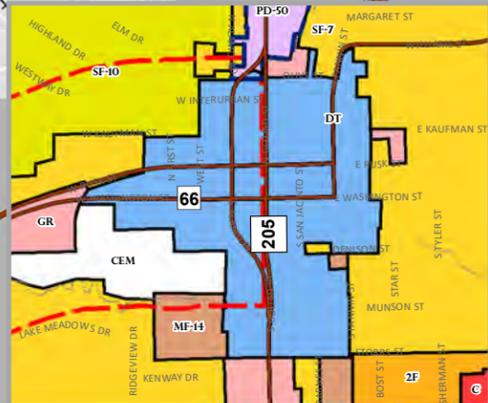
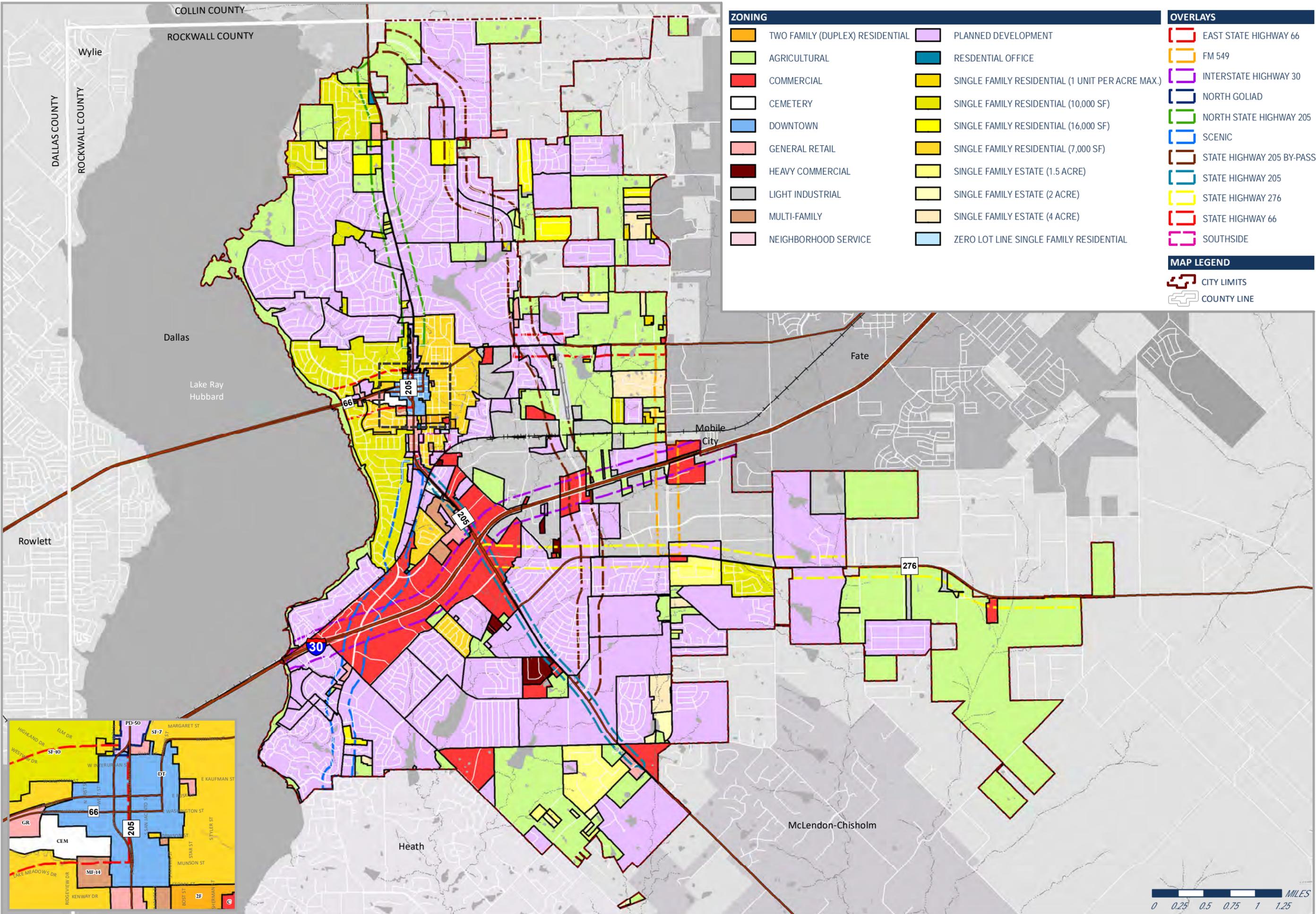
CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE

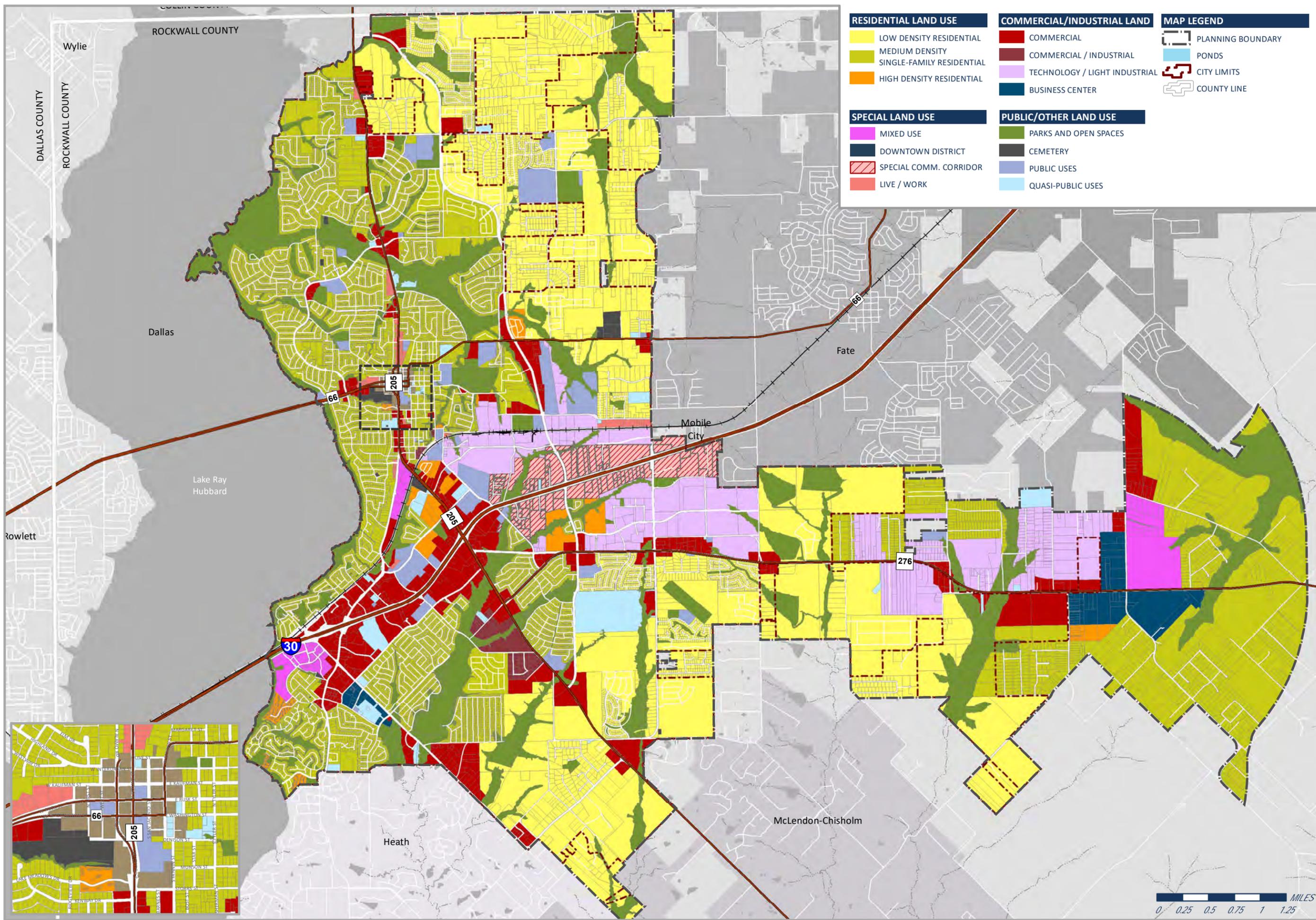


CURRENT LAND USE MAP 3.6

CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE







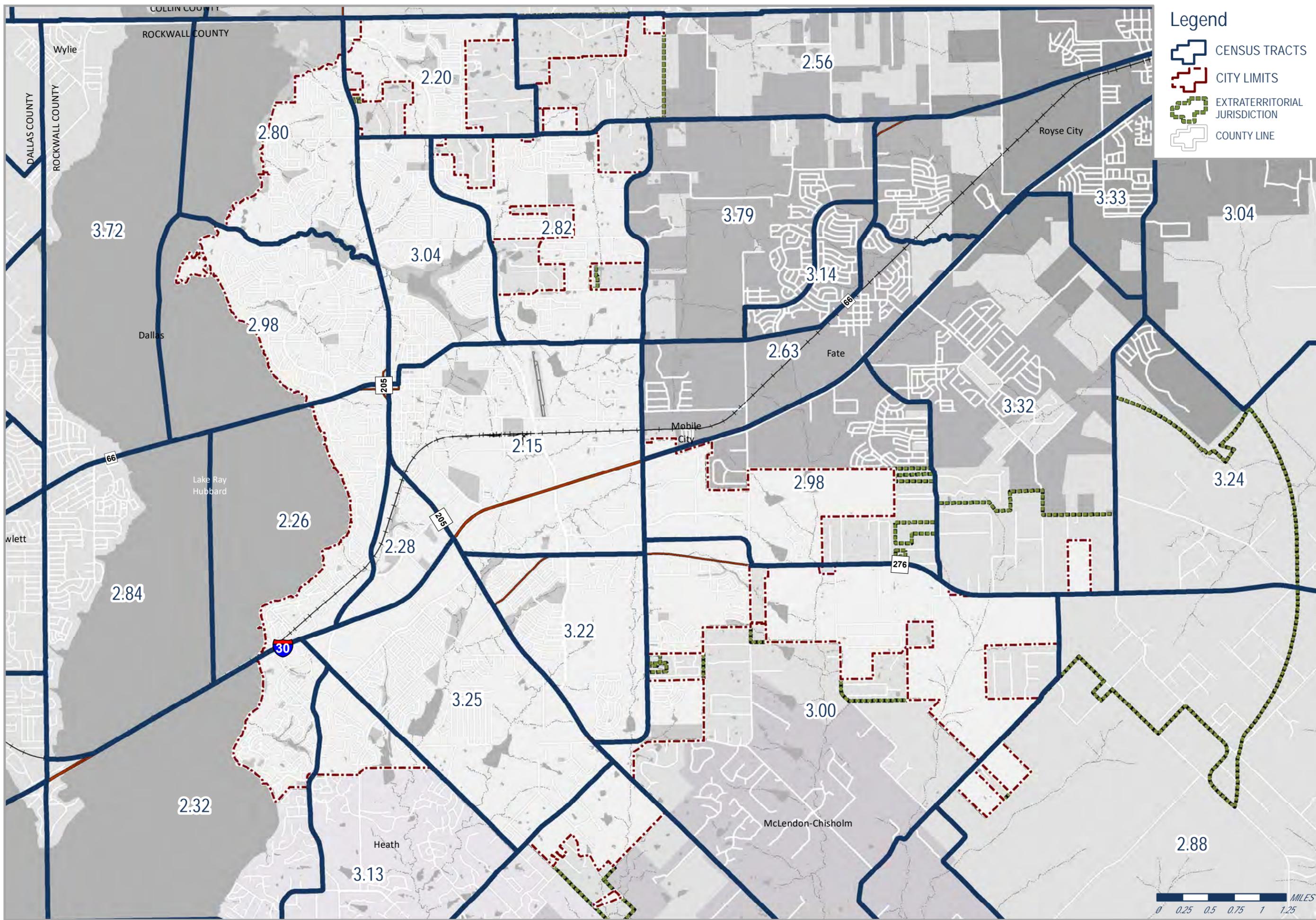
| RESIDENTIAL LAND USE | COMMERCIAL/INDUSTRIAL LAND | MAP LEGEND |
|--|-------------------------------|-------------------|
| LOW DENSITY RESIDENTIAL | COMMERCIAL | PLANNING BOUNDARY |
| MEDIUM DENSITY SINGLE-FAMILY RESIDENTIAL | COMMERCIAL / INDUSTRIAL | PONDS |
| HIGH DENSITY RESIDENTIAL | TECHNOLOGY / LIGHT INDUSTRIAL | CITY LIMITS |
| | BUSINESS CENTER | COUNTY LINE |
| SPECIAL LAND USE | PUBLIC/OTHER LAND USE | |
| MIXED USE | PARKS AND OPEN SPACES | |
| DOWNTOWN DISTRICT | CEMETERY | |
| SPECIAL COMM. CORRIDOR | PUBLIC USES | |
| LIVE / WORK | QUASI-PUBLIC USES | |

FUTURE LAND USE MAP

CH 3: CURRENT LAND USE, ZONING, AND FUTURE LAND USE

3.8





- Legend**
- CENSUS TRACTS
 - CITY LIMITS
 - EXTRATERRITORIAL JURISDICTION
 - COUNTY LINE

2020 - CENSUS TRACT
PEOPLE PER HOUSEHOLD

3.9

CH 3: CURRENT
LAND USE, ZONING,
AND FUTURE LAND USE



4 HOUSING AND NEIGHBORHOODS





TERRACINA SUBDIVISION



SOMERSET PARK, PHASE 1 SUBDIVISION



THE TERRACES SUBDIVISION



NELSON LAKE SUBDIVISION



HOMESTEAD SUBDIVISION



GIDEON GROVE SUBDIVISION



WINDING CREEK SUBDIVISION



NORTH GATE SUBDIVISION



SADDLE STAR, PHASE 2 SUBDIVISION

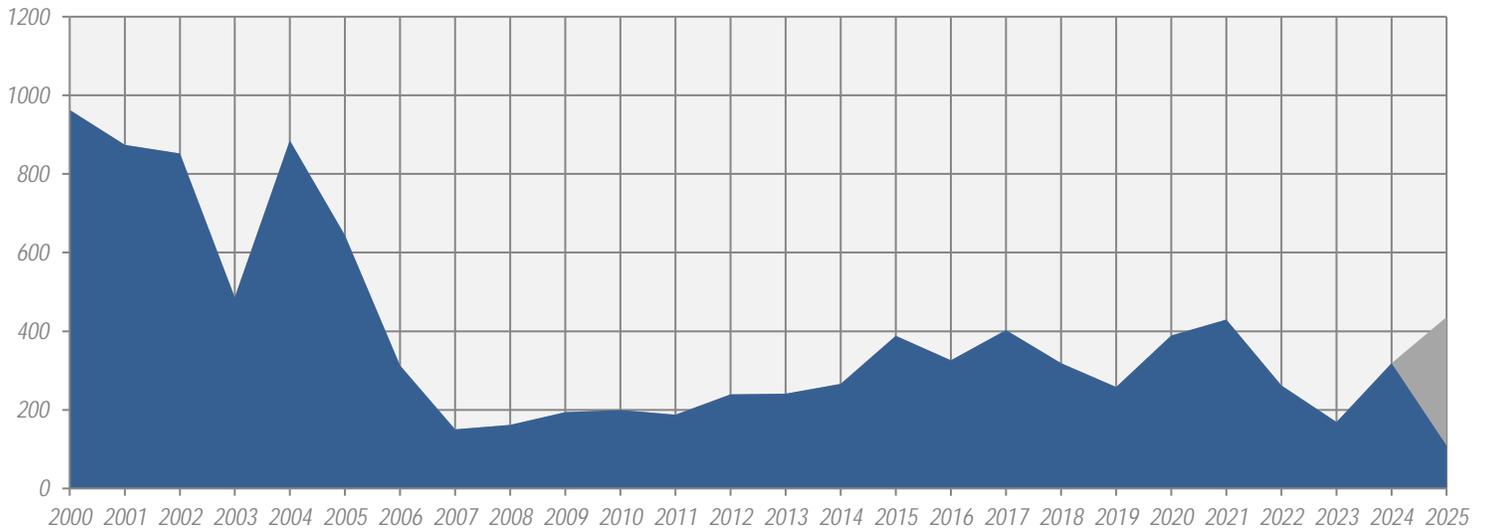


QUAIL HOLLOW SUBDIVISION

FIGURE 4.1: NEW SINGLE-FAMILY HOME PERMITS ISSUED BETWEEN 2000 – 2025

NOTE: 2025 IS THROUGH MARCH.

SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DEPARTMENT



HOUSING

Housing in the City of Rockwall is primarily based on population growth driven by the establishment of new subdivisions; however, some of the growth is caused by internal migration of existing residents. Due to the topography and the unique development constraints of the City, residential development has been slower to develop than the majority of comparable cities in the North Texas region. This has allowed the City of Rockwall to demand high quality housing options that lead to well-designed, value sustaining neighborhoods. Moving forward, the City needs to continue to balance housing demand with the ability to provide housing options that will maintain the community's housing value and desirability.

HOUSING COMPOSITION

As of January 1, 2025, the City of Rockwall had 23,013 residential housing units within its corporate boundaries. This represents an increase of 4,716 residential housing units or an increase of 25.77% since January 1, 2017. Of the 23,013 units, 17,969 units were single-family homes, which represents 78.08% of the City's total housing stock. Since 2017, this represents an increase of 2,867 additional single-family housing units or an increase of 18.98% in the City's single-family housing stock.

Looking at the City's multi-family units -- which include apartments, townhomes, and condominiums --, as of January 1, 2025 there were a total of 4,007 units. This represents 17.41% of all residential housing units, and an increase of 1,742 units or an increase of

76.91% since January 1, 2017. In addition, there are nine (9) assisted living facilities with a total of 937 beds that make up 4.07% of all housing units in the City. This is an increase of five (5) assisted living facilities adding 107 beds since January 1, 2017. There are also 100 government housing units in the City totaling 0.43% of all housing units. This has not changed since January 1, 2017. Figure 4.2 shows the change in the City's housing composition from January 1, 2017 to January 1, 2025.

HOUSING DISTRIBUTION

Looking at the distribution of households within the City of Rockwall, approximately 12,928 households or 56.18% of all current households have been constructed north of IH-30, and 10,085 households or 43.82% of all current households have been constructed south of IH-30 (see Figure 4.3). Despite the current distribution, the number of households projected south of IH-30 is expected to be far greater than the number of households north of IH-30 at buildout.

SINGLE-FAMILY

Single-family homes are the basic building block of the City of Rockwall representing 78.08% of all housing units within the City and 29.48% of all current land uses in the City.

EXISTING SINGLE-FAMILY HOUSING STOCK

The majority of the City's 17,696 single-family homes that make up the City's housing stock were constructed within the last 35-years (i.e. 1990-2025). This represents

FIGURE 4.2: HOUSING COMPOSITION, 2017-2025

KEY (CHART 1: 2017): YELLOW = SINGLE FAMILY (15,102 UNITS OR 82.54%); ORANGE = MULTI-FAMILY (2,265 UNITS OR 12.38%); GREEN = ASSISTED LIVING (830 UNITS OR 4.54%); RED = GOVERNMENT HOUSING (100 UNITS OR 0.55%)



KEY (CHART 2: 2025): YELLOW = SINGLE FAMILY (17,969 UNITS OR 78.08%); ORANGE = MULTI-FAMILY (4,007 UNITS OR 17.41%); GREEN = ASSISTED LIVING (937 UNITS OR 4.07%); RED = GOVERNMENT HOUSING (100 UNITS OR 0.43%)

SOURCE: CITY OF ROCKWALL GIS DIVISION

68.31% or 12,274 homes. Of this, approximately 9,613 homes or 53.50% were constructed between 2000-2025 (see Table 4.1). The highest period of growth in any ten (10) year period for single-family housing was between 2000-2009. During this time period, the city added 5,650 homes, which represents 31.44% of the City's housing stock. Currently, about 6.67% or 1,199 homes in the City were constructed within the last five (5) years. Only about 19.37% or 3,480 homes were constructed prior to 1990.

TABLE 4.1: SINGLE-FAMILY HOUSES BY DECADE

| DECADE | LOT COUNT | PERCENTAGE |
|--------------|-----------|------------|
| UNKNOWN DATE | 2,215 | 12.33% |
| PRE-1900 | 16 | 0.09% |
| 1900-1909 | 8 | 0.04% |
| 1910-1919 | 16 | 0.09% |
| 1920-1929 | 20 | 0.11% |
| 1930-1939 | 8 | 0.04% |
| 1940-1949 | 43 | 0.24% |
| 1950-1959 | 63 | 0.35% |
| 1960-1969 | 106 | 0.59% |
| 1970-1979 | 639 | 3.56% |
| 1980-1989 | 2,561 | 14.25% |
| 1990-1999 | 2,661 | 14.81% |
| 2000-2009 | 5,650 | 31.44% |
| 2010-2019 | 2,764 | 15.38% |
| 2020-2025 | 1,199 | 6.67% |
| TOTAL | 17,969 | 100.00% |

SOURCE: ROCKWALL CENTRAL APPRAISAL DISTRICT (RCAD)

Looking at the average assessed value of homes -- based on Rockwall Central Appraisal District's (RCAD's) Market Values for 2016 & 2024 --, houses in the City of Rockwall have seen large increases in their values ranging between 57.41% and 162.26% over the last nine (9) years. Homes that were constructed between 2010-2019 have the highest current assessed value at an average of \$598,976.00, which is an increase of 96.65% from the values reported for homes constructed between 2010-2016 back in 2017. This would equate to an estimated \$252.95 per average square foot, which is an increase of \$151.15 per average square foot over the value reported in 2017. Homes constructed between 2020-2025 have a slightly lower value of \$520,135.00; however, homes constructed during this time period are slightly larger than homes constructed during 2010-2020 by an average of 222 SF. Homes constructed between 2000-2009 had the largest average square footage at 2,590 SF (see Table 4.2).

TABLE 4.2: HOUSING VALUE AND SIZE

| DECADE | AVERAGE LIVING AREA | HOUSEHOLD VALUES | | |
|-------------|---------------------|------------------|--------------|----------|
| | | 2017 | 2025 | % CHANGE |
| PRE-1950 | 2,131 | \$158,301.00 | \$298,809.00 | 88.76% |
| 1950-1959 | 1,306 | \$102,738.00 | \$161,720.00 | 57.41% |
| 1960-1969 | 1,438 | \$101,903.00 | \$267,251.00 | 162.26% |
| 1970-1979 | 1,683 | \$127,746.00 | \$312,300.00 | 144.47% |
| 1980-1989 | 2,095 | \$187,955.00 | \$400,907.00 | 113.30% |
| 1990-1999 | 2,485 | \$247,902.00 | \$484,847.00 | 95.58% |
| 2000-2009 | 2,590 | \$241,367.00 | \$453,272.00 | 87.79% |
| 2010-2019 * | 2,368 | \$304,596.00 | \$598,976.00 | 96.65% |
| 2020-2025 | 2,524 | N/A | \$520,135.00 | N/A |

NOTES:

* THE 2017 HOUSEHOLD VALUES END IN 2016.

(1) BASED ON 2016 & 2024 ROCKWALL CENTRAL APPRAISAL DISTRICT (RCAD) VALUES.

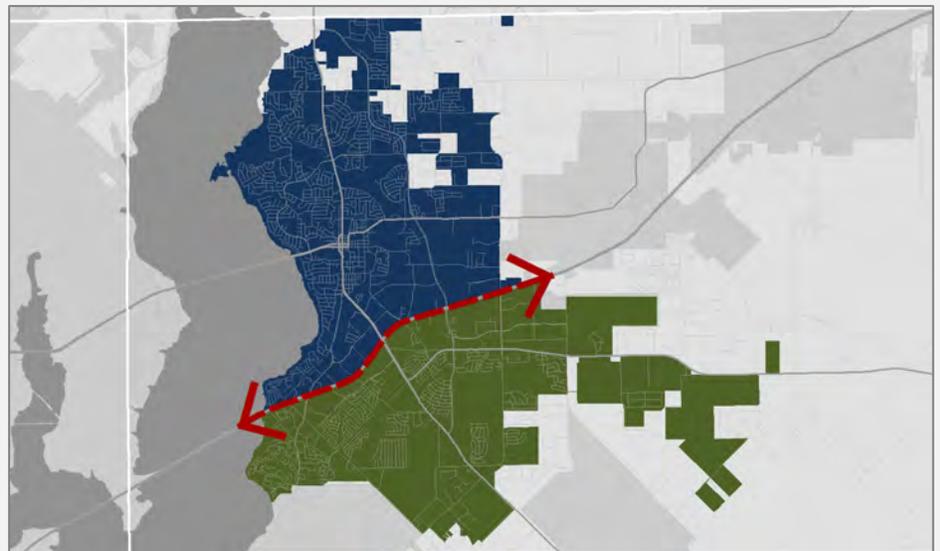
(2) DOES NOT ACCOUNT FOR DEMOS AND/OR ADDITIONS.

(3) AVERAGE LIVING AREA ONLY INCLUDES CONDITIONED SPACE.

SOURCE: CITY OF ROCKWALL GIS DIVISION

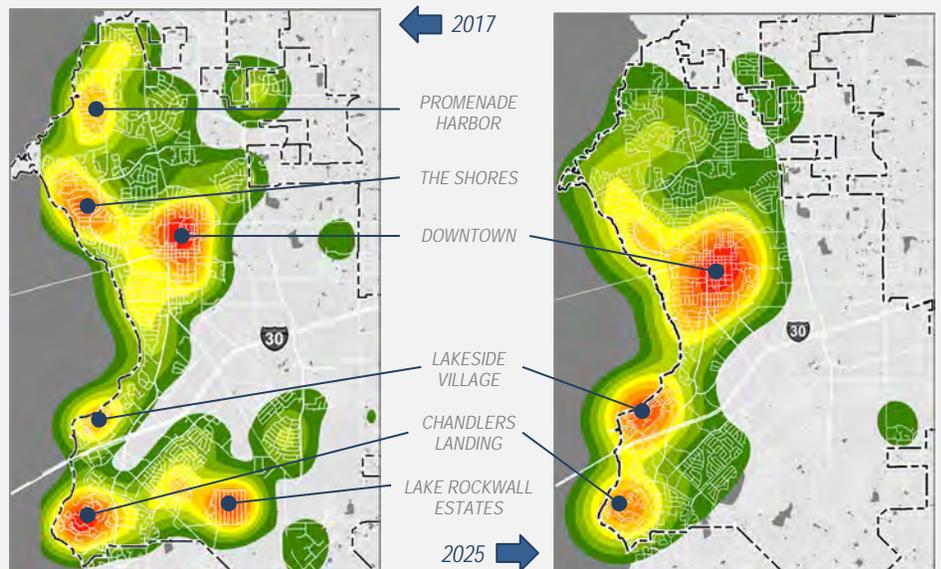
FIGURE 4.3: HOUSEHOLD DISTRIBUTION

KEY: GREEN = HOUSEHOLDS SOUTH OF IH-30 (10,085 or 43.82%); BLUE = HOUSEHOLDS NORTH OF IH-30 (12,928 or 56.18%); RED = IH-30; NOTE: THESE INCLUDE ALL SINGLE-FAMILY, MULTI-FAMILY, ASSISTED LIVING, AND GOVERNMENT UNITS IN THE CITY.



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 4.4: REMODEL PERMITS DISTRIBUTION, 2017-2025



SOURCE: CITY OF ROCKWALL GIS DIVISION

BUILDING PERMITS: NEW CONSTRUCTION AND REMODEL PERMITS

As depicted in Figure 4.1 (on the first page of this section) the number of single-family housing permits dropped significantly in 2006; however, since 2011 the number of single-family permits issued per year has been on an increasing trend (with the exception of 2022 & 2023). While it is not anticipated that new construction permits will reach 2000-2006 levels, the homes currently being constructed have a much higher average value than the homes that were constructed during the 2000-2006 time period (see Table 4.3 below).

TABLE 4.3: CURRENT HOUSING VALUE AND SIZE BY YEAR, 2017-2023

| YEAR | AVERAGE APPRAISAL MARKET VALUE | AVERAGE LIVING AREA |
|------|--------------------------------|---------------------|
| 2017 | \$ 634,523.00 | 2,440 SF |
| 2018 | \$ 629,279.00 | 2,522 SF |
| 2019 | \$ 657,177.00 | 2,605 SF |
| 2020 | \$ 655,531.00 | 2,554 SF |
| 2021 | \$ 710,331.00 | 2,635 SF |
| 2022 | \$ 681,788.00 | 2,399 SF |
| 2023 | \$ 572,325.00 | 2,378 SF |

NOTE: ALL APPRAISAL VALUES HAVE BEEN ADJUSTED TO REFLECT 2023 DOLLARS.

SOURCE: ROCKWALL CENTRAL APPRAISAL DISTRICT (RCAD)

Remodel permits within the City are an indication of reinvestment within the City's housing stock. Figure 4.4 shows the aggregate values of all single-family remodel permits issued in the City for 2017 & 2025. Based on this map, the City has seen large amounts of reinvestment in the Downtown and Old Town Rockwall (OTR) Historic District (i.e. adjacent to/east of the Downtown area) during both years. In addition, in 2017 the City saw large amounts of reinvestment in the eastern side of the Lake Rockwall Estates Subdivision, the Chandler's Landing Subdivision, Lakeside Village Subdivision, the Promenade Harbor Subdivision, and portions of the Shores Subdivision. The map changed in 2025, showing more reinvestment in the Lakeside Village Subdivision and the Chandler's Landing Subdivision. Figure 4.5 depicts the age of the City's single-family housing stock. These two (2) maps show a correlation between age and reinvestment.

MULTI-FAMILY

Multi-Family housing currently makes up 17.41% of the City's housing stock, which represents an increase of 76.91% or 1,742 units from 2017 to 2025. A large majority of this growth has been in the City's Harbor District, which was originally entitled for 1,162 condominium units in 2010; however, many of

these projects were slow to develop and many were not constructed until after 2017. Of the 4,007 multi-family units that currently make up the 17.41% of the City's housing stock, ~24.16% or 968-units were constructed prior to 1986. This means that a large percentage of the City's multi-family units are 30+ years in age. Despite their age, these units have a relatively low vacancy rate and are demanding relatively high rental rates for the region (see Table 4.4). About 1,297 multi-family units or 32.37% were constructed between 1998-2011, and the remaining 1,742 units or 43.47% were constructed between 2017-2025.

TABLE 4.4: RANGE OF RENTS FOR APARTMENTS IN THE CITY OF ROCKWALL

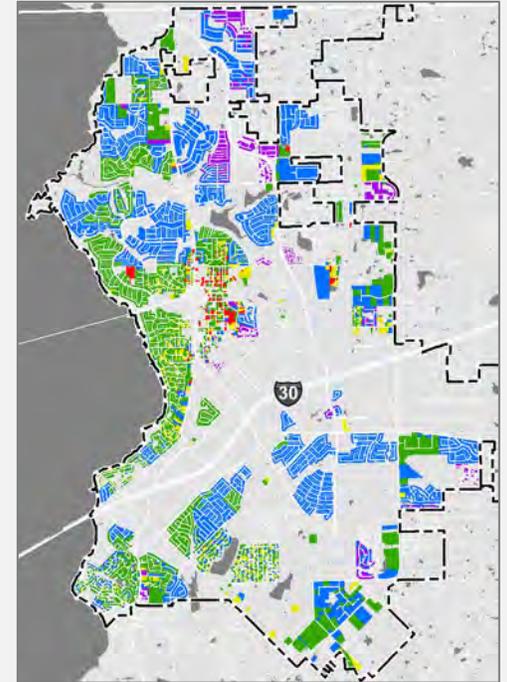
| FACILITY | LOW | HIGH |
|-------------------------------|---------|---------|
| LAKESIDE APARTMENTS | \$1,099 | \$1,345 |
| EASTBANK APARTMENTS | \$1,055 | \$1,805 |
| WILDER APARTMENTS | \$1,020 | \$1,780 |
| MISSION ROCKWALL | \$1,099 | \$1,824 |
| ROCKWALL COMMONS | \$1,100 | \$2,100 |
| EVERGREEN AT ROCKWALL | \$1,004 | \$1,456 |
| SIXTEEN50 AT LAKE RAY HUBBARD | \$1,433 | \$5,127 |
| MARQUIS ROCKWALL | \$1,185 | \$2,395 |
| SONOMA COURT | \$1,421 | \$1,561 |
| ALDERS AT ROCKWALL | \$1,750 | \$2,179 |
| LAKEVIEW APARTMENTS | \$1,315 | \$2,425 |
| LUXIA ROCKWALL DOWNES | \$1,249 | \$2,298 |
| CORBAN DISCOVERY | \$1,422 | \$2,195 |
| FLORENCE AT THE HARBOR | \$1,170 | \$2,256 |
| HARBOR HILL APARTMENTS | \$1,260 | \$3,565 |
| ABLON AT HARBOR VILLAGE | \$1,190 | \$2,115 |

NOTES:

- KEY: BLUE = CONSTRUCTED PRIOR TO 1986
- (1) THESE NUMBERS ARE MEANT TO PROVIDE A GENERAL COMPARISON OF RENT VALUES AND DO NOT CORRESPOND TO THE NUMBER OF BEDROOMS OR SQUARE FOOTAGE OF EACH UNIT (I.E. THE NUMBERS HAVE NOT BEEN NORMALIZED).
- (2) ALDERS AT ROCKWALL, MARQUIS ROCKWALL, AND EVERGREEN AT ROCKWALL ARE AGE RESTRICTED FACILITIES.
- (3) EVERGREEN AT ROCKWALL IS AN INCOME RESTRICTED FACILITY.
- (4) ALL RENTS WERE OBTAINED FROM ONLINE RESOURCES.

FIGURE 4.5: AGE OF HOUSING STOCK

KEY: RED = 1800 - 1959; YELLOW = 1960 - 1979; GREEN = 1980 - 1999; BLUE = 2000 - 2019; PURPLE = 2020 - 2024.



SOURCE: CITY OF ROCKWALL GIS DIVISION

TABLE 4.5: MULTI-FAMILY PROPERTIES

| FACILITY | UNITS | YEAR |
|------------------------------------|-------|------|
| LAKESIDE APARTMENTS | 101 | 1972 |
| CUTTER HILL CONDOMINIUMS | 62 | 1979 |
| EASTBANK APARTMENTS | 164 | 1980 |
| WILDER APARTMENTS | 164 | 1982 |
| SPYGLASS HILL CONDOMINIUMS | 155 | 1982 |
| SIGNAL RIDGE CONDOMINIUMS | 302 | 1983 |
| MATCHPOINTE TOWNHOMES | 5 | 1985 |
| THE CABANAS | 15 | 1985 |
| MISSION ROCKWALL | 224 | 1998 |
| ROCKWALL COMMONS | 202 | 2005 |
| WATER'S EDGE (VILLAS DE PORTOFINO) | 72 | 2006 |
| EVERGREEN AT ROCKWALL | 141 | 2007 |
| MARQUIS ROCKWALL | 200 | 2009 |
| SIXTEEN 50 AT LAKE RAY HUBBARD | 334 | 2009 |
| SONOMA COURT APARTMENTS | 124 | 2011 |
| LAKEVIEW APARTMENTS | 140 | 2013 |
| FLORENCE AT THE HARBOR | 228 | 2016 |
| LUXIA ROCKWALL DOWNES | 295 | 2017 |
| ALDERS AT ROCKWALL | 144 | 2018 |
| HARBOR HILL | 265 | 2018 |
| ABLON AT HARBOR VILLAGE | 375 | 2018 |
| CORBAN DISCOVERY | 295 | 2020 |
| TOTAL: | 4,007 | |

SOURCE: CITY OF ROCKWALL GIS/PLANNING DIVISION

Of the 4,007 multi-family units in the City, 611 are owner occupied (i.e. *townhome or condominium*), 141 are income and age restricted (or *subsidized*), 344 are age restricted and market rate, and 2,911 are market rate apartments.

ASSISTED LIVING

The City currently has 937 assisted living units/beds (i.e. 4.07%). This housing type has seen a dramatic increase since 2010 with over 73.75% (i.e. 691 units) of all units being added to the City between 2011-2025 (see Table 4.6). This increase has been an emerging trend not just in the City of Rockwall, but also across the country.

TABLE 4.6: ASSISTED LIVING FACILITIES

| FACILITY | UNITS | YEAR |
|---|-------|------|
| ROCKWALL NURSING CENTER | 164 | 1984 |
| SUMMER RIDGE ASSISTED LIVING | 82 | 1998 |
| COLONIAL OAKS AT ROCKWALL | 46 | 2011 |
| ARBOR HOUSE OF ROCKWALL | 75 | 2012 |
| LIBERTY HEIGHTS SENIOR LIVING | 118 | 2013 |
| ROCKRIDGE SENIOR CARE | 85 | 2013 |
| HIGHLAND MEADOWS HEALTH AND REHAB/OAC SENIOR LIVING | 120 | 2014 |
| BROADMOOR MEDICAL RESORT | 140 | 2012 |
| THE HERITAGE HOUSE | 6 | 2022 |
| VILLAGE GREEN ALZHEIMER'S CARE HOME | 28 | 2018 |
| BROOKDALE SUMMER RIDGE | 140 | 1998 |
| COLONIAL OAKS AT ROCKWALL | 50 | 2011 |
| LAKESHORE ASSISTED LIVING AND MEMORY CARE | 86 | 2009 |
| TOTAL: | 937 | |

GOVERNMENT HOUSING

There are currently 100 government-housing units within the City of Rockwall making up 0.43% of all housing in the City. The number of government-housing units in the City has not changed since 2017. Of these 100-units, the North East Texas Community Development Corporation currently owns and operates 36-units in a housing complex (i.e. *the Meadows*) that provides housing to low-income families and recipients of Section 8 housing assistance. In addition, the Rockwall Housing Authority is responsible for the provision of 64 affordable housing units within the City.

HOUSING OCCUPANCY/VACANCY

Looking at the City's occupancy rates, the percent of owner-occupied housing units

according to the 2000 US Census was 76.50% (i.e. 5,055 houses) of the estimated 6,605 homes in the City limits pre-2000. This number remained relatively unchanged when looking at the 2010 US Census, which reported an estimated 13,212 homes in the City with an owner-occupied rate of 76.90% (i.e. 10,165 houses). The 2019-2023 American Community Survey reported a slight decrease in the owner-occupied housing units showing a percentage of 74.70%. Based on the current reported 17,696, this would equate to ~13,219 homes being owner occupied and ~4,477 homes being renter occupied.

Like occupancy, the vacancy rates remained fairly consistent between 2000-2010, showing a slightly declining trend for homeowner vacancies. The 2000 US Census reported a homeowner vacancy rate of 3.90% and a rental vacancy rate of 7.20%, and the 2010 US Census reported a homeowner vacancy rate of 2.10% and a rental vacancy rate of 6.80%. The 2019-2023 American Community Survey showed that these numbers have declined to a homeowner vacancy rate of 1.10% and a rental vacancy rate of 2.70%.

FUTURE HOUSING DEVELOPMENT

As of March 31, 2025, there were ~1,140 vacant, platted single-family lots within 27 active subdivisions across the City (see Table 4.7). Based on the City's current permitting trends over the last ten (10) years this represents an estimated 3½-year lot inventory. In addition, the City has an estimated 2,216 single-family lots that are entitled (but not platted) through various Planned Development Districts. Table 4.8 shows the estimated 2,216 single-family lots broken down by lot size. According to this table, the City is expecting 460, 7,700 SF lots, which equates to 20.76% of all entitled lots. The next highest entitled lots are the 7,200 SF lots, which are at 456 lots or 20.58% of the future housing stock. Of all the anticipated future lots, 31.59% or 700 lots are expected to be lots greater than 10,000 SF.

Looking at future multi-family units, half of the anticipated growth is in the City's IH-30 corridor with 485-units being approved with Planned Development District 102 (PD-102) [i.e. *Rockwall Heights*]. The remaining units are divided between the City's Harbor District -- which has 225-units remaining --, and the Downtown (i.e. *the Bailey* which will consist

TABLE 4.7: ACTIVE SUBDIVISIONS BUILT/VACANT

| SUBDIVISION | YEAR | BUILT | VACANT |
|------------------------------|------|-------|--------|
| ARKOMA | 2022 | 0 | 3 |
| BREEZY HILL LANE | 2020 | 0 | 3 |
| BREEZY HILL, PHASE 11 | 2020 | 80 | 1 |
| BREEZY HILL, PHASE 6 | 2018 | 79 | 1 |
| DISCOVERY LAKES, PHASE 1 | 2023 | 4 | 32 |
| EMERSON FARMS | 2023 | 5 | 103 |
| GIDEON GROVE, PHASE 2 | 2022 | 80 | 16 |
| HOMESTEAD, PHASE 1 | 2024 | 42 | 144 |
| KENNEDY | 2021 | 1 | 1 |
| LEE ACRES | 2024 | 2 | 2 |
| MARINA VILLAGE | 2023 | 1 | 38 |
| NELSON LAKE ESTATES | 2023 | 74 | 194 |
| NORTHGATE | 2020 | 31 | 9 |
| PARK PLACE, PHASE 4 | 2022 | 0 | 5 |
| PARK PLACE WEST, PHASE 3 | 2018 | 65 | 21 |
| PEOPLES TRACT | 2024 | 4 | 4 |
| QUAIL HOLLOW, PHASE 1 | 2024 | 45 | 105 |
| SADDLE STAR ESTATES, PHASE 1 | 2019 | 63 | 6 |
| SADDLE STAR ESTATES, PHASE 2 | 2023 | 49 | 31 |
| SOMERSET PARK, PHASE 2 | 2022 | 72 | 99 |
| SOMERSET PARK, PHASE 1 | 2016 | 151 | 1 |
| TERRACES, PHASE 1 | 2024 | 44 | 143 |
| TERRACINA, PHASE 1 | 2020 | 74 | 39 |
| THE HIGHLANDS | 2018 | 36 | 4 |
| THE STANDARD | 2018 | 26 | 26 |
| WHISPER ROCK | 2018 | 22 | 8 |
| WINDING CREEK | 2024 | 37 | 101 |
| TOTAL: | | 1,087 | 1,140 |

SOURCE: CITY OF ROCKWALL GIS/PLANNING DIVISION

TABLE 4.8: ENTITLED SINGLE FAMILY LOTS BY LOT SIZE

| LOT SIZE | # OF LOTS | % OF TOTAL |
|-----------|-----------|------------|
| 6,600 SF | 145 | 6.54% |
| 7,200 SF | 456 | 20.58% |
| 7,700 SF | 460 | 20.76% |
| 8,400 SF | 163 | 7.36% |
| 8,750 SF | 249 | 11.24% |
| 9,600 SF | 43 | 1.94% |
| 10,000 SF | 271 | 12.23% |
| 10,400 SF | 66 | 2.98% |
| 12,000 SF | 168 | 7.58% |
| 20,000 SF | 62 | 2.80% |
| 32,670 SF | 59 | 2.66% |
| 43,560 SF | 62 | 2.80% |
| 65,340 SF | 12 | 0.54% |
| TOTAL: | 2,216 | 100.00% |

SOURCE: CITY OF ROCKWALL GIS/PLANNING DIVISION

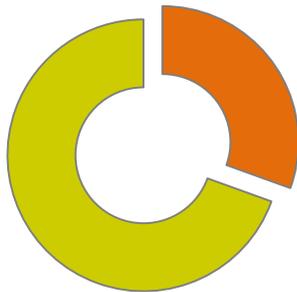
of 263-units). Table 4.9 shows a list of all 963-units of remaining multi-family entitlements in the City.

TABLE 4.9: ENTITLED MULTI-FAMILY UNITS

| PROJECT | # OF UNITS |
|---------------------|------------|
| THE BAILEY | 263 |
| HARBOR RESIDENCES | 176 |
| HARBOR CONDOMINIUMS | 42 |
| ROCKWALL HEIGHTS | 485 |
| HARBOR TOWNHOMES | 7 |
| TOTAL: | 973 |

FIGURE 4.6: SINGLE-FAMILY TO MULTI-FAMILY ENTITLED RATIO

KEY: **YELLOW** = SINGLE FAMILY (77.99%); **ORANGE** = MULTI-FAMILY (18.21%);



If all the City's entitlements were constructed and no additional entitlements were granted the City's housing mix would be 77.99% single-family, 18.21% multi-family, 3.43% assisted living, and 0.37% government housing (see Figure 4.7).

FIGURE 4.7: PROJECTED HOUSING MIX

KEY: **YELLOW** = SINGLE FAMILY (77.99%); **ORANGE** = MULTI-FAMILY (18.21%); **GREEN** = ASSISTED LIVING (3.43%); **RED** = GOVERNMENT HOUSING (0.37%)

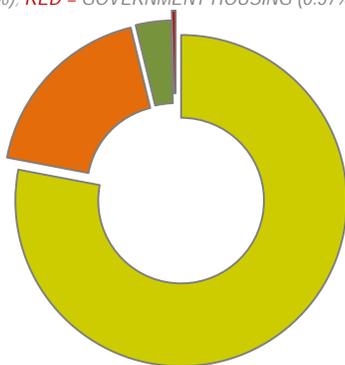


TABLE INDEX

- (1) TABLE 4.10: SUBDIVISION TABLE

MAP INDEX

- (1) MAP 4.1: SUBDIVISION MAP
- (2) MAP 4.2: MAP OF RESIDENTIAL LAND USES

TABLE 4.10: RESIDENTIAL SUBDIVISION LIST AS OF APRIL 2, 2025

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---|------|------|-------|--------|-------|
| 3358 | CHANDLERS LANDING PH 17 | MFR | 1985 | 98 | 0 | 98 |
| 3581 | EASTSHORE ADDITION | MFR | 2006 | 3 | 0 | 3 |
| 3883 | HARBOR HILLS ADDITION | MFR | 2020 | 1 | 2 | 3 |
| 3815 | HARBOR VILLAGE ADDITION | MFR | 2017 | 2 | 1 | 3 |
| 4372 | MANSIONS FAMILY ADDITION | MFR | 2009 | 1 | 0 | 1 |
| 4373 | MANSIONS SENIOR ADDITION | MFR | 2009 | 1 | 0 | 1 |
| 4630 | PEBBLEBROOK | MFR | 1978 | 1 | 0 | 1 |
| 4631 | PEBBLEBROOK 2 | MFR | 1982 | 1 | 0 | 1 |
| 4810 | ROCCA VILLA | MFR | 1980 | 106 | 0 | 106 |
| 4821 | ROCKWALL PLACE APARTMENTS | MFR | 1997 | 1 | 0 | 1 |
| 4965 | SIGNAL RIDGE 1 | MFR | 1982 | 39 | 0 | 39 |
| 4966 | SIGNAL RIDGE 2 | MFR | 1983 | 57 | 0 | 57 |
| 4967 | SIGNAL RIDGE 3 | MFR | 1984 | 115 | 0 | 115 |
| 4968 | SIGNAL RIDGE 4 | MFR | 1992 | 98 | 0 | 98 |
| 5029 | SONOMA COURT | MFR | 2010 | 1 | 0 | 1 |
| 4995 | SPYGLASS HILL 1 | MFR | 1982 | 1 | 0 | 1 |
| 4996 | SPYGLASS HILL 2 | MFR | 1982 | 44 | 0 | 44 |
| 4997 | SPYGLASS HILL 3 | MFR | 1985 | 47 | 0 | 47 |
| 5138 | SWBC ADDITION PHASE 2 | MFR | 2022 | 1 | 0 | 1 |
| 5146 | SWBC ROCKWALL ADDITION PH 1 | MFR | 2019 | 1 | 0 | 1 |
| 5418 | TAC ROCKWALL ADDITION | MFR | 2021 | 0 | 1 | 1 |
| 5215 | WATERSEdge AT LRH FKA VILLAS DE PORTOFINO | MFR | 2001 | 88 | 0 | 88 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------------|-------------|------|-------|--------|-------|
| 4180 | LOWE AND ALLEN | MIXED | N/A | 19 | 4 | 23 |
| 4720 | RAINBOW ACRES | MIXED | 1962 | 12 | 7 | 19 |
| 4820 | ROCKWALL ORIGINAL TOWN | MIXED | 1861 | 96 | 6 | 102 |
| 3809 | HARBOR ROCKWALL THE | MIXED USE | 2011 | 7 | 0 | 7 |
| 4866 | ROCKWALL COMMONS | MIXED USE | 2014 | 1 | 0 | 1 |
| 4990 | SKYVIEW COUNTRY ESTATES PHASE 3 | OTHER | 1984 | 4 | 0 | 4 |
| 5307 | AARON SELDEN ADDITION | RESIDENTIAL | 2021 | 0 | 0 | 2 |
| 3017 | ABBOTT ADDITION | RESIDENTIAL | 2023 | 1 | 0 | 1 |
| 3005 | ADAMS ADDN | RESIDENTIAL | 1984 | 1 | 0 | 1 |
| 3005 | ADAMS ADDN | RESIDENTIAL | 1984 | 2 | 0 | 2 |
| 3020 | AIRPORT ACRES | RESIDENTIAL | 1984 | 17 | 3 | 20 |
| 5304 | AKROMA ADDITION | RESIDENTIAL | 2022 | 0 | 3 | 3 |
| 3025 | ALAMO VALLEY | RESIDENTIAL | 1983 | 3 | 0 | 3 |
| 3034 | ALDERS AT ROCKWALL | RESIDENTIAL | 2019 | 1 | 0 | 1 |
| 3030 | ALEXANDER | RESIDENTIAL | 1954 | 6 | 1 | 7 |
| 3995 | ALLEN HOGUE SUBD | RESIDENTIAL | 1984 | 1 | 1 | 1 |
| 3040 | AMACHRIS PLACE | RESIDENTIAL | 1980 | 7 | 0 | 7 |
| 3050 | AMICK | RESIDENTIAL | 1997 | 26 | 4 | 30 |
| 3051 | AMICK 20A THOMAS SUBD | RESIDENTIAL | 1984 | 1 | 0 | 1 |
| 3070 | AUSTIN | RESIDENTIAL | N/A | 4 | 2 | 6 |
| 3069 | AUTUMN ADDITION | RESIDENTIAL | 2017 | 2 | 0 | 2 |
| 3081 | BALTAZAR GONZALES | RESIDENTIAL | 2009 | 1 | 0 | 1 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--------------------------------|-------------|------|-------|--------|-------|
| 3079 | BARKER ADDN | RESIDENTIAL | 1995 | 2 | 0 | 2 |
| 3090 | BARNES | RESIDENTIAL | N/A | 3 | 2 | 5 |
| 3091 | BARNES ESTATE ETJ | RESIDENTIAL | 2014 | 1 | 0 | 1 |
| 3092 | BARZ ACRE | RESIDENTIAL | 1991 | 1 | 1 | 2 |
| 3096 | BENTON COURT | RESIDENTIAL | 1997 | 6 | 0 | 6 |
| 3099 | BENTON WOODS | RESIDENTIAL | 1993 | 34 | 0 | 34 |
| 3105 | BEST ESTATE ADDITION | RESIDENTIAL | 2018 | 1 | 0 | 1 |
| 3006 | BIRDS NEST ADDITION | RESIDENTIAL | 2021 | 0 | 0 | 2 |
| 3110 | BISHOP UNRECORDED | RESIDENTIAL | 1907 | 8 | 0 | 8 |
| 3124 | BLASE | RESIDENTIAL | 2008 | 2 | 0 | 2 |
| 3123 | BLUE SKY SUBD | RESIDENTIAL | 1985 | 1 | 0 | 1 |
| 3126 | BLUEBERRY HILL ESTATE ADDITION | RESIDENTIAL | 2017 | 1 | 0 | 1 |
| 3142 | BREEZY HILL ESTATES ETJ | RESIDENTIAL | 2015 | 4 | 0 | 4 |
| 3127 | BREEZY HILL LANE ADDITION | RESIDENTIAL | 2020 | 0 | 3 | 3 |
| 3146 | BREEZY HILL PH 1 | RESIDENTIAL | 2014 | 27 | 0 | 27 |
| 3137 | BREEZY HILL PH 10 | RESIDENTIAL | 2018 | 82 | 0 | 82 |
| 3082 | BREEZY HILL PH 11 | RESIDENTIAL | 2020 | 80 | 1 | 81 |
| 3147 | BREEZY HILL PH 2A AND 2B | RESIDENTIAL | 2014 | 131 | 0 | 131 |
| 3148 | BREEZY HILL PH 3 | RESIDENTIAL | 2014 | 73 | 0 | 73 |
| 3149 | BREEZY HILL PH 4 | RESIDENTIAL | 2015 | 51 | 0 | 51 |
| 3161 | BREEZY HILL PH 5 | RESIDENTIAL | 2016 | 82 | 0 | 82 |
| 3163 | BREEZY HILL PH 6 | RESIDENTIAL | 2018 | 79 | 1 | 80 |
| 3139 | BREEZY HILL PH 7 | RESIDENTIAL | 2017 | 11 | 0 | 11 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|----------------------|-------------|------|-------|--------|-------|
| 3198 | BREEZY HILL PH 8 | RESIDENTIAL | 2018 | 62 | 0 | 62 |
| 3141 | BREEZY HILL PH 9 | RESIDENTIAL | 2016 | 59 | 0 | 59 |
| 3134 | BREWER BEND ADDITION | RESIDENTIAL | 2024 | 3 | 0 | 3 |
| 3136 | BRIONES ADDITION | RESIDENTIAL | 2018 | 2 | 0 | 2 |
| 3170 | BUFFINGTON | RESIDENTIAL | N/A | 1 | 0 | 1 |
| 3193 | BURKE RIDGE | RESIDENTIAL | 2016 | 2 | 0 | 2 |
| 3191 | BUTTGEN ADDITION | RESIDENTIAL | 2005 | 1 | 0 | 1 |
| 3230 | CANUPS | RESIDENTIAL | 1944 | 36 | 18 | 54 |
| 3247 | CARROLL ESTATES SUBD | RESIDENTIAL | 1987 | 2 | 0 | 2 |
| 3248 | CARUTH LAKE PH 1 | RESIDENTIAL | 1994 | 34 | 0 | 34 |
| 3269 | CARUTH LAKE PH 1B | RESIDENTIAL | 2015 | 2 | 0 | 2 |
| 3249 | CARUTH LAKE PH 2 | RESIDENTIAL | 1994 | 9 | 0 | 9 |
| 3251 | CARUTH LAKE PH 3 | RESIDENTIAL | 1996 | 19 | 0 | 19 |
| 3252 | CARUTH LAKE PH 4 | RESIDENTIAL | 1997 | 74 | 0 | 74 |
| 3253 | CARUTH LAKE PH 5 | RESIDENTIAL | 1997 | 137 | 0 | 137 |
| 3256 | CARUTH LAKE PH 6 | RESIDENTIAL | 2005 | 157 | 0 | 157 |
| 3261 | CARUTH LAKE PH 7A | RESIDENTIAL | 2012 | 20 | 0 | 20 |
| 3262 | CARUTH LAKES PH 7B | RESIDENTIAL | 2012 | 67 | 0 | 67 |
| 3264 | CARUTH LAKES PH 7C | RESIDENTIAL | 2013 | 31 | 0 | 31 |
| 3265 | CARUTH LAKES PH 7D | RESIDENTIAL | 2013 | 37 | 0 | 37 |
| 3263 | CARUTH LAKES PH 8A | RESIDENTIAL | 2012 | 67 | 0 | 67 |
| 3266 | CARUTH LAKES PH 8B | RESIDENTIAL | 2013 | 69 | 0 | 69 |
| 3241 | CASTLE RIDGE PH 1 | RESIDENTIAL | 2006 | 56 | 0 | 56 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|----------------------------------|-------------|------|-------|--------|-------|
| 3238 | CASTLE RIDGE PH2 | RESIDENTIAL | 2008 | 80 | 0 | 80 |
| 3237 | CASTLE RIDGE PH3 | RESIDENTIAL | 2016 | 60 | 0 | 60 |
| 3260 | CHANDLERS LANDING PH 1 | RESIDENTIAL | 1973 | 24 | 0 | 24 |
| 3350 | CHANDLERS LANDING PH 10 | RESIDENTIAL | 1975 | 60 | 0 | 60 |
| 3355 | CHANDLERS LANDING PH 14 | RESIDENTIAL | 1984 | 31 | 0 | 31 |
| 3356 | CHANDLERS LANDING PH 15 REPLAT-2 | RESIDENTIAL | 1994 | 87 | 0 | 87 |
| 3357 | CHANDLERS LANDING PH 16 REPLAT | RESIDENTIAL | 1998 | 56 | 0 | 56 |
| 3360 | CHANDLERS LANDING PH 18 | RESIDENTIAL | 1984 | 13 | 0 | 13 |
| 3361 | CHANDLERS LANDING PH 18 SEC 2 | RESIDENTIAL | 1985 | 38 | 0 | 38 |
| 3362 | CHANDLERS LANDING PH 19 | RESIDENTIAL | 1985 | 73 | 2 | 75 |
| 3270 | CHANDLERS LANDING PH 2 | RESIDENTIAL | 1973 | 50 | 0 | 50 |
| 3363 | CHANDLERS LANDING PH 20 | RESIDENTIAL | 1985 | 62 | 0 | 62 |
| 3280 | CHANDLERS LANDING PH 3 | RESIDENTIAL | 1973 | 43 | 0 | 43 |
| 3290 | CHANDLERS LANDING PH 4 | RESIDENTIAL | 1997 | 22 | 0 | 22 |
| 3300 | CHANDLERS LANDING PH 5 | RESIDENTIAL | 1976 | 37 | 0 | 37 |
| 3310 | CHANDLERS LANDING PH 6 | RESIDENTIAL | 1977 | 68 | 0 | 68 |
| 3320 | CHANDLERS LANDING PH 7 | RESIDENTIAL | 1976 | 34 | 0 | 34 |
| 3340 | CHANDLERS LANDING PH 9 | RESIDENTIAL | 1975 | 14 | 0 | 14 |
| 3335 | CHANDLERS LANDING PH 9 SEC 1 | RESIDENTIAL | 1981 | 1 | 0 | 1 |
| 3259 | CHANDLERS LANDING YACHT CLUB | RESIDENTIAL | 1974 | 2 | 0 | 2 |
| 3380 | CHAPMAN | RESIDENTIAL | 1978 | 1 | 0 | 1 |
| 3232 | CLARK ADDITION | RESIDENTIAL | 2019 | 2 | 0 | 2 |
| 3398 | CLARK HOMESTEAD ADDITION | RESIDENTIAL | 2001 | 1 | 0 | 1 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 3399 | CLARK STREET HOMESITE | RESIDENTIAL | 2004 | 2 | 0 | 2 |
| 3408 | CONOVER ADDITION | RESIDENTIAL | 2010 | 2 | 0 | 2 |
| 3414 | COUNSELMAN ADDITION | RESIDENTIAL | 2003 | 1 | 0 | 1 |
| 3436 | COX ACRES | RESIDENTIAL | 2017 | 2 | 0 | 2 |
| 3416 | COX ADDITION | RESIDENTIAL | 2005 | 1 | 0 | 1 |
| 3423 | CRAWFORD ADDN | RESIDENTIAL | 1988 | 1 | 0 | 1 |
| 3424 | CREEKSIDE THE SHORES | RESIDENTIAL | 1999 | 27 | 0 | 27 |
| 3422 | CREEKSIDE VILLAGE - PH 2 | RESIDENTIAL | 2000 | 66 | 0 | 66 |
| 3428 | CRESTVIEW PH 1 THE SHORES | RESIDENTIAL | 1997 | 38 | 0 | 38 |
| 3429 | CRESTVIEW PH 2 THE SHORES | RESIDENTIAL | 1997 | 76 | 0 | 76 |
| 3433 | CRESTVIEW PH 3 | RESIDENTIAL | 1999 | 93 | 0 | 93 |
| 3440 | CULLINS ADDN | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 3450 | CUTTER HILL 1 | RESIDENTIAL | 1977 | 20 | 0 | 20 |
| 3460 | CUTTER HILL 2 | RESIDENTIAL | 1977 | 27 | 0 | 27 |
| 3470 | CUTTER HILL 3 | RESIDENTIAL | 1981 | 13 | 0 | 13 |
| 3543 | D R TAYLOR ADDITION | RESIDENTIAL | 2003 | 1 | 0 | 1 |
| 3476 | DABNEY ADDITION | RESIDENTIAL | 2006 | 2 | 0 | 2 |
| 3478 | DALTON RANCH | RESIDENTIAL | 2006 | 149 | 0 | 149 |
| 3481 | DALTON RANCH PH 2 | RESIDENTIAL | 2007 | 2 | 0 | 2 |
| 3080 | DANNY BARKER | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 3500 | DAWSON | RESIDENTIAL | N/A | 16 | 0 | 16 |
| 3483 | DC ADDITION | RESIDENTIAL | 2021 | 2 | 0 | 2 |
| 3512 | DEL BOSQUE SUBD | RESIDENTIAL | 1984 | 1 | 0 | 1 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--------------------------------|-------------|------|-------|--------|-------|
| 3516 | DEVOLL PLACE ADDITION | RESIDENTIAL | 2020 | 1 | 0 | 1 |
| 3525 | DIRKWOOD ESTATES | RESIDENTIAL | 1993 | 2 | 1 | 3 |
| 5386 | DISCOVERY LAKES PHASE 1 | RESIDENTIAL | 2023 | 4 | 32 | 36 |
| 3540 | DODSON HARDIN | RESIDENTIAL | 1953 | 3 | 0 | 3 |
| 3539 | DOUBLE T VENTURES | RESIDENTIAL | 2014 | 1 | 0 | 1 |
| 3542 | DOWELL RD ADDITION | RESIDENTIAL | 1994 | 9 | 0 | 9 |
| 3550 | DUKE | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 3559 | EAGLE POINT ESTATES | RESIDENTIAL | 2012 | 11 | 2 | 13 |
| 3584 | EDWARDS ACRES SUBD | RESIDENTIAL | 2004 | 2 | 0 | 2 |
| 3545 | ELB SUBDIVISION | RESIDENTIAL | 1986 | 1 | 0 | 1 |
| 3587 | ELSEY ADDITION | RESIDENTIAL | 2007 | 2 | 0 | 2 |
| 3552 | EMERSON FARMS | RESIDENTIAL | 2023 | 5 | 103 | 108 |
| 3562 | ENSLEY ADDN | RESIDENTIAL | 2019 | 1 | 0 | 1 |
| 3590 | EPPERSON | RESIDENTIAL | 1980 | 1 | 0 | 1 |
| 3600 | EPPSTEIN | RESIDENTIAL | 1977 | 44 | 0 | 44 |
| 3605 | EPPSTEIN STARK ADDITION | RESIDENTIAL | 1980 | 4 | 0 | 4 |
| 3601 | EPTON ADDITION | RESIDENTIAL | 2018 | 1 | 0 | 1 |
| 3606 | ESTATES ON THE RIDGE | RESIDENTIAL | 2016 | 10 | 0 | 10 |
| 3690 | F AND M | RESIDENTIAL | 1991 | 61 | 2 | 63 |
| 3617 | FAIRWAY POINTE PH 1 THE SHORES | RESIDENTIAL | 1997 | 7 | 0 | 7 |
| 3618 | FAIRWAY POINTE PH 2 THE SHORES | RESIDENTIAL | 1997 | 69 | 0 | 69 |
| 3619 | FAIRWAY POINTE PH 3 | RESIDENTIAL | 1999 | 40 | 0 | 40 |
| 3685 | FLAGSTONE ADDITION | RESIDENTIAL | 2006 | 82 | 0 | 82 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--------------------------|-------------|------|-------|--------|-------|
| 3700 | FONDREN | RESIDENTIAL | 1945 | 12 | 0 | 12 |
| 3705 | FONTANNA RANCH PH 1 | RESIDENTIAL | 2007 | 106 | 0 | 106 |
| 3706 | FONTANNA RANCH PH2 | RESIDENTIAL | 2016 | 47 | 0 | 47 |
| 3709 | FONTANNA RANCH PHASE III | RESIDENTIAL | 2018 | 53 | 0 | 53 |
| 3710 | FOREE | RESIDENTIAL | 1913 | 24 | 0 | 24 |
| 3701 | FOX ADDITION | RESIDENTIAL | 2022 | 2 | 0 | 2 |
| 3713 | FOXCHASE PH 1 | RESIDENTIAL | 1989 | 32 | 0 | 32 |
| 3711 | FOXCHASE PH 2 | RESIDENTIAL | 1994 | 25 | 0 | 25 |
| 3712 | FOXCHASE PH 3 | RESIDENTIAL | 1994 | 25 | 0 | 25 |
| 3714 | FOXCHASE PH 4 | RESIDENTIAL | 1995 | 40 | 0 | 40 |
| 3716 | FOXCHASE PH 5 | RESIDENTIAL | 1998 | 48 | 0 | 48 |
| 3717 | FOXCHASE PH 6 | RESIDENTIAL | 2000 | 37 | 0 | 37 |
| 3718 | FOXCHASE PH 7 | RESIDENTIAL | 2005 | 11 | 0 | 9 |
| 3723 | GAMEZ ADDITION | RESIDENTIAL | 2005 | 1 | 0 | 1 |
| 3722 | GAMEZ ADDITION RIDGE RD | RESIDENTIAL | 2005 | 1 | 0 | 1 |
| 3730 | GARNER | RESIDENTIAL | N/A | 19 | 3 | 22 |
| 3732 | GEORGE MORTON ESTATE | RESIDENTIAL | 2023 | 0 | 3 | 3 |
| 3756 | GIDEON GROVE NORTH | RESIDENTIAL | 2018 | 73 | 0 | 73 |
| 3728 | GIDEON GROVE PHASE 2 | RESIDENTIAL | 2022 | 80 | 16 | 96 |
| 3753 | GOODMAN ADDITION | RESIDENTIAL | 2006 | 1 | 0 | 1 |
| 4730 | GRADY RASH SUBD | RESIDENTIAL | 1984 | 4 | 0 | 4 |
| 3760 | GREEN MEADOWS | RESIDENTIAL | 1975 | 9 | 0 | 1 |
| 3750 | GREENLEE | RESIDENTIAL | 1972 | 21 | 0 | 21 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|----------------------------|-------------|------|-------|--------|-------|
| 3770 | GREENVALLEY | RESIDENTIAL | 1958 | 6 | 0 | 6 |
| 3780 | GRIFFITH | RESIDENTIAL | N/A | 35 | 1 | 36 |
| 3813 | HAIRSTON ADDN | RESIDENTIAL | 1985 | 3 | 0 | 3 |
| 4680 | HAL PHELPS | RESIDENTIAL | 1959 | 11 | 0 | 11 |
| 3803 | HALEY AND KYLE ADDITION | RESIDENTIAL | 2020 | 2 | 0 | 2 |
| 3807 | HARBOR LANDING PH 1 | RESIDENTIAL | 1986 | 38 | 0 | 38 |
| 3808 | HARBOR LANDING PH 2 | RESIDENTIAL | 1987 | 22 | 7 | 29 |
| 3816 | HARLAN PARK PH 1 | RESIDENTIAL | 1985 | 65 | 0 | 65 |
| 3817 | HARLAN PARK PH 2 | RESIDENTIAL | 1995 | 30 | 0 | 30 |
| 3820 | HARRIS | RESIDENTIAL | 1960 | 7 | 1 | 8 |
| 3825 | HARRIS HEIGHTS 1 | RESIDENTIAL | 1970 | 58 | 0 | 58 |
| 3826 | HARRIS HEIGHTS 2 | RESIDENTIAL | 1985 | 15 | 0 | 15 |
| 3827 | HARRIS HEIGHTS 3 | RESIDENTIAL | 1985 | 26 | 0 | 26 |
| 3828 | HARRIS HEIGHTS 4 | RESIDENTIAL | 1989 | 4 | 0 | 4 |
| 3842 | HARTMAN ADDITION | RESIDENTIAL | 2003 | 2 | 0 | 2 |
| 3860 | HERITAGE HEIGHTS | RESIDENTIAL | 1970 | 78 | 0 | 78 |
| 3867 | HICKORY RIDGE PH 1 | RESIDENTIAL | 2001 | 139 | 0 | 139 |
| 3868 | HICKORY RIDGE PH 2 | RESIDENTIAL | 2001 | 119 | 0 | 119 |
| 3869 | HICKORY RIDGE PH 3 | RESIDENTIAL | 2001 | 41 | 0 | 41 |
| 3871 | HICKORY RIDGE PH 4 | RESIDENTIAL | 2003 | 245 | 0 | 245 |
| 3870 | HIDDEN VALLEY ESTATES NO 2 | RESIDENTIAL | 2018 | 3 | 0 | 3 |
| 3880 | HIGHLAND ACRES | RESIDENTIAL | 1968 | 6 | 0 | 6 |
| 3890 | HIGHLAND HILLS | RESIDENTIAL | 1979 | 59 | 0 | 59 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--------------------------|-------------|------|-------|--------|-------|
| 3895 | HIGHLAND MEADOWS 1 | RESIDENTIAL | 1996 | 100 | 0 | 100 |
| 3896 | HIGHLAND MEADOWS 2 | RESIDENTIAL | 2000 | 97 | 0 | 97 |
| 3940 | HIGHRIDGE EST | RESIDENTIAL | 1972 | 97 | 0 | 97 |
| 3950 | HIGHWOOD | RESIDENTIAL | 1955 | 73 | 2 | 75 |
| 3967 | HILLCREST SHORES | RESIDENTIAL | 1994 | 67 | 0 | 67 |
| 3968 | HILLCREST SHORES PH 2 | RESIDENTIAL | 1995 | 114 | 0 | 114 |
| 3976 | HILLCREST SHORES PH 3 | RESIDENTIAL | 2002 | 110 | 0 | 110 |
| 3971 | HILLSIDE PH 1 THE SHORES | RESIDENTIAL | 1997 | 8 | 0 | 8 |
| 3972 | HILLSIDE PH 2 THE SHORES | RESIDENTIAL | 1997 | 74 | 0 | 74 |
| 3973 | HILLSIDE PH 3 THE SHORES | RESIDENTIAL | 1999 | 65 | 0 | 65 |
| 3974 | HILLSIDE PH 4 THE SHORES | RESIDENTIAL | 2000 | 69 | 0 | 69 |
| 3975 | HILLSIDE PH 5 THE SHORES | RESIDENTIAL | 2000 | 117 | 0 | 117 |
| 3984 | HODGDON ADDITION | RESIDENTIAL | 2017 | 2 | 0 | 2 |
| 3996 | HOMESTEAD PHASE I | RESIDENTIAL | 2024 | 42 | 144 | 186 |
| 4019 | HUDSON SOTO | RESIDENTIAL | 2009 | 1 | 0 | 1 |
| 4023 | HUNTER ADDITION | RESIDENTIAL | 2024 | 1 | 0 | 1 |
| 4021 | HURST ADDITION | RESIDENTIAL | 2002 | 2 | 0 | 2 |
| 4043 | INDEPENDENCE PASS | RESIDENTIAL | 2009 | 2 | 1 | 3 |
| 4041 | INTEGRITY ADDITION PH 1 | RESIDENTIAL | 2007 | 3 | 0 | 3 |
| 4039 | INTEGRITY ADDITION PH 2 | RESIDENTIAL | 2013 | 4 | 0 | 4 |
| 5242 | ISAAC ADDITION | RESIDENTIAL | 2021 | 2 | 0 | 2 |
| 4048 | ISAAC PENA | RESIDENTIAL | 2007 | 1 | 0 | 1 |
| 4660 | J L PEOPLES ADDITION | RESIDENTIAL | 1989 | 15 | 0 | 15 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--------------------------|-------------|------|-------|--------|-------|
| 3510 | J W DAY | RESIDENTIAL | N/A | 6 | 0 | 6 |
| 4051 | JACK CANUP | RESIDENTIAL | 2007 | 3 | 1 | 4 |
| 4049 | JAMESON ADDITION | RESIDENTIAL | 2006 | 1 | 0 | 1 |
| 4050 | JAYROE ADDN | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 4131 | JERRI LAMROCK ADDN | RESIDENTIAL | 1989 | 1 | 0 | 1 |
| 4058 | JOHNSON ADDITION | RESIDENTIAL | 2021 | 1 | 0 | 1 |
| 4096 | KATHLEENS ADDITION | RESIDENTIAL | 1998 | 2 | 0 | 2 |
| 4077 | KAYCE LYNN ADDITION NO | RESIDENTIAL | 2018 | 1 | 0 | 1 |
| 4076 | KAYCE LYNN ADDITION NO 1 | RESIDENTIAL | 2018 | 2 | 0 | 2 |
| 4102 | KELLY RANCH ADDITION | RESIDENTIAL | 2017 | 1 | 0 | 1 |
| 4143 | KENNEDY ADDITION | RESIDENTIAL | 2021 | 1 | 1 | 1 |
| 4109 | KINSEY ADDITION | RESIDENTIAL | 2023 | 4 | 0 | 4 |
| 4330 | L AND W | RESIDENTIAL | 1949 | 13 | 0 | 13 |
| 4310 | L L LEONARD | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 4141 | LADERA ROCKWALL | RESIDENTIAL | 2018 | 84 | 0 | 84 |
| 4126 | LAGO VISTA | RESIDENTIAL | 1994 | 92 | 0 | 92 |
| 4190 | LAKE MEADOWS | RESIDENTIAL | 1968 | 26 | 0 | 26 |
| 4200 | LAKE RAY HUBBARD EST | RESIDENTIAL | 1973 | 64 | 0 | 64 |
| 4230 | LAKEHILL ADDITION PH 1 | RESIDENTIAL | 1977 | 12 | 0 | 12 |
| 4240 | LAKEHILL ADDITION PH 2 | RESIDENTIAL | 1978 | 9 | 0 | 9 |
| 4210 | LAKERIDGE EST | RESIDENTIAL | 1984 | 3 | 1 | 4 |
| 4220 | LAKERIDGE PARK | RESIDENTIAL | 1972 | 89 | 0 | 89 |
| 4221 | LAKERIDGE PARK BOTO ADDN | RESIDENTIAL | 1984 | 1 | 1 | 2 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 4129 | LAKES ASSEMBLY ADDITION | RESIDENTIAL | 2014 | 1 | 0 | 1 |
| 4250 | LAKESIDE VILLAGE PH 1 | RESIDENTIAL | 1971 | 114 | 0 | 114 |
| 4260 | LAKESIDE VILLAGE PH 2 | RESIDENTIAL | 1971 | 119 | 1 | 120 |
| 4270 | LAKESIDE VILLAGE PH 3 | RESIDENTIAL | 1972 | 116 | 2 | 118 |
| 4280 | LAKESIDE VILLAGE PH 4 | RESIDENTIAL | 1973 | 59 | 2 | 61 |
| 4284 | LAKESIDE VILLAGE PH 5A | RESIDENTIAL | 1998 | 24 | 0 | 24 |
| 4283 | LAKESIDE VILLAGE PH 5B | RESIDENTIAL | 1999 | 21 | 1 | 22 |
| 4282 | LAKESIDE VILLAGE PH 5C | RESIDENTIAL | 2003 | 57 | 4 | 61 |
| 4289 | LAKEVIEW SUMMIT PH 1 | RESIDENTIAL | 2000 | 103 | 0 | 103 |
| 4291 | LAKEVIEW SUMMIT PH 1A | RESIDENTIAL | 2001 | 157 | 0 | 157 |
| 4292 | LAKEVIEW SUMMIT PH 2 | RESIDENTIAL | 2003 | 104 | 0 | 104 |
| 4293 | LAKEVIEW SUMMIT PH 3 | RESIDENTIAL | 2005 | 84 | 0 | 84 |
| 4294 | LAKEVIEW SUMMIT PH 4 | RESIDENTIAL | 2007 | 101 | 1 | 102 |
| 4298 | LAMAR STREET HABITAT NO 1 | RESIDENTIAL | 2018 | 2 | 0 | 2 |
| 4134 | LAS PRIMERAS | RESIDENTIAL | 2003 | 1 | 3 | 4 |
| 4149 | LEE ACRES ADDITION | RESIDENTIAL | 2024 | 2 | 2 | 4 |
| 4745 | LEE RHOADES SUBDIVISION | RESIDENTIAL | 1986 | 2 | 0 | 2 |
| 4160 | LEONARD AND ADAMS | RESIDENTIAL | 1925 | 7 | 0 | 7 |
| 4320 | LOFLAND | RESIDENTIAL | 1995 | 5 | 4 | 9 |
| 4316 | LOFLAND FARMS PH 1 | RESIDENTIAL | 2000 | 86 | 0 | 86 |
| 4317 | LOFLAND FARMS PH 2 | RESIDENTIAL | 2000 | 62 | 0 | 62 |
| 4318 | LOFLAND FARMS PH 3 | RESIDENTIAL | 2002 | 11 | 0 | 11 |
| 4319 | LOFLAND FARMS PH 4 | RESIDENTIAL | 2002 | 74 | 0 | 74 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|------------------------------|-------------|------|-------|--------|-------|
| 4314 | LOFLAND FARMS PH 5A | RESIDENTIAL | 2003 | 86 | 0 | 86 |
| 4311 | LOFLAND FARMS PH 5B | RESIDENTIAL | 2004 | 73 | 1 | 74 |
| 4323 | LOFLAND LAKE ESTATES | RESIDENTIAL | 1994 | 6 | 1 | 7 |
| 4324 | LOFLAND LAKE ESTATES 2 | RESIDENTIAL | 1995 | 0 | 0 | 7 |
| 4331 | LYNDEN PARK ESTATES PHASE 1A | RESIDENTIAL | 1997 | 70 | 0 | 70 |
| 4332 | LYNDEN PARK ESTATES PHASE 1B | RESIDENTIAL | 1997 | 27 | 0 | 27 |
| 4333 | LYNDEN PARK ESTATES PHASE 2 | RESIDENTIAL | 2000 | 104 | 0 | 104 |
| 4334 | LYNDEN PARK ESTATES PHASE 3 | RESIDENTIAL | 2001 | 71 | 0 | 71 |
| 4336 | LYNDEN PARK ESTATES PHASE 4 | RESIDENTIAL | 2004 | 84 | 0 | 84 |
| 4383 | M AND M JOHNSON | RESIDENTIAL | 2014 | 1 | 0 | 1 |
| 4625 | M C PASSMORE | RESIDENTIAL | 1983 | 1 | 0 | 1 |
| 4335 | MAC NO 1 SUBD | RESIDENTIAL | 1984 | 5 | 0 | 5 |
| 4397 | MARINA VILLAGE | RESIDENTIAL | 2023 | 1 | 38 | 39 |
| 4349 | MASON STEED ADDITION | RESIDENTIAL | 1993 | 2 | 2 | 4 |
| 4352 | MATCHPOINT TOWNHOMES WEST | RESIDENTIAL | 1981 | 1 | 0 | 1 |
| 4351 | MAYNARD PLACE | RESIDENTIAL | 2008 | 1 | 0 | 1 |
| 4355 | MAYTONA RANCH ESTATES | RESIDENTIAL | 1983 | 19 | 0 | 19 |
| 4497 | MCLEAN MOORE ADDITION | RESIDENTIAL | 1987 | 2 | 0 | 2 |
| 4358 | MEADOWCREEK ESTATES PH 1 | RESIDENTIAL | 1999 | 158 | 0 | 158 |
| 4359 | MEADOWCREEK ESTATES PH 2 | RESIDENTIAL | 2000 | 103 | 0 | 103 |
| 4363 | MEADOWCREEK ESTATES PH 3 | RESIDENTIAL | 2001 | 131 | 0 | 131 |
| 4364 | MEADOWCREEK ESTATES PH 4 | RESIDENTIAL | 2001 | 192 | 0 | 192 |
| 4380 | MEADOWVIEW RANCH ESTATES | RESIDENTIAL | 1977 | 24 | 0 | 24 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|----------------------------|-------------|------|-------|--------|-------|
| 4390 | MELTON | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 4405 | MICK | RESIDENTIAL | 1982 | 1 | 0 | 1 |
| 4420 | MILL CO | RESIDENTIAL | N/A | 18 | 0 | 18 |
| 4443 | MONNIE RODGERS SUBDIVISION | RESIDENTIAL | 1964 | 4 | 0 | 4 |
| 4480 | MUSTANG ACRES | RESIDENTIAL | 1967 | 5 | 0 | 5 |
| 4505 | NANCY D | RESIDENTIAL | 1983 | 3 | 0 | 3 |
| 4900 | NE&JO ADD | RESIDENTIAL | 2004 | 2 | 0 | 2 |
| 4518 | NELLER ADDITION | RESIDENTIAL | 2021 | 1 | 1 | 2 |
| 5390 | NELSON LAKE ESTATES | RESIDENTIAL | 2023 | 74 | 194 | 268 |
| 4507 | NEWPORT PLACE | RESIDENTIAL | 1996 | 43 | 0 | 43 |
| 4509 | NORTH TOWNE ADDITION | RESIDENTIAL | 1998 | 41 | 0 | 41 |
| 4508 | NORTH WEST SUBD | RESIDENTIAL | 1984 | 2 | 0 | 2 |
| 4520 | NORTHCREST EST 2 | RESIDENTIAL | 1978 | 12 | 0 | 12 |
| 4514 | NORTHGATE ADDITION | RESIDENTIAL | 2020 | 31 | 9 | 40 |
| 4560 | NORTHSHORE PH 1 | RESIDENTIAL | 1974 | 132 | 0 | 132 |
| 4550 | NORTHSHORE PH 1A | RESIDENTIAL | 1981 | 94 | 0 | 94 |
| 4575 | NORTHSHORE PH 2A | RESIDENTIAL | 1984 | 80 | 0 | 80 |
| 4576 | NORTHSHORE PH 2B | RESIDENTIAL | 1986 | 76 | 0 | 76 |
| 4581 | NORTHSHORE PH 4 | RESIDENTIAL | 1987 | 45 | 0 | 45 |
| 4600 | OAK CREEK | RESIDENTIAL | 1984 | 2 | 76 | 78 |
| 4603 | OAKS OF BUFFALO WAY | RESIDENTIAL | 1997 | 60 | 0 | 60 |
| 4601 | OAKS OF BUFFALO WAY PH 2 | RESIDENTIAL | 2004 | 3 | 0 | 3 |
| 4638 | OLIVE FANNIN ADDITION | RESIDENTIAL | 2021 | 3 | 0 | 3 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 4605 | ORLEANS ON THE LAKE | RESIDENTIAL | 1984 | 41 | 0 | 41 |
| 4608 | ORSORNIO SILVA | RESIDENTIAL | 2009 | 4 | 0 | 4 |
| 4618 | PARK PLACE 1 | RESIDENTIAL | 1987 | 1 | 0 | 1 |
| 4644 | PARK PLACE PHASE 4 | RESIDENTIAL | 2022 | 0 | 5 | 5 |
| 4621 | PARK PLACE WEST PH 2 | RESIDENTIAL | 2005 | 88 | 4 | 92 |
| 4622 | PARK PLACE WEST PH 3 | RESIDENTIAL | 2018 | 65 | 21 | 86 |
| 4626 | PATRICIA A MAY ADDITION | RESIDENTIAL | 2003 | 2 | 0 | 2 |
| 4640 | PECAN GROVE | RESIDENTIAL | 1978 | 2 | 0 | 2 |
| 4656 | PEOPLES TRACT ADDITION | RESIDENTIAL | 2024 | 4 | 4 | 8 |
| 4679 | PIERCY PLACE | RESIDENTIAL | 2015 | 1 | 0 | 1 |
| 4690 | PITTMAN | RESIDENTIAL | 1978 | 4 | 0 | 4 |
| 4709 | PROMENADE HARBOR | RESIDENTIAL | 2002 | 168 | 0 | 168 |
| 4704 | PROMENADE HARBOR PH 2 | RESIDENTIAL | 2004 | 19 | 0 | 19 |
| 4713 | PROMISE LAND ADDITION | RESIDENTIAL | 2001 | 1 | 0 | 1 |
| 4758 | QUAIL HOLLOW PHASE I | RESIDENTIAL | 2024 | 45 | 105 | 150 |
| 4706 | QUAIL RUN VALLEY NO 1 | RESIDENTIAL | 2001 | 56 | 0 | 56 |
| 4707 | QUAIL RUN VALLEY NO 2 | RESIDENTIAL | 2001 | 129 | 0 | 129 |
| 4718 | RAINBOW LAKE ESTATES | RESIDENTIAL | 2000 | 66 | 0 | 66 |
| 4723 | RALPH HALL ADDITION PH 2 | RESIDENTIAL | 1999 | 1 | 0 | 1 |
| 4722 | RANDOM OAKS AT THE SHORES | RESIDENTIAL | 2000 | 75 | 0 | 75 |
| 4732 | RED RIVER I | RESIDENTIAL | 2001 | 1 | 0 | 1 |
| 4737 | RENFRO CREEKSIDE ESTATES | RESIDENTIAL | 2010 | 1 | 1 | 2 |
| 4742 | RENFRO PLACE NORTH | RESIDENTIAL | 2002 | 2 | 0 | 2 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|--|-------------|------|-------|--------|-------|
| 4741 | RENFRO PLACE SOUTH | RESIDENTIAL | 2002 | 2 | 0 | 2 |
| 4753 | RHDC ADDITION | RESIDENTIAL | 2020 | 2 | 0 | 2 |
| 3835 | RICHARD HARRIS 2 | RESIDENTIAL | 1982 | 7 | 0 | 7 |
| 3836 | RICHARD HARRIS 3 | RESIDENTIAL | 1983 | 3 | 0 | 3 |
| 3841 | RICHARD HARRIS 6 | RESIDENTIAL | 1986 | 1 | 0 | 1 |
| 4760 | RIDGE HAVEN ESTATES PH2 | RESIDENTIAL | 1984 | 1 | 1 | 1 |
| 4790 | RIDGE ROAD VILLAGE | RESIDENTIAL | 1964 | 182 | 3 | 185 |
| 4749 | RIDGECREST ADDITION | RESIDENTIAL | 2016 | 45 | 0 | 45 |
| 4770 | RIDGELL | RESIDENTIAL | N/A | 5 | 2 | 7 |
| 4800 | RIDGEVIEW | RESIDENTIAL | 1964 | 22 | 0 | 22 |
| 4804 | RIOS BUFFINGTON ADDITION | RESIDENTIAL | 1996 | 1 | 0 | 1 |
| 4795 | ROBBINS ADDITION | RESIDENTIAL | 2005 | 5 | 0 | 5 |
| 4874 | ROCKWALL DOWNES PH 1 | RESIDENTIAL | 2015 | 34 | 0 | 34 |
| 4877 | ROCKWALL DOWNES PH 2 | RESIDENTIAL | 2017 | 31 | 0 | 31 |
| 4781 | ROCKWALL DOWNES PH 3 | RESIDENTIAL | 2017 | 28 | 0 | 28 |
| 4902 | ROCKWALL LAKE DEVELOPMENT NO 1 LAKE ECHO | RESIDENTIAL | 2013 | 3 | 0 | 3 |
| 4142 | ROCKWALL LAKE ESTATES EAST ADDITION | RESIDENTIAL | 2021 | 3 | 1 | 4 |
| 5370 | ROCKWALL LAKE ESTATES PH1 | RESIDENTIAL | 1956 | 369 | 73 | 442 |
| 5380 | ROCKWALL LAKE ESTATES PH2 | RESIDENTIAL | 1956 | 503 | 135 | 638 |
| 4902 | ROCKWALL LAKE ESTATES WEST | RESIDENTIAL | 1956 | 1 | 0 | 1 |
| 4852 | ROLLING MEADOWS ESTATES | RESIDENTIAL | 1997 | 18 | 0 | 18 |
| 4859 | ROSS ADDITION | RESIDENTIAL | 2004 | 1 | 0 | 1 |
| 4860 | ROYAL PARK PLACE | RESIDENTIAL | 1977 | 38 | 1 | 39 |

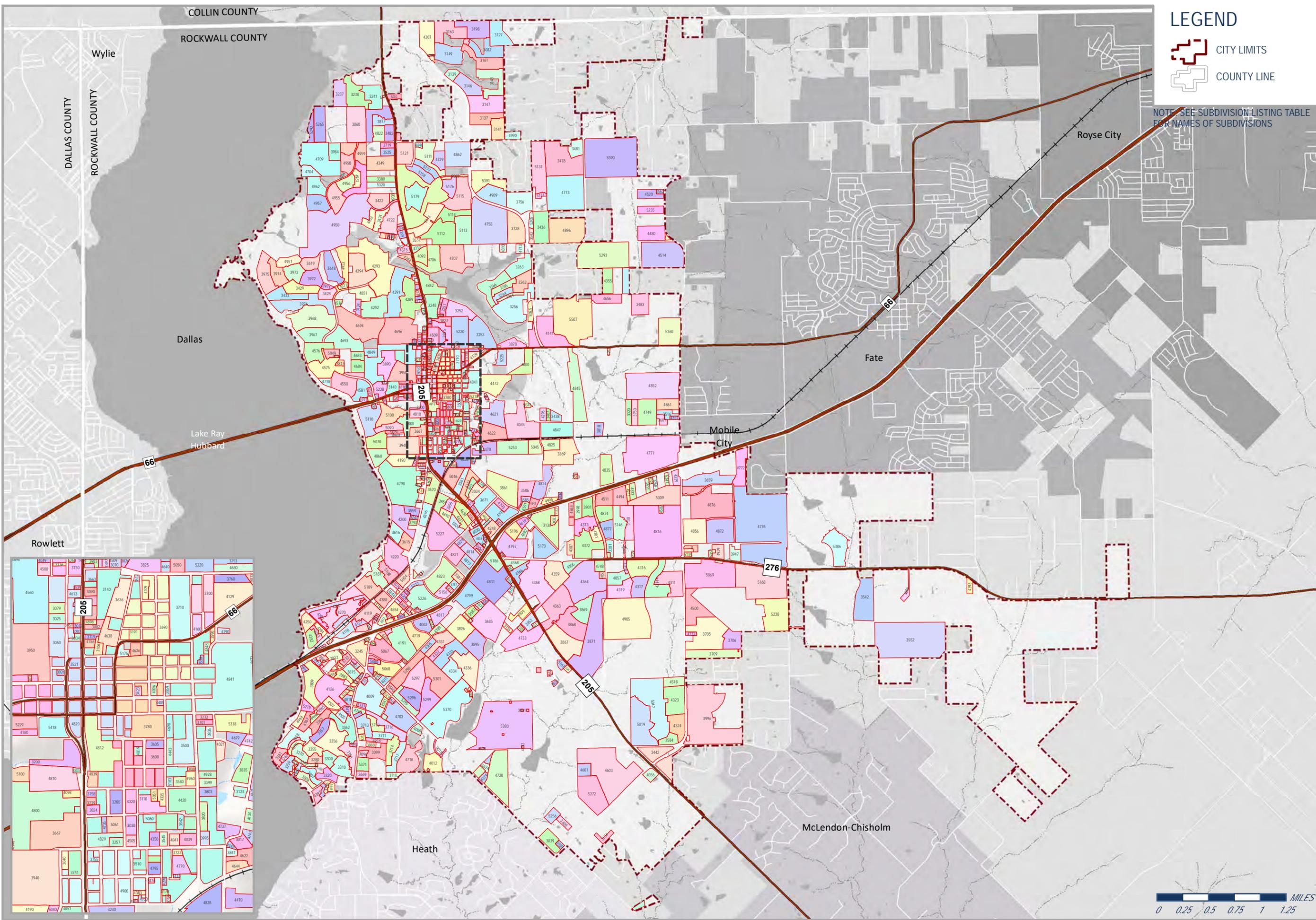
| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 4870 | RUDOLPH | RESIDENTIAL | 1979 | 1 | 0 | 1 |
| 4878 | RUFF AND SARTAIN ADDITION | RESIDENTIAL | 2011 | 1 | 1 | 2 |
| 5035 | S SPARKS ADDITION | RESIDENTIAL | 1987 | 1 | 0 | 1 |
| 4909 | SADDLE STAR ESTATES PH1 | RESIDENTIAL | 2019 | 63 | 6 | 69 |
| 5391 | SADDLE STAR PHASE 2 | RESIDENTIAL | 2023 | 49 | 31 | 80 |
| 4896 | SADDLEBROOK ESTATES 2 | RESIDENTIAL | 1978 | 45 | 0 | 45 |
| 4900 | SANGER | RESIDENTIAL | 2004 | 57 | 11 | 68 |
| 4918 | SHADYDALE ESTATES | RESIDENTIAL | 2014 | 13 | 1 | 14 |
| 4928 | SHAW | RESIDENTIAL | 2007 | 1 | 1 | 2 |
| 4960 | SHIELDS | RESIDENTIAL | 1981 | 4 | 0 | 4 |
| 4955 | SHORES NORTH PH 2A | RESIDENTIAL | 2001 | 82 | 0 | 82 |
| 4954 | SHORES NORTH PH 2B | RESIDENTIAL | 2004 | 76 | 0 | 76 |
| 4956 | SHORES NORTH PH 3A | RESIDENTIAL | 2001 | 56 | 0 | 56 |
| 4961 | SHORES NORTH PH 3B | RESIDENTIAL | 2004 | 32 | 0 | 32 |
| 4957 | SHORES NORTH PH 4A | RESIDENTIAL | 2002 | 68 | 0 | 68 |
| 4962 | SHORES NORTH PH 4B | RESIDENTIAL | 2004 | 63 | 0 | 63 |
| 4958 | SHORES NORTH PH 5 | RESIDENTIAL | 2001 | 60 | 0 | 60 |
| 4959 | SHORES NORTH PH 6 | RESIDENTIAL | 2002 | 70 | 0 | 70 |
| 4940 | SHOREVIEW EST | RESIDENTIAL | 1976 | 13 | 0 | 13 |
| 4969 | SIX O SUBD | RESIDENTIAL | 1983 | 1 | 0 | 1 |
| 5013 | SOLAR VILLAGE | RESIDENTIAL | 2014 | 6 | 0 | 6 |
| 5047 | SOMERSET PARK PH 2 | RESIDENTIAL | 2022 | 72 | 99 | 171 |
| 5019 | SOMERSET PARK PH1 | RESIDENTIAL | 2016 | 151 | 1 | 152 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|-------------------------|-------------|------|-------|--------|-------|
| 5040 | SPONG | RESIDENTIAL | 1961 | 5 | 0 | 5 |
| 5060 | ST MARYS | RESIDENTIAL | 1978 | 2 | 1 | 3 |
| 5061 | ST MARYS PLACE | RESIDENTIAL | 1978 | 14 | 0 | 14 |
| 5050 | STARK | RESIDENTIAL | 1979 | 4 | 0 | 4 |
| 5051 | STARK SUBDIVISION | RESIDENTIAL | 1998 | 2 | 0 | 2 |
| 5069 | STERLING FARMS ADDITION | RESIDENTIAL | 1997 | 48 | 0 | 48 |
| 5178 | STONE CREEK PH 1 | RESIDENTIAL | 2008 | 200 | 0 | 200 |
| 5115 | STONE CREEK PH 10 | RESIDENTIAL | 2020 | 122 | 0 | 122 |
| 5176 | STONE CREEK PH 2A | RESIDENTIAL | 2012 | 42 | 0 | 42 |
| 5177 | STONE CREEK PH 2B | RESIDENTIAL | 2012 | 52 | 0 | 52 |
| 5179 | STONE CREEK PH 3 | RESIDENTIAL | 2012 | 52 | 0 | 52 |
| 5174 | STONE CREEK PH 4 | RESIDENTIAL | 2013 | 59 | 0 | 59 |
| 5158 | STONE CREEK PH 5 | RESIDENTIAL | 2013 | 45 | 0 | 45 |
| 5111 | STONE CREEK PH 6 | RESIDENTIAL | 2014 | 76 | 0 | 76 |
| 5112 | STONE CREEK PH 7 | RESIDENTIAL | 2016 | 80 | 0 | 80 |
| 5113 | STONE CREEK PH 8 | RESIDENTIAL | 2018 | 106 | 0 | 106 |
| 5114 | STONE CREEK PH9 | RESIDENTIAL | 2019 | 65 | 0 | 65 |
| 5070 | STONEBRIDGE MEADOWS 1 | RESIDENTIAL | 1978 | 41 | 0 | 41 |
| 5080 | STONEBRIDGE MEADOWS 2 | RESIDENTIAL | 1977 | 13 | 0 | 13 |
| 5090 | STONEBRIDGE MEADOWS 3 | RESIDENTIAL | 1977 | 39 | 0 | 39 |
| 5100 | STONEBRIDGE MEADOWS 4 | RESIDENTIAL | 1979 | 72 | 0 | 72 |
| 5110 | STONEBRIDGE MEADOWS 5 | RESIDENTIAL | 1978 | 72 | 0 | 72 |
| 5131 | STONEY HOLLOW | RESIDENTIAL | 2002 | 1 | 0 | 1 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|----------------------------------|-------------|------|-------|--------|-------|
| 5131 | STONE HOLLOW ADDITION | RESIDENTIAL | 2002 | 96 | 0 | 96 |
| 5133 | STRANGE DENSON ADDN | RESIDENTIAL | 1990 | 1 | 0 | 1 |
| 5507 | TERRACES PH 1 | RESIDENTIAL | 2024 | 44 | 143 | 187 |
| 5238 | TERRACINA PH1 | RESIDENTIAL | 2020 | 74 | 39 | 113 |
| 5170 | THE CABANAS AT CHANDLERS LANDING | RESIDENTIAL | 2014 | 10 | 0 | 10 |
| 5171 | THE CABANAS REPLAT | RESIDENTIAL | 1990 | 7 | 0 | 7 |
| 3615 | THE ESTATES OF COAST ROYALE 1 | RESIDENTIAL | 1985 | 4 | 0 | 4 |
| 3616 | THE ESTATES OF COAST ROYALE 2 | RESIDENTIAL | 1985 | 13 | 0 | 13 |
| 3878 | THE HIGHLANDS | RESIDENTIAL | 2018 | 36 | 4 | 40 |
| 4297 | THE LANDON | RESIDENTIAL | 2022 | 0 | 19 | 19 |
| 4500 | THE MCLENDON COMPANIES ADDITION | RESIDENTIAL | 1999 | 5 | 0 | 5 |
| 4385 | THE MEADOWS | RESIDENTIAL | 1986 | 23 | 0 | 23 |
| 4684 | THE PINNACLE PH 2 | RESIDENTIAL | 1992 | 29 | 0 | 29 |
| 4683 | THE PINNACLE-PHASE 1 | RESIDENTIAL | 1981 | 16 | 0 | 16 |
| 4696 | THE PRESERVE PH 1 | RESIDENTIAL | 2015 | 135 | 0 | 135 |
| 4693 | THE PRESERVE PH 2 | RESIDENTIAL | 2007 | 84 | 0 | 84 |
| 4694 | THE PRESERVE PH 3 | RESIDENTIAL | 2007 | 117 | 0 | 117 |
| 4950 | THE SHORES | RESIDENTIAL | 1978 | 258 | 0 | 258 |
| 4951 | THE SHORES CLUB HOUSE | RESIDENTIAL | 2014 | 7 | 0 | 7 |
| 4952 | THE SHORES PH 2 | RESIDENTIAL | 1990 | 32 | 0 | 32 |
| 4953 | THE SHORES PH 3 | RESIDENTIAL | 1990 | 50 | 0 | 50 |
| 5046 | THE STANDARD ROCKWALL | RESIDENTIAL | 2018 | 26 | 26 | 52 |
| 5256 | THE WALLACE ADDITION | RESIDENTIAL | 2023 | 1 | 2 | 3 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 5168 | TIMBER CREEK ESTATES | RESIDENTIAL | 2001 | 242 | 0 | 242 |
| 5183 | TOVAR SUBDIVISION | RESIDENTIAL | 2000 | 2 | 0 | 2 |
| 5173 | TOWNSEND VILLAGE | RESIDENTIAL | 2013 | 80 | 0 | 80 |
| 5187 | TURTLE COVE | RESIDENTIAL | 1985 | 75 | 0 | 75 |
| 5188 | TURTLE COVE ADDITION PH 2 | RESIDENTIAL | 1998 | 53 | 0 | 53 |
| 5189 | TURTLE COVE PH 3 | RESIDENTIAL | 2000 | 101 | 1 | 102 |
| 5205 | UTLEY ADDITION | RESIDENTIAL | 2015 | 2 | 0 | 2 |
| 3205 | W E CAMPBELL | RESIDENTIAL | 1984 | 1 | 0 | 1 |
| 5220 | WADE | RESIDENTIAL | 1954 | 105 | 0 | 105 |
| 5219 | WALKER ADDITION | RESIDENTIAL | 2016 | 3 | 0 | 3 |
| 5214 | WANDA RIDGE ESTATES PH 2 | RESIDENTIAL | 2017 | 44 | 0 | 44 |
| 5227 | WATERSTONE ESTATES | RESIDENTIAL | 1994 | 123 | 0 | 123 |
| 5235 | WATSON ESTATES | RESIDENTIAL | 2008 | 3 | 0 | 3 |
| 5371 | WHISPER ROCK | RESIDENTIAL | 2018 | 22 | 8 | 30 |
| 5254 | WIDBOOM ADDITION | RESIDENTIAL | 2018 | 1 | 0 | 1 |
| 5260 | WILLIAMS | RESIDENTIAL | 1980 | 1 | 0 | 1 |
| 5265 | WILLIS ADDITION | RESIDENTIAL | 1995 | 1 | 0 | 1 |
| 5270 | WILLOW BEND ESTATES | RESIDENTIAL | 1980 | 18 | 0 | 18 |
| 5272 | WILLOWCREST ESTATES | RESIDENTIAL | 1996 | 28 | 0 | 28 |
| 5290 | WILSON | RESIDENTIAL | 1977 | 1 | 0 | 1 |
| 5292 | WIMPEE ACRES | RESIDENTIAL | 2017 | 1 | 0 | 1 |
| 5293 | WINDING CREEK SUBDIVISION | RESIDENTIAL | 2024 | 37 | 101 | 138 |
| 5295 | WINDMILL RIDGE | RESIDENTIAL | 1982 | 54 | 0 | 54 |

| # | SUBDIVISION NAME | TYPE | YEAR | BUILT | VACANT | TOTAL |
|------|---------------------------|-------------|------|-------|--------|-------|
| 5296 | WINDMILL RIDGE ESTATES 2A | RESIDENTIAL | 1984 | 82 | 0 | 82 |
| 5297 | WINDMILL RIDGE ESTATES 3A | RESIDENTIAL | 1986 | 111 | 0 | 111 |
| 5298 | WINDMILL RIDGE ESTATES 3B | RESIDENTIAL | 1994 | 80 | 0 | 80 |
| 5299 | WINDMILL RIDGE ESTATES 4 | RESIDENTIAL | 1997 | 77 | 0 | 77 |
| 5301 | WINDMILL RIDGE ESTATES 4B | RESIDENTIAL | 2001 | 165 | 0 | 165 |
| 5320 | WINKLER | RESIDENTIAL | 1977 | 1 | 0 | 1 |
| 5318 | WOOD ESTATES | RESIDENTIAL | 2017 | 1 | 0 | 1 |
| 5360 | ZION ADDITION | RESIDENTIAL | 1977 | 20 | 4 | 24 |



LEGEND

-  CITY LIMITS
-  COUNTY LINE

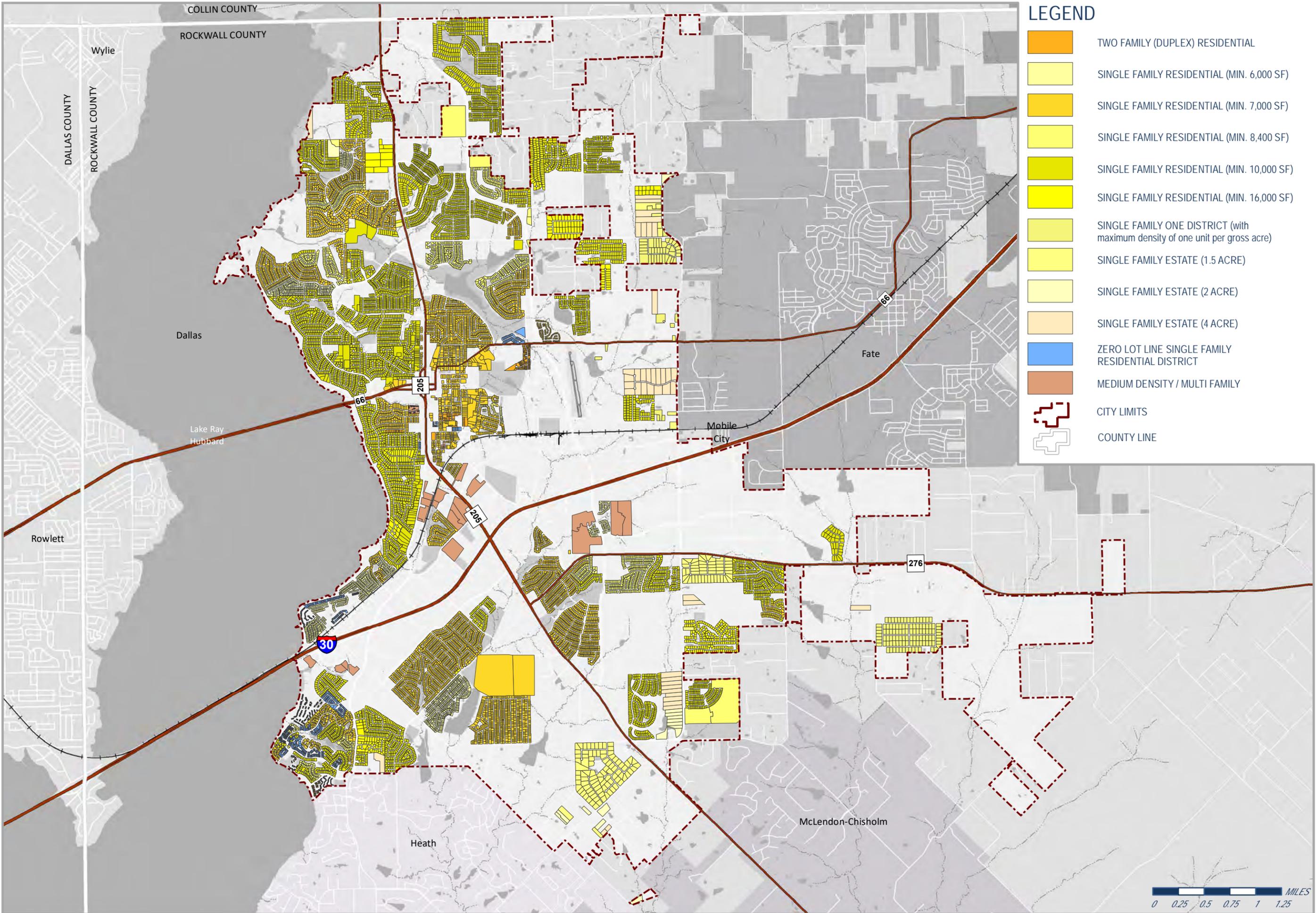
NOTE: SEE SUBDIVISION LISTING TABLE FOR NAMES OF SUBDIVISIONS

4.1

SUBDIVISION MAP

CH 4: HOUSING AND NEIGHBORHOODS





LEGEND

- TWO FAMILY (DUPLEX) RESIDENTIAL
- SINGLE FAMILY RESIDENTIAL (MIN. 6,000 SF)
- SINGLE FAMILY RESIDENTIAL (MIN. 7,000 SF)
- SINGLE FAMILY RESIDENTIAL (MIN. 8,400 SF)
- SINGLE FAMILY RESIDENTIAL (MIN. 10,000 SF)
- SINGLE FAMILY RESIDENTIAL (MIN. 16,000 SF)
- SINGLE FAMILY ONE DISTRICT (with maximum density of one unit per gross acre)
- SINGLE FAMILY ESTATE (1.5 ACRE)
- SINGLE FAMILY ESTATE (2 ACRE)
- SINGLE FAMILY ESTATE (4 ACRE)
- ZERO LOT LINE SINGLE FAMILY RESIDENTIAL DISTRICT
- MEDIUM DENSITY / MULTI FAMILY
- CITY LIMITS
- COUNTY LINE

5 NON-RESIDENTIAL DEVELOPMENT

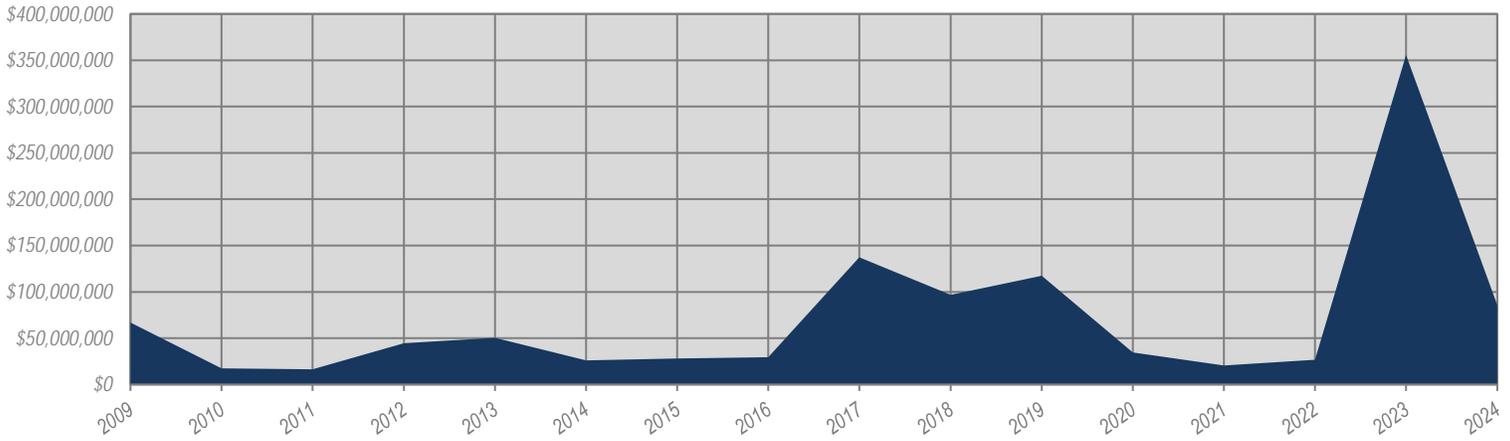




GROUNDBREAKING FOR HEB AND IKEA

PICTURED ON THIS PAGE ARE THE GROUNDBREAKINGS FOR HEB GROCERY STORE AND IKEA FURNITURE STORE. THE HEB GROCERY STORE WILL CONSIST OF ~127,000 SF OF COMMERCIAL/RETAIL SPACE. THE IKEA FURNITURE STORE WILL CONSIST OF ~161,069 SF OF COMMERCIAL/RETAIL SPACE. IN ADDITION, THE IKEA PROJECT WILL ALSO INCORPORATE APPROXIMATELY 33.215-ACRES FOR A FUTURE REGIONAL SHOPPING CENTER. THESE TWO (2) PROJECTS WILL CONTINUE THE TRANSFORMATION OF THE IH-30 CORRIDOR AND THE CITY OF ROCKWALL AS A REGIONAL COMMERCIAL CENTER.

FIGURE 5.1: VALUE OF NEW COMMERCIAL BUILDING PERMITS SUBMITTED 2009-2024
 SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DEPARTMENT



NON-RESIDENTIAL DEVELOPMENT

The growth experienced by the City of Rockwall has shaped the City's landscape, and improved job opportunities, shopping, entertainment and dining options within the community. As a result, residents from Rockwall and adjoining communities are able to work and shop locally, and Rockwall's commercial offerings have turned the City into a regional destination. While the City is considered to be a regional employment center, the majority of its residents commute to jobs in neighboring communities daily.

Since the previous *Existing Conditions Report* in 2017, the City of Rockwall has continued to experience consistent growth in non-residential development (see *Figure 5.4 and Table 5.1*). From 2021 to 2025, the City of Rockwall has added an estimated 2,424,187.00 SF of new non-residential development (see *Figure 5.3*). This translates to an estimated value of \$494,199,450.00, based on the building permits issued between 2021-2025 (see *Figure 5.1*).

TABLE 5.1: BUILDING PERMITS SUBMITTED BETWEEN 2017-2025

| YEAR | # | PERMIT VALUE | AVG. VALUE/PERMIT |
|--------|-----|------------------|-------------------|
| 2017 | 19 | \$137,134,932.00 | \$7,217,628.00 |
| 2018 | 21 | \$96,612,264.88 | \$4,600,584.04 |
| 2019 | 18 | \$17,436,687.00 | \$6,524,260.39 |
| 2020 | 17 | \$34,643,066.00 | \$2,037,827.41 |
| 2021 | 10 | \$20,551,200.00 | \$2,055,120.00 |
| 2022 | 16 | \$26,603,450.00 | \$1,662,715.63 |
| 2023 | 28 | \$355,708,800.00 | \$12,703,885.71 |
| 2024 | 27 | \$85,686,000.00 | \$3,173,555.56 |
| 2025 | 4 | \$5,650,000.00 | \$1,412,500.00 |
| TOTAL: | 160 | \$880,026,399.88 | \$4,598,675.19 |

NOTE: 2025 IS THROUGH MARCH

COMMERCIAL/RETAIL

According to the *2025 Shopping Center Review and Forecast* prepared by Weitzman, at the end of 2024 the City of Rockwall had 2,451,264 SF of gross retail space, with approximately 101,102 SF or 3.98% of this area available for lease¹. This vacancy was far below the average vacancy rate in the Dallas submarket, which was 4.87%. The majority of this commercial/retail square footage is located within the IH-30 corridor, which serves as the primary retail corridor for the City; however, both the downtown and Harbor District have made significant contributions to the addition of retail and restaurant square footage and sales tax within the City (see *Figure 5.2 and Map 5.1*). In addition, the frontage along the Ridge Road [FM-740] and Goliad Street [SH-205] have primarily developed with commercial/retail businesses. It should be noted, that the IH-30 corridor only has 25 vacant parcels of land, consisting of 170.27-acres or ~8,597.00 linear feet of frontage, remaining of undeveloped land.

TRADE AREA

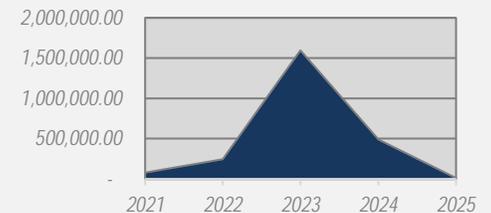
Some of the growth in the City's commercial/retail sector can be attributed to its large trade area (see *Map 5.2*). Trade area is considered to be the geographic area from which a community generates the majority of its customers. The trade areas depicted in *Map 5.2* are the result of a study by the Geographic Information Systems (GIS) Division of the City and are based on an aggregate of the Huff Model, Gravity Model and a drive-time analysis. This trade area map shows Rockwall's *Convenience and Shopping Trade Areas*. The *Convenience Trade Area* is assumed to be the area in which people will drive for convenience goods (i.e. groceries, gasoline, etc.) [and is largely

FIGURE 5.2: COMMERCIAL DEVELOPMENT WITHIN THE CITY OF ROCKWALL
 KEY: BLUE = NON-RETAIL COMMERCIAL; GREEN = COMMERCIAL/RETAIL/OFFICE



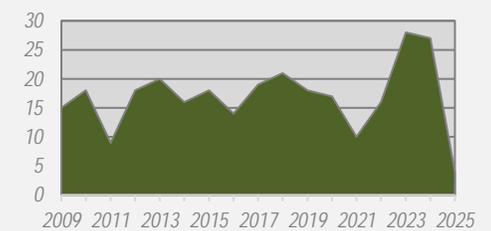
SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 5.3: TOTAL SF OF NON-RESIDENTIAL BUILDING PERMITS, 2021-2025



NOTE: 2025 IS THROUGH MARCH.
 SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.4: NUMBER OF NON-RESIDENTIAL BUILDING PERMITS, 2009-2025



NOTE: 2025 IS THROUGH MARCH.
 SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

based on ease of access to these types of products]. The City's Shopping Trade Area is a trade area for larger purchases (i.e. clothing, furniture, appliances, etc.). As depicted in the map, the City's trade area extends farther east than west. This is due to two (2) factors: [1] the density of stores located west of Rockwall in the DFW Metroplex, and [2] Lake Ray Hubbard, which creates a natural barrier west of Rockwall. This also creates a benefit for the City, as people are less likely to cross the lake to travel west of Rockwall to shop. Not depicted on this map is the City's Wide Trade Area, which extends beyond the City of Greenville to the northeast and takes in the City of Terrell to the south (see Map 5.3 to view the wide trade area).

SALES TAX

The combined sales tax rate for the City of Rockwall is 8.25%. This total is a combination of the State of Texas' tax rate of 6.25% and the City's tax rate of 2.00%. Of the City's 2.00%, 1.50% is allocated to the City's general fund and 0.50% is allocated to the Rockwall Economic Development Corporation (REDC).

As was stated in the *Population and Demographics* chapter of this document, a study by smartasset™ -- a personal finance and technology company -- ranked Rockwall County's Purchasing Power Index based on Cost of Living as third in the State of Texas and 11th nationally in 2024. Another study by smartasset™ ranked Rockwall County's Purchasing Power Index based on Salary as first in the State of Texas and 16th nationally in 2025³. These studies support the sales tax analysis performed by the City that showed that of comparable cities, Rockwall had the highest per capita sales tax at \$533.26. This is \$192.85 higher than the \$340.41 per capita sales tax reported as part of the 2017 Existing Conditions Report (see Table 5.2 and Table 5.3), and shows that Rockwall continues to be a regional shopping destination.

TABLE 5.2: PER CAPITA SALES TAX, JANUARY 1, 2024

| CITY | POP | SALES TAX | PER CAPITA |
|-----------------|---------------|------------------------|-----------------|
| ALLEN | 106,009 | \$32,488,068.00 | \$306.47 |
| FLOWER MOUND | 80,707 | \$37,844,425.00 | \$468.91 |
| ROWLETT | 66,711 | \$9,808,807.00 | \$147.03 |
| WYLIE | 62,171 | \$9,980,434.00 | \$160.53 |
| ROCKWALL | 52,882 | \$28,200,000.00 | \$533.26 |
| AVERAGE | 73,696 | \$23,664,346.80 | \$323.24 |

SOURCE: CITY OF ROCKWALL FINANCE DEPARTMENT

TABLE 5.3: CHANGE IN PER CAPITA SALES TAX, 2015-2024

| CITY | 2015 | 2024 | CHANGE |
|-----------------|-----------------|-----------------|---------------|
| ALLEN | \$193.70 | \$306.47 | 58.22% |
| FLOWER MOUND | \$145.91 | \$468.91 | 221.37% |
| ROWLETT | \$101.87 | \$147.03 | 44.34% |
| WYLIE | \$86.93 | \$160.53 | 84.67% |
| ROCKWALL | \$340.41 | \$533.26 | 56.65% |
| AVERAGE | \$173.76 | \$323.24 | 86.02% |

SOURCE: CITY OF ROCKWALL FINANCE DEPARTMENT

BUILDING PERMITS

Looking at building permits issued for commercial/retail since 2021, the number of permits has fluctuated between two (2) and nine (9) permits per year, but the City has seen an increasing trend in commercial/retail permitting from 2021-2024 (see Figure 5.5). As opposed to office and industrial building permits -- which saw their greatest increase in permitting numbers and values in 2023 -- commercial/retail saw the biggest increase in permitting numbers and values in 2024. This was due in large part to several projects being submitted, including both the DuWest Retail Shell buildings, the McDonalds restaurant, an HTeaO restaurant, a daycare, carwash, and a new 7/11 convenience store and gas station; however, the biggest boost to permitting values during this time period was the issuance of the building permit for HEB grocery store, which had an estimated permit value of \$32,058,367.00 (see Figure 5.6).

COMMERCIAL/NON-RETAIL DEVELOPMENT

Looking at commercial/non-retail development, Rockwall has seen positive trends in the amount of office and industrial development being permitted during 2021-2024 (see Figures 5.7, 5.8, 5.9 & 5.10).

INDUSTRIAL

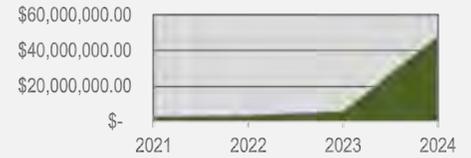
With the exception of 2024, industrial building permits have seen an increasing trend from 2021-2023 (see Figure 5.7). During this time period, the issued permits increased from four (4) building permits to seven (7) building permits. The seven (7) building permits issued in 2023 also equated to the largest value in permits at \$58,200,000.00. This included permits for a number of large industrial projects, including a new distribution center for SPR Packaging, two (2) building permits for the Rayburn Electric Cooperative's corporate campus, and Interstate Classic Cars. Despite 2023 having the highest overall permitting value, 2022 had the highest average permit value at

FIGURE 5.5: NUMBER OF COMMERCIAL BUILDING PERMITS ISSUED, 2021-2024



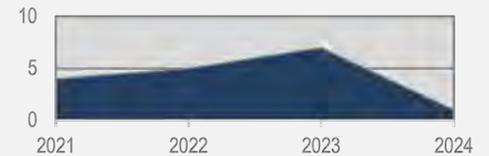
SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.6: VALUE OF COMMERCIAL BUILDING PERMITS ISSUED, 2021-2024



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.7: NUMBER OF INDUSTRIAL BUILDING PERMITS ISSUED, 2021-2025



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.8: VALUE OF INDUSTRIAL BUILDING PERMITS ISSUED, 2021-2024



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.9: NUMBER OF OFFICE BUILDING PERMITS ISSUED, 2021-2025



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

FIGURE 5.10: VALUE OF OFFICE BUILDING PERMITS ISSUED, 2021-2024



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

\$10,580,330.40 per permit. During this year the City issued five (5) building permits with an estimated value of \$52,901,652.00. This large value was due in part to three (3) permits that were issued for large distribution centers (i.e. *STREAM's Distribution Center along Corporate Crossing and Seefried's two [2] large Distribution Centers along IH-30*). These projects alone accounted for \$44,801,652.00 of the total \$52,901,652.00 in permitting value issued in 2022 (see *Figure 5.8*).

OFFICE

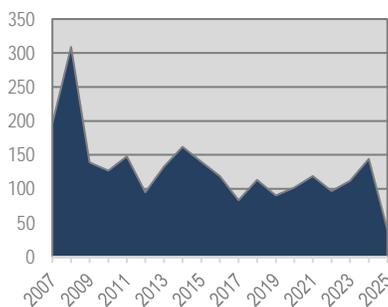
Despite 2024 being a down year for the permitting of office buildings with only two (2) building permits being issued, the City of Rockwall has seen a steady number of permits being issued for office from 2021-2023 (see *Figure 5.9*). In 2023 -- like the industrial building permit -- the City saw the largest number of office building permits being submitted at seven (7) permits, and the largest permitting value being recorded at \$17,703,839.00 with an average permit value of \$2,529,119.86 (see *Figure 5.10*). The majority of these permits were single tenant medical office buildings (e.g. *Helping Hands, Capps Orthodontics, and the Pet Doctor Veterinarian Office*).

CERTIFICATES OF OCCUPANCY

A Certificate of Occupancy (CO) is a permit issued by a local government certifying that a proposed land use is permitted on a property, the building or lease space on that property complies with all applicable building codes, and the building is in suitable condition for occupancy. CO's are a good indication of how many new businesses are coming into a community on an annual basis. The City of Rockwall has issued 1,159 CO's from 2015 to present. A breakdown of the number of CO's issued by year is depicted in *Figure 5.11* below.

FIGURE 5.11: CERTIFICATES OF OCCUPANCY (CO) ISSUED, 2007-2025

NOTE: 2025 IS THROUGH MARCH



SOURCE: CITY OF ROCKWALL BUILDING INSPECTIONS DIVISION

LABOR FORCE AND EMPLOYMENT

Over the last 25-years (i.e. from 2000-2025), the City of Rockwall's labor force and employment has grown consistently with the gains in population experienced by the area.

LABOR FORCE

The civilian labor force, according to the *QuickFacts* page on the *US Census Bureau's* website, was 69.70% of the City's total population -- 16 years of age and older -- from 2019-2023. This would equate to an estimated 28,940 residents, based on the City's current population (i.e. 52,882 residents with approximately 41,521 residents above the age of 15). This estimated number is close to the number estimated by the *US Bureau of Labor and Statistics*, which estimates the City of Rockwall's labor force to be a total of 27,432 residents -- with 26,384 employed and 1,048 unemployed -- as of October 2024. Comparing the current estimate (i.e. 28,940 residents) to the estimated work force in 2010 of 19,639 residents and 2000 of 9,663 residents, the City's overall labor force has increased 47.36% since 2010 and 199.49% since 2000.

Looking at the composition of the City's labor force, a *Community Profile* prepared by the Rockwall Economic Development Corporation (REDC) showed that the City's labor force is above the national average in professional concentrations dealing with architecture and engineering (+127.00%), legal (+101.00%), sales (+51.00%), management (+38.00%), and business and financial operations (+5.00%)².

SALARY

The City of Rockwall's high median household income is reflective of the high salaries earned by its residents. According to the *2019-2023 America Community Survey*, the City's median household income in 2023 dollars was \$114,926.00 and its per capita income was \$51,908.00. This was slightly less than Rockwall County's median household income (i.e. \$124,917.00) and per capita income (\$54,098.00), but much higher than the median household income (\$76,292.00) and per capita income (i.e. \$39,446) for the State of Texas. In addition, the per capita income for the City of Rockwall was \$51,908.00. In addition, smartasset™ -- a personal finance and technology company - ranked the *Most Paycheck Friendly Places* for the State of Texas. This company found that Rockwall County ranked 1st in the State of Texas (i.e. 16th in the United States) in terms of its *Paycheck Friendly Score*, which

TABLE 5.3: TYPICAL ANNUAL SALARY BY OCCUPATION IN ROCKWALL COUNTY

| OCCUPATION AREA | ANNUAL SALARY 2017 | ANNUAL SALARY 2025 | % CHANGE |
|--|--------------------|--------------------|----------|
| MANAGEMENT | 101,670 | 128,150 | 26.05% |
| BUSINESS AND FINANCIAL OPERATIONS | 67,280 | 83,710 | 24.42% |
| COMPUTER AND MATHEMATICAL | 80,360 | 105,070 | 30.75% |
| ARCHITECTURE AND ENGINEERING | 80,430 | 97,760 | 21.55% |
| LIFE, PHYSICAL, AND SOCIAL SCIENCE | 57,500 | 78,790 | 37.03% |
| COMMUNITY AND SOCIAL SERVICE LEGAL | 44,040 | 54,630 | 24.05% |
| LEGAL | 77,220 | 126,130 | 63.34% |
| EDUCATION, TRAINING, AND LIBRARY | 47,820 | 61,140 | 27.85% |
| ARTS, DESIGN, ENTERTAINMENT, SPORTS, AND MEDIA | 43,810 | 65,950 | 50.54% |
| HEALTHCARE PRACTITIONERS AND TECHNICAL | 61,080 | 94,540 | 54.78% |
| HEALTHCARE SUPPORT | 24,590 | 31,370 | 27.57% |
| PROTECTIVE SERVICE | 38,020 | 53,660 | 41.14% |
| FOOD PREPARATION AND SERVING RELATED | 18,930 | 29,730 | 57.05% |
| BUILDING AND GROUNDS CLEANING/MAINTENANCE | 21,170 | 32,900 | 55.41% |
| PERSONAL CARE AND SERVICE | 18,680 | 33,700 | 80.41% |
| SALES AND RELATED | 26,300 | 50,060 | 90.34% |
| OFFICE AND ADMINISTRATIVE SUPPORT | 31,850 | 44,980 | 41.22% |
| FARMING, FISHING, & FORESTRY | 21,970 | 38,060 | 73.24% |
| CONSTRUCTION AND EXTRACTION | 36,320 | 52,260 | 43.89% |
| INSTALLATION, MAINTENANCE, AND REPAIR | 39,950 | 55,430 | 38.75% |
| PRODUCTION | 31,530 | 46,480 | 47.42% |
| TRANSPORTATION AND MATERIAL MOVING | 29,780 | 45,360 | 52.32% |

SOURCE: MIT LIVING WAGE CALCULATOR

considered the *Semi-Monthly Paycheck, Purchasing Power, Unemployment Rate, and Income Growth*. The company also found that Rockwall County ranked 1st in the State of Texas and 7th in the United States in *Purchasing Power*, 11th in Texas and 64th in the United States in *Income Growth*, and 1st in the State of Texas and 25th in the United States in *Semi-Monthly Paychecks*. These results are depicted in *Tables 5.4 – 5.6*³. It should be noted that this study (*i.e. Purchasing Power based on Salary*) is *not* the same study that is referenced in *Chapter 1, Population and Demographics*, which is a study by smartasset™ on *Purchasing Power based on Cost of Living*.

Finally, *Table 5.3* depicts the typical annual salaries for various professions in Rockwall County from 2017 and 2025. These were the results of a larger study on living wages conducted by the Massachusetts Institute of Technology⁴.

EMPLOYMENT

According to the North Central Texas Council of Government (NCTCOG), in 2019 the City of Rockwall had 42,041 jobs. This represents an increase of 42.22% or an additional 12,481 jobs over the numbers from 2014 reported with the *Existing Conditions Report* from 2017⁵. Of the 42,041 jobs, 27,698 were in a *Service Providing Industry*, 8,044 were in a *Good Producing Industry* and 6,299 were in a *Retail Industry* (see *Table 5.7*).

In 2017, it was reported that the NCTCOG estimated that the City had a day-time population of 43,024, which was greater than the population that was reported at that time (*i.e. 41,370*). This means that the City had an ER Ratio of 1.11 in 2017. An ER Ratio evaluates employment to the overall population. An ER Ratio greater than 1.00 indicates that there are more workers in a place than the total population. In Rockwall's case, it is estimated that there are 11.00% more people working in the City than living in the City. Unfortunately, the NCTCOG did not have an updated number for the City's daytime population, and staff was unable to update this figure; however, considering that the City has an estimated 42,041 jobs and the City's estimated labor force is 28,940, this number is expected to have grown since 2017, and is estimated to be at an ER Ratio of 1.12 or an increase in daytime population of 12.00%. This means that the City of Rockwall has continued to be a labor importer, which continues to suggest that the City is positioned as a regional

TABLE 5.4: SEMI-MONTHLY PAYCHECK SCORE, 2025

| STATE RANK | NATIONAL RANK | COUNTY | INCOME GROWTH | UNEMPLOYMENT RATE | SEMI-MONTHLY PAYCHECK | SEMI-MONTHLY PAYCHECK SCORE |
|------------|---------------|-----------|---------------|-------------------|-----------------------|-----------------------------|
| 1 | 25 | ROCKWALL | 19.01% | 3.60% | \$5,204.88 | 84.78 |
| 2 | 41 | COLLIN | 18.89% | 3.80% | \$4,899.50 | 78.53 |
| 3 | 53 | FORT BEND | 18.40% | 4.20% | \$4,725.38 | 74.97 |
| 4 | 63 | KENDALL | 19.20% | 3.30% | \$4,604.08 | 72.49 |

SOURCE: SMARTASSET

TABLE 5.5: PURCHASING POWER, 2025

| STATE RANK | NATIONAL RANK | COUNTY | INCOME GROWTH | UNEMPLOYMENT RATE | PURCHASING POWER | PURCHASING POWER SCORE |
|------------|---------------|----------|---------------|-------------------|------------------|------------------------|
| 1 | 7 | ROCKWALL | 19.01% | 3.60% | 3.23 | 92.25 |
| 2 | 19 | PARKER | 14.32% | 3.50% | 2.94 | 82.26 |
| 3 | 21 | CHAMBERS | 21.52% | 4.70% | 2.94 | 82.04 |
| 4 | 34 | DENTON | 21.05% | 3.70% | 2.85 | 79.02 |

SOURCE: SMARTASSET

TABLE 5.6: PAYCHECK FRIENDLINESS, 2025

| STATE RANK | NATIONAL RANK | COUNTY | INCOME GROWTH | PURCHASING POWER | SEMI-MONTHLY PAYCHECK | SEMI-MONTHLY PAYCHECK SCORE |
|------------|---------------|----------|---------------|------------------|-----------------------|-----------------------------|
| 1 | 16 | ROCKWALL | 19.01% | 3.23 | \$5,204.88 | 86.57 |
| 2 | 36 | COLLIN | 18.89% | 2.79 | \$4,899.50 | 79.45 |
| 3 | 44 | DENTON | 21.05% | 2.85 | \$4,507.71 | 76.05 |
| 4 | 47 | KENDALL | 19.20% | 2.64 | \$4,604.08 | 75.76 |

SOURCE: SMARTASSET

TABLE 5.7: EMPLOYMENT BREAKDOWN, 2014-2019

| | 2014 | | 2019 | | % CHANGE |
|----------|------------|---------|------------|---------|----------|
| | EMPLOYMENT | PERCENT | EMPLOYMENT | PERCENT | |
| GOODS | 5,701 | 19.29% | 8,044 | 19.13% | 41.10% |
| RETAIL | 4,025 | 13.62% | 6,299 | 14.98% | 56.50% |
| SERVICES | 19,834 | 67.10% | 27,698 | 65.88% | 39.65% |
| TOTAL | 29,560 | 100.00% | 42,041 | 100.00% | |

SOURCE: NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (NCTCOG)

TABLE 5.8: EMPLOYMENT BREAKDOWN, 2014-2019

| NAME | ADDRESS | TYPE | EMPLOYEES |
|--------------------------------|-----------------------|---------------------|-----------|
| ROCKWALL ISD ADMIN | 1050 WILLIAMS ST | EDUCATION | 2,224 |
| CHANNELL COMMERCIAL CORP. | 1700 JUSTIN RD | INDUSTRIAL | 467 |
| CITY HALL | 385 S GOLIAD ST | GOVERNMENT/MILITARY | 354 |
| SPR PACKAGING | 1480 JUSTIN RD | INDUSTRIAL | 350 |
| TEXAS HEALTH HOSPITAL ROCKWALL | 3150 HORIZON RD | MEDICAL | 350 |
| WAL-MART SUPER STORE | 782 E INTERSTATE 30 | COMMERCIAL | 300 |
| L3HARRIS | 1700 SCIENCE PL | COMMERCIAL | 250 |
| PRATT INDUSTRIES | 3400 DISCOVERY BLVD | INDUSTRIAL | 250 |
| LOLLICUP | 3201 CAPITAL BLVD | INDUSTRIAL | 240 |
| LAKE POINTE CHURCH | 701 E INTERSTATE 30 | RELIGIOUS | 200 |
| PEGASUS FOODS | 1635 INNOVATION DR | INDUSTRIAL | 200 |
| SPECIAL PRODUCTS | 2625 DISCOVERY BLVD | INDUSTRIAL | 200 |
| BIMBO BAKERY | 3055 DISCOVERY BLVD | INDUSTRIAL | 130 |
| HIGHLAND MEADOWS HEALTH-REHAB | 1870 S JOHN KING BLVD | MEDICAL | 104 |
| ROCKWALL CO DETENTION CTR | 950 T L TOWNSEND RD | GOVERNMENT/MILITARY | 99 |
| RAYBURN COUNTRY ELECTRIC | 950 SIDS RD | COMMERCIAL | 98 |
| GRAHAM PACKAGING PET TECH | 700 INDUSTRIAL BLVD | INDUSTRIAL | 97 |
| RTT ENGINEERED SOLUTIONS | 2975 DISCOVERY BLVD | INDUSTRIAL | 90 |
| INTERSTATE WIRE CO. | 2635 OBSERVATION TRL | INDUSTRIAL | 79 |
| LAKESIDE CHEVROLET | 2005 S GOLIAD ST | COMMERCIAL | 78 |

SOURCE: CITY OF ROCKWALL GIS DIVISION

employment center despite being a commuter city.

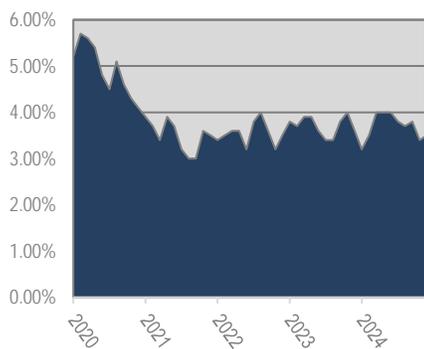
MAJOR EMPLOYERS

Currently, the largest employer in the City of Rockwall is the Rockwall Independent School District (RISD), which is estimated to employ 2,224 people. Beyond this, the largest non-institutional employers include Channell Commercial Corporation (i.e. ~467 employees), Texas Health Hospital (i.e. ~350 employees), SPR Packaging (i.e. 350 employees), Wal-Mart Super Center (i.e. 300 employees), and Pratt Industries (i.e. 250 employees). A list of the top 20 employers in the City is depicted in Table 5.8.

UNEMPLOYMENT

According to the Texas Workforce Commission, there were 217 unemployment claims -- 119 males and 98 females -- between March 2, 2025 and March 29, 2025⁶. This would be less than one (1) percent of the reported workforce population of 107,819 listed for Rockwall County. Regardless of this figure, current estimates are that the City of Rockwall had an unemployment rate of 3.50% for January 2025, and the unemployment rate averaged 3.87% for the period of December 31, 2020 to January 31, 2025 (see Figure 5.12)⁷.

FIGURE 5.12: CITY OF ROCKWALL UNEMPLOYMENT RATE, DECEMBER 31, 2020 – JANUARY 31, 2025



SOURCE: Y CHARTS

LOOKING FORWARD

The demographic and permitting information provided in this section indicates that the City of Rockwall has continued to be a regional center for employment and consumer goods (i.e. commercial/retail). In addition, the evidence suggests that the City has additional capacities for expanding its current employment centers and preserving its strategic retail centers. Moving forward the City should continue to look for regional

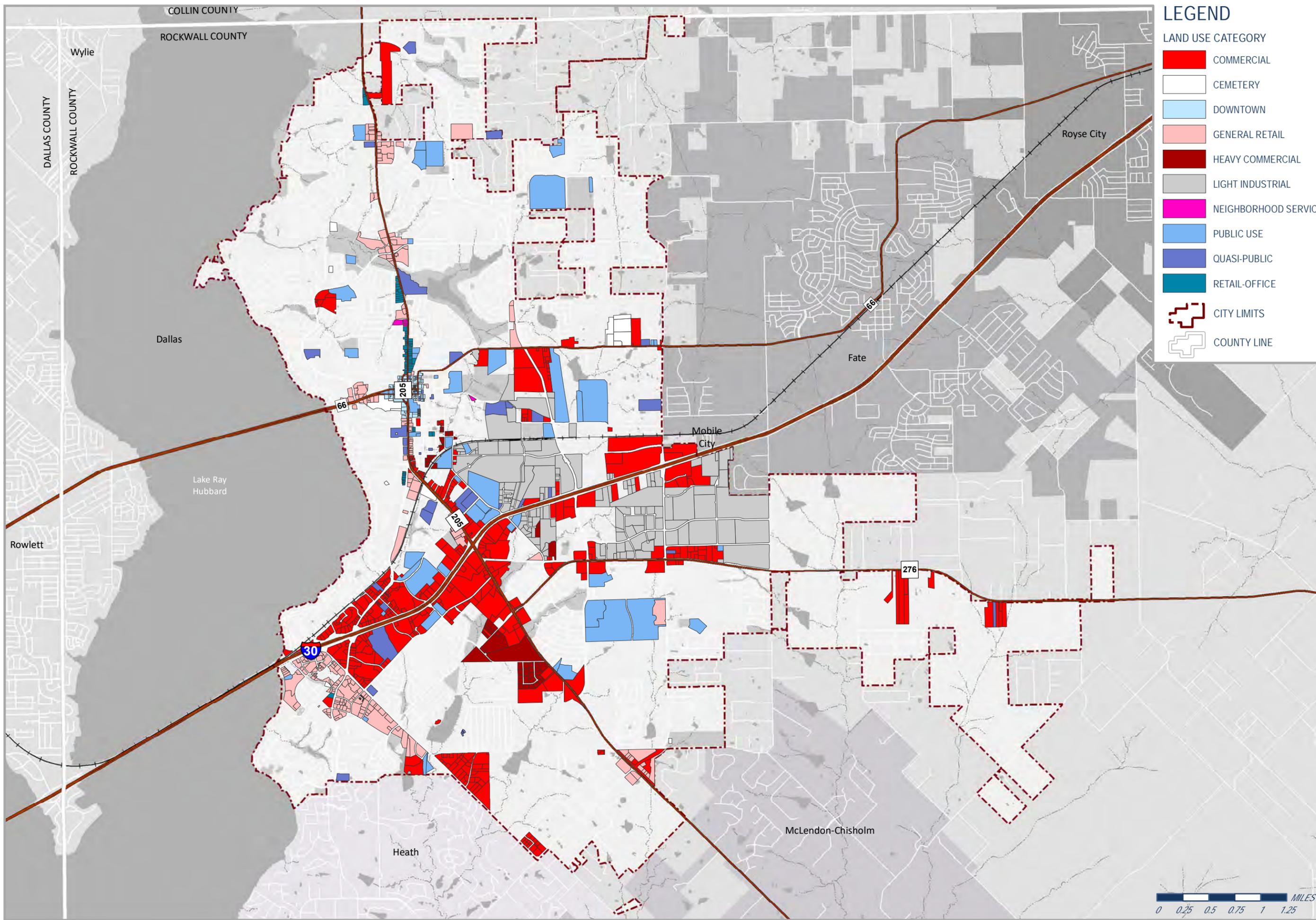
development and employment when planning for its existing vacant land.

ENDNOTES

- 1 [2025 Shopping Center Review and Forecast](#). Weitzman. Retrieved April 7, 2025.
- 2 [Community Profile](#). Rockwall Economic Development Corporation. Retrieved April 8, 2025. <https://rockwalledc.com/community-profile/>
- 3 [Most Paycheck Friendly Places](#). Smart Asset. Retrieved April 8, 2025. <https://smartasset.com/taxes/paycheck-calculator#texas/>
- 4 [Living Wage Calculator \(2017\)](#). Dr. Amy K. Glasmeier and the Massachusetts Institute of Technology. Retrieved January 31, 2017. <http://livingwage.mit.edu/>
- 5 [2019 Employment Data \(Released in 2024\)](#). North Central Texas Council of Government's Regional Data Center. Retrieved April 8, 2025.
- 6 [Unemployment Data \(January 2025\)](#). North Central Texas Council of Government's Regional Data Center. Retrieved April 8, 2025.
- 7 [Unemployment for the City of Rockwall](#). Y Charts. Retrieved April 8, 2025. https://ycharts.com/indicators/rockwall_tx_unemployment_rate#:~:text=Level%20Chart,long%20term%20average%20of%204.43%25

MAP INDEX

- (1) [MAP 5.1: NON-RESIDENTIAL LAND MAP](#)
- (2) [MAP 5.2: CONVENIENCE AND SHOPPING TRADE AREAS](#)
- (3) [MAP 5.3: WIDE TRADE AREA MAP](#)
- (4) [MAP 5.4: MAP OF THE TECHNOLOGY PARK](#)



LEGEND

LAND USE CATEGORY

- COMMERCIAL
- CEMETERY
- DOWNTOWN
- GENERAL RETAIL
- HEAVY COMMERCIAL
- LIGHT INDUSTRIAL
- NEIGHBORHOOD SERVICES
- PUBLIC USE
- QUASI-PUBLIC
- RETAIL-OFFICE
- CITY LIMITS
- COUNTY LINE

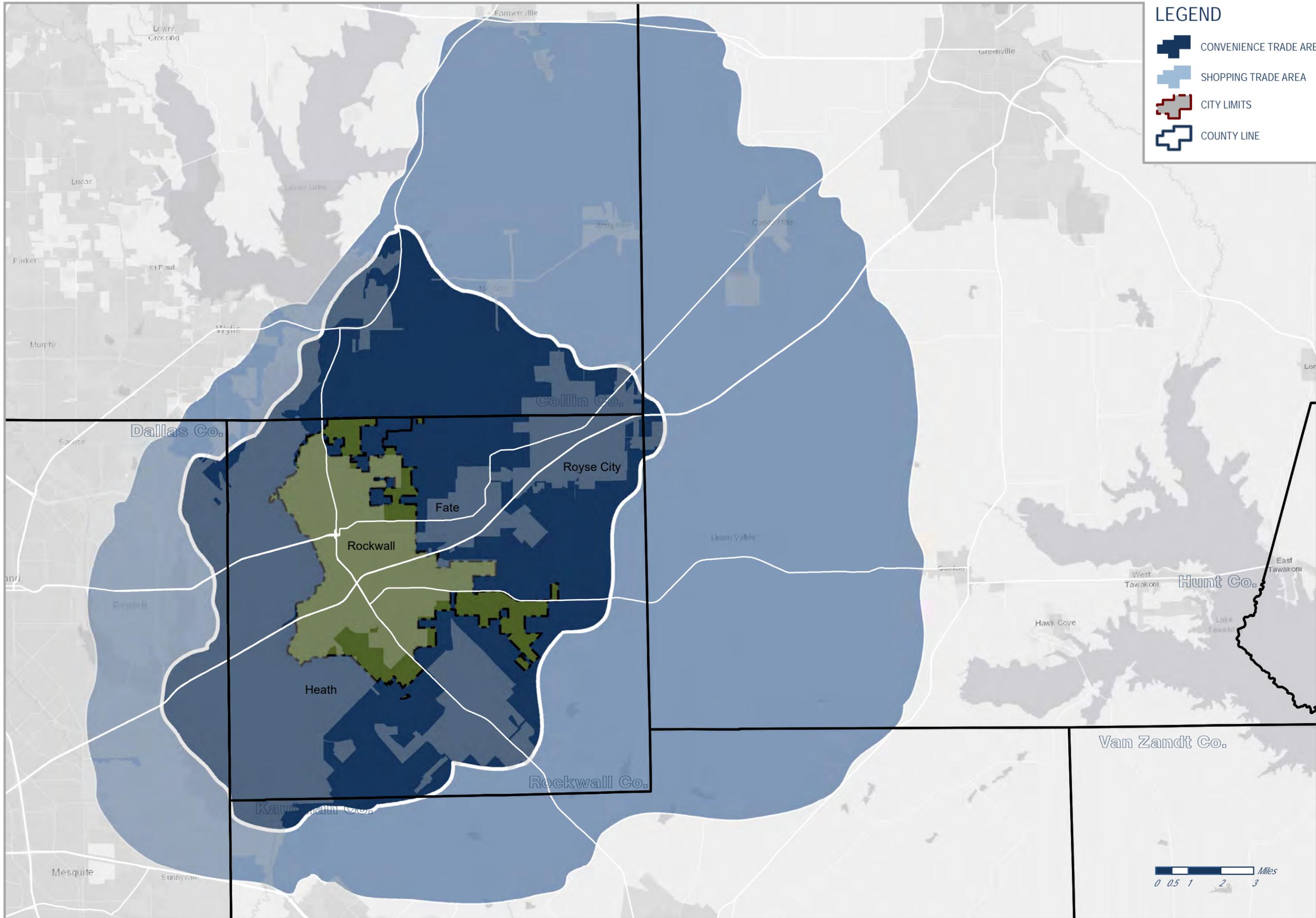


5.1

NON-RESIDENTIAL LAND

CH 5: NON-RESIDENTIAL





LEGEND

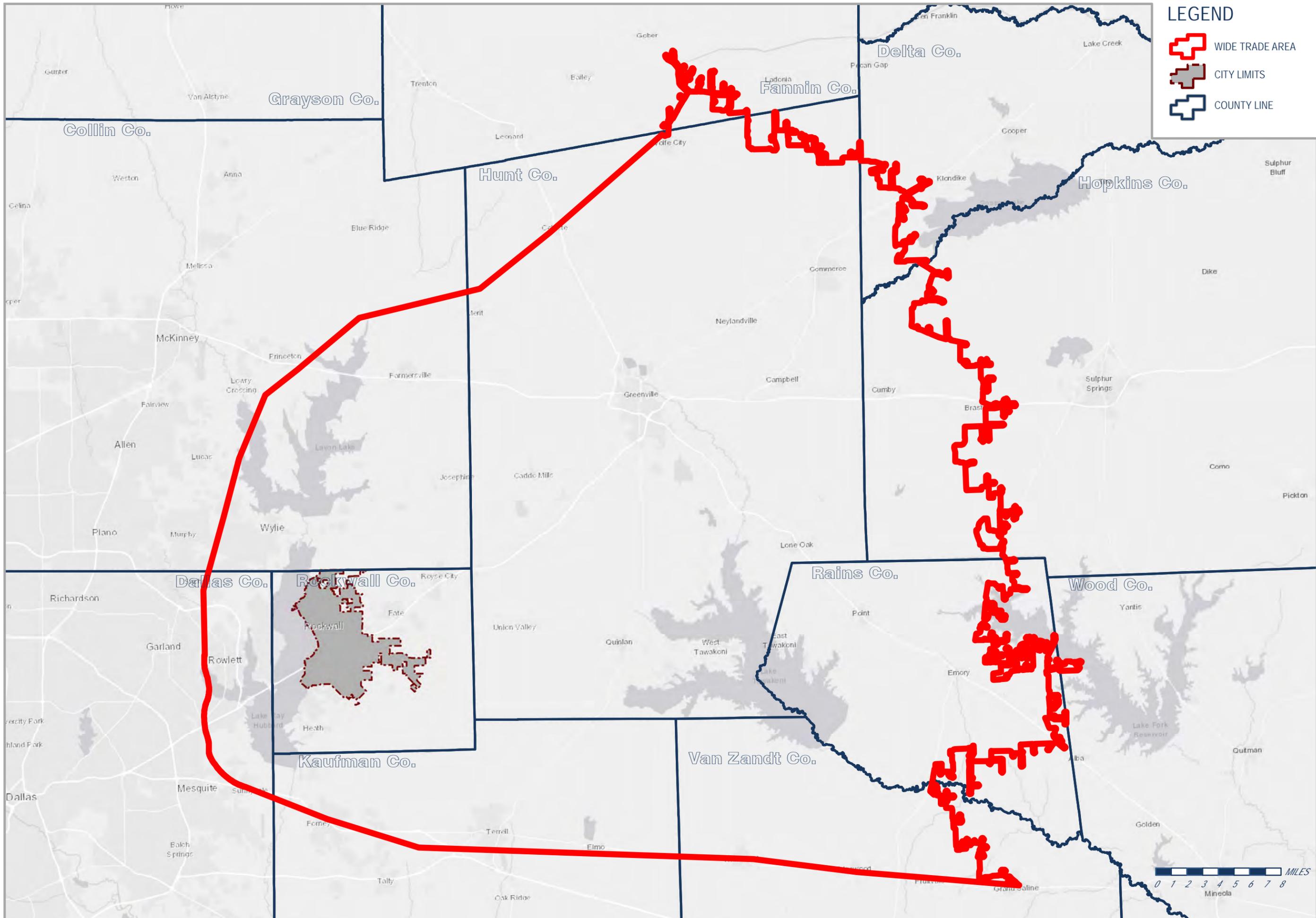
-  CONVENIENCE TRADE AREA
-  SHOPPING TRADE AREA
-  CITY LIMITS
-  COUNTY LINE

CONVENIENCE AND SHOPPING
TRADE AREA MAP

5.2

CH 5: NON-RESIDENTIAL

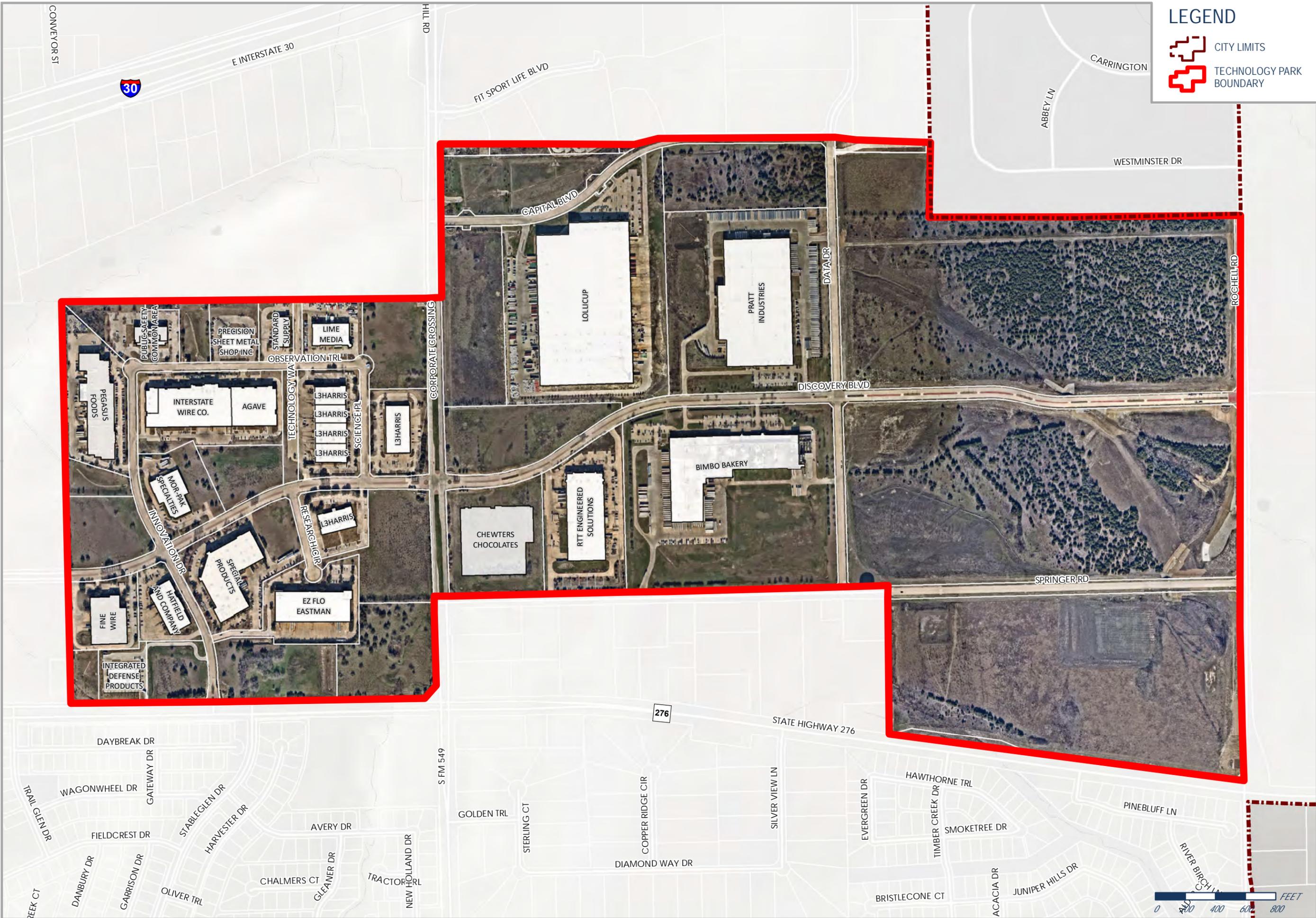




LEGEND

-  WIDE TRADE AREA
-  CITY LIMITS
-  COUNTY LINE





LEGEND

-  CITY LIMITS
-  TECHNOLOGY PARK BOUNDARY

5.4

ROCKWALL TECHNOLOGY PARK

CH 5: NON-RESIDENTIAL



6 PUBLIC FACILITIES





▲ THE ORIGINAL RENDERINGS OF THE CITY HALL BUILDING.



CITY OF ROCKWALL CITY HALL

THE THREE (3) STORY, 19,530 SF BUILDING THAT SERVES AS CITY HALL FOR THE CITY OF ROCKWALL WAS CONSTRUCTED IN 2002. IT IS SITUATED ON A FIVE (5) ACRE PARCEL OF LAND THAT -- PRIOR TO BEING CITY HALL -- WAS THE ROCKWALL PUBLIC / GRADE SCHOOL, WHICH WAS ORIGINALLY CONSTRUCTED IN 1908. THE CURRENT BUILDING WAS DESIGNED BY RON HOBBS ARCHITECTS, AND CURRENTLY HOUSES MANY OF THE CITY'S DEPARTMENTS INCLUDING DEVELOPMENT SERVICES, FINANCE, IT, UTILITY BILLING, HUMAN RESOURCES, AND THE CITY'S ADMINISTRATION.

PUBLIC FACILITIES

The following is a list of all major public facilities within the corporate boundaries of the City of Rockwall.

CITY OF ROCKWALL: CITY SERVICES

1 | ANIMAL ADOPTION CENTER

YEAR: 2007
BUILDING: 7,122 SF
ACREAGE: 64.514-Acres
ADDRESS: 1825 Airport Road
Rockwall, TX 75087



2 | CITY HALL

YEAR: 2002
BUILDING: 19,530 SF
ACREAGE: 5.0-Acres
ADDRESS: 385 S. Goliad Street
Rockwall, TX 75087



3 | CITY SERVICE CENTER

YEAR: 1984
BUILDING: 8,400 SF
ACREAGE: 8.43-Acres
ADDRESS: 1600 Airport Road
Rockwall, TX 75087



4 | COMMUNITY CENTER

YEAR: N/A
BUILDING: 2,820 SF
ACREAGE: 0.7-Acres
ADDRESS: 815 E. Washington Street
Rockwall, TX 75087



5 | THE CENTER

YEAR: 2002
BUILDING: 10,441 SF
ACREAGE: 2.0-Acres
ADDRESS: 108 E. Washington Street
Rockwall, TX 75087

6 | MUNICIPAL COURTS BUILDING

YEAR: 1999
BUILDING: 7,224 SF
ACREAGE: 2.0-Acres
ADDRESS: 2860 SH-66
Rockwall, TX 75087



7 | PARKS AND RECREATION DEPARTMENT

YEAR: N/A
BUILDING: 3,600 SF
ACREAGE: 0.122-Acres
ADDRESS: 108 E. Washington Street
Rockwall, TX 75087

8 | RALPH HALL MUNICIPAL AIRPORT

YEAR: N/A
BUILDINGS: 4,411 SF
ACREAGE: 47.89-Acres
ADDRESS: 1701 Airport Road
Rockwall, TX 75087

CITY OF ROCKWALL: FIRE SERVICES

1 | FIRE STATION #1 (BENNY GRACE MEMORIAL FIRE STATION)

YEAR: 1984
BUILDING: 8,600 SF
ACREAGE: 0.732-Acres
ADDRESS: 305 E. Boydston Avenue
 Rockwall, TX 75087



2 | FIRE STATION #2

YEAR: 2002
BUILDING: 9,400 SF
ACREAGE: 1.24-Acres
ADDRESS: 920 Rockwall Parkway
 Rockwall, TX 75032



3 | FIRE STATION #3

YEAR: 2009
BUILDING: 10,949 SF
ACREAGE: 1.512-Acres
ADDRESS: 191 E. Quail Run Road
 Rockwall, TX 75087

4 | FIRE STATION #4

YEAR: 2009
BUILDING: 12,081 SF
ACREAGE: 2.462-Acres
ADDRESS: 2915 S. Goliad Street
 Rockwall, TX 75032

CITY OF ROCKWALL: POLICE SERVICES

1 | POLICE STATION

YEAR: 1984
BUILDING: 15,904 SF
ACREAGE: 1.048-Acres
ADDRESS: 205 W. Rusk Street
 Rockwall, TX 75087



2 | PUBLIC SAFETY CENTER SOUTH

YEAR: 2024
BUILDING: 22,698 SF
ACREAGE: 4.86-Acres
ADDRESS: 2610 Observation Trail
 Rockwall, TX 75032



3 | REGIONAL FIREARMS TRAINING CENTER

YEAR: 2007
BUILDING: 16,837 SF
ACREAGE: 10.0-Acres
ADDRESS: 1815 Airport Road
 Rockwall, TX 75087



QUASI-PUBLIC FACILITIES

1 | ROCKWALL CENTRAL APPRAISAL DISTRICT

YEAR: 1998, 2024 Expansion
BUILDING: 14,881 SF
ACREAGE: 1.707-Acres
ADDRESS: 841 Justin Road
 Rockwall, TX 75087

28 YEARS

AVERAGE AGE OF CITY BUILDINGS

19 YEARS

AVERAGE AGE OF POLICE BUILDINGS

24 YEARS

AVERAGE AGE OF FIRE STATIONS

2 | **ROCKWALL ECONOMIC DEVELOPMENT CORPORATION (REDC)**

YEAR: 2005
BUILDING: 2,262 SF
ACREAGE: 4.895-Acres
ADDRESS: 2610 Observation Trail
 Rockwall, TX 75032

3 | **ROCKWALL CHAMBER OF COMMERCE**

YEAR: 1986
BUILDING: 3,606 SF
ACREAGE: 0.765-Acres
ADDRESS: 697 E. IH-30
 Rockwall, TX 75087

ROCKWALL COUNTY: GOVERNMENT BUILDINGS

1 | **ROCKWALL COUNTY COURTHOUSE**

YEAR: 2011
BUILDING: 121,208 SF
ACREAGE: 12.79-Acres
ADDRESS: 1111 E. Yellow Jacket Road
 Rockwall, TX 75087

*22,025 SF Annex Expansion starting 2025

2 | **HISTORIC COURTHOUSE**

YEAR: 1941
BUILDING: 12,000
ACREAGE: 0.918-Acres
ADDRESS: 101 E. Rusk Street
 Rockwall, TX 75087

3 | **ROCKWALL COUNTY LIBRARY**

YEAR: 2007
BUILDING: 52,621 SF
ACREAGE: 5.909-Acres
ADDRESS: 1215 Yellow Jacket Lane
 Rockwall, TX 75087

4 | **ROCKWALL COUNTY DETENTION CENTER, JUSTICE CENTER, & SHERIFF'S OFFICE**

YEAR: 1999, 2024 Expansion
BUILDING: 134,398 SF
ACREAGE: 10.193-Acres
ADDRESS: 950 & 964 T. L. Townsend Drive
 Rockwall, TX 75087

5 | **ROCKWALL COUNTY MAINTENANCE**

YEAR: 2005
BUILDING: 30,308 SF
ACREAGE: 3.35-Acres
ADDRESS: 915 Whitmore Drive
 Rockwall, TX 75087

6 | **ROCKWALL COUNTY ADULT PROBATIONS**

YEAR: 1990
BUILDING: 8,240 SF
ACREAGE: 0.918-Acres
ADDRESS: 365 W. Rusk Street
 Rockwall, TX 75087



131.656 ◀ ACRES OF CITY SERVICES
 ACRES OF FIRE STATIONS ▶ 5.946
 15.908 ◀ ACRES OF POLICE BUILDINGS
 QUASI-PUBLIC FACILITIES ▶ 7.367
 34.087 ◀ ACRES OF COUNTY SERVICES
 ACRES OF RISD FACILITIES ▶ 490.123
 5.00 ◀ ACRES OF STATE FACILITIES

ROCKWALL INDEPENDENT SCHOOL DISTRICT: SCHOOLS & OFFICES

1 | AMANDA ROCHELL ELEMENTARY SCHOOL

YEAR: 2023
BUILDING: ~76,900 SF
ACREAGE: 11.605-Acres
ADDRESS: 401 East Ralph Hall Pkwy
 Rockwall, TX 75032



2 | CELIA HAYES ELEMENTARY SCHOOL

YEAR: 2007
BUILDING: 93,932 SF
ACREAGE: 11.036-Acres
ADDRESS: 1880 Tannerson Drive
 Rockwall, TX 75087



3 | DORRIS A. JONES ELEMENTARY SCHOOL

YEAR: 2003
BUILDING: ~97,276 SF
ACREAGE: 15.428-Acres
ADDRESS: 2051 Trail Glen
 Rockwall, TX 75032



4 | GRACE HARTMAN ELEMENTARY SCHOOL

YEAR: 1980
BUILDING: ~78,150 SF
ACREAGE: 15.092-Acres
ADDRESS: 1325 Petaluma Drive
 Rockwall, TX 75087



5 | HERMAN E. UTLEY MIDDLE SCHOOL

YEAR: 2009
BUILDING: 266,277 SF
ACREAGE: 41.649-Acres
ADDRESS: 1201 T. L. Townsend Drive
 Rockwall, TX 75087



6 | HOWARD DOBBS ELEMENTARY SCHOOL

YEAR: 2017
BUILDING: ~93,281 SF
ACREAGE: 18.407-Acres
ADDRESS: 901 E. Interurban Street
 Rockwall, TX 75087

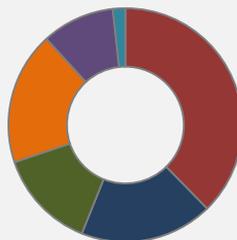
7 | J. W. WILLIAMS MIDDLE SCHOOL

YEAR: 2002, 2025 Expansion
BUILDING: 148,448 SF
ACREAGE: 25.569-Acres
ADDRESS: 625 FM-552
 Rockwall, TX 75087

8 | NEBBIE WILLIAMS ELEMENTARY SCHOOL

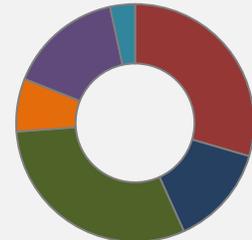
YEAR: 1995
BUILDING: 65,504 SF
ACREAGE: 11.599-Acres
ADDRESS: 350 Dalton Road
 Rockwall, TX 75087

FIGURE 6.1: SCHOOL BUILDING SF
 KEY: RED: ELEMENTARY SCHOOLS [849,806 SF]; BLUE: MIDDLE SCHOOLS [414,725 SF]; GREEN: 9TH GRADE CENTERS [310,869 SF]; ORANGE: HIGH SCHOOLS [413,908 SF]; PURPLE: SPECIALTY SCHOOLS [227,041 SF]; TEAL: SUPPORT FACILITIES [38,795 SF].



SOURCE: CITY OF ROCKWALL GIS DIVISION

FIGURE 6.2: SCHOOL BUILDING SITE ACREAGE
 KEY: RED: ELEMENTARY SCHOOLS [849,806 SF]; BLUE: MIDDLE SCHOOLS [414,725 SF]; GREEN: 9TH GRADE CENTERS [310,869 SF]; ORANGE: HIGH SCHOOLS [413,908 SF]; PURPLE: SPECIALTY SCHOOLS [227,041 SF]; TEAL: SUPPORT FACILITIES [38,795 SF].



SOURCE: CITY OF ROCKWALL GIS DIVISION

9 | OUIDA SPRINGER ELEMENTARY SCHOOL

YEAR: 2005
BUILDING: 78,596 SF
ACREAGE: 12.556-Acres
ADDRESS: 3025 Limestone Hill Lane
Rockwall, TX 75032



2

CITY OWNED AND MAINTAINED CEMETERIES

14

OUTDOOR WARNING SIRENS IN THE CITY

10 | ROCKWALL HIGH SCHOOL

YEAR: 1992
BUILDING: 413,908 SF
ACREAGE: 35.295-Acres
ADDRESS: 901 Yellow Jacket Lane
Rockwall, TX 75087



11 | ROCKWALL QUEST ACADEMY & RISD ADMINISTRATIVE OFFICES

YEAR: 2003
BUILDING: ~100,901 SF
ACREAGE: 21.326-Acres
ADDRESS: 1050 Williams Street
Rockwall, TX 75087

12 | SHARON SHANNON ELEMENTARY SCHOOL

YEAR: 2007
BUILDING: 93,932 SF
ACREAGE: 10.001-Acres
ADDRESS: 3130 Fontanna Boulevard
Rockwall, TX 75032



13 | VIRGINIA REINHARDT ELEMENTARY SCHOOL

YEAR: 1984, 2018 Expansion
BUILDING: 66,715 SF
ACREAGE: 6.98-Acres
ADDRESS: 615 Highland Drive
Rockwall, TX 75087

14 | ROCKWALL ISD BUS MAINTENANCE, STORAGE FACILITY AND BUS BARN (NORTH)

YEAR: N/A
BUILDING 1: 27,000 SF
ACREAGE: 3.810-Acres

YEAR: N/A
BUILDING 2: 4,800 SF
ACREAGE: 5.5-Acres
ADDRESS: 801 E. Washington Street
Rockwall, TX 75087



15 | ROCKWALL ISD BUS MAINTENANCE, STORAGE FACILITY AND BUS BARN (SOUTH)

YEAR: N/A
BUILDING: 6,995 SF
ACREAGE: 7.2-Acres
ADDRESS: 981 Sids Road
Rockwall, TX 75032



3

AMPHITHEATER AND OUTDOOR STAGES IN THE CITY

3

CITY OWNED BOAT RAMPS

16 | SHERRY AND PAUL HAMM ELEMENTARY SCHOOL

YEAR: 2020
BUILDING: 105,520 SF
ACREAGE: 32.66-Acres
ADDRESS: 2911 Greenway Drive
Rockwall, TX 75087

17 | ROCKWALL HIGH SCHOOL
FRESHMAN CENTER (NORTH)
YEAR: 2025
BUILDING: 155,405 SF
ACREAGE: 69.53-Acres
ADDRESS: 2850 FM 1141
Rockwall, TX 75087

18 | ROCKWALL-HEATH SCHOOL
FRESHMAN CENTER (SOUTH)
YEAR: 2025
BUILDING: 155,405 SF
ACREAGE: 80.0-Acres
ADDRESS: 2727 S. John King Blvd.
Rockwall, TX 75032

19 | DR. GENE BURTON COLLEGE AND
CAREER ACEDEMY
YEAR: 2017
BUILDING: 113,994 SF
ACREAGE: 54.88-Acres
ADDRESS: 2301 S. John King Blvd.
Rockwall, TX 75032

STATE OF TEXAS: STATE FACILITIES

1 | TEXAS DEPARTMENT OF
TRANSPORTATION: ROCKWALL
MAINTENANCE FACILITY
YEAR: N/A
BUILDING: ~17,163
ACREAGE: 5.0-Acres
ADDRESS: 981 Sids Road
Rockwall, TX 75087

PUBLIC FACILITIES

The City of Rockwall has eight (8) general service buildings, totaling 63,548 SF of building area, on 131.656-acres of land; four (4) fire stations with a total building area of 41,030 SF on 5.946-acres of land; and, three (3) police facilities, totaling 55,439 SF of building area on ~15.91-acres of land. This equates to a total of 15 buildings, 160,017 SF of building area, on 152.51-acres of land. Currently, the City does not have any plans to expand any of the existing facilities or build any new administrative or support facilities; however, the City of Rockwall is in the process of finishing a facilities study that will help create a long-term plan for City facilities.

Rockwall County currently has seven (7) facilities inside the city limits, including the Rockwall County Courthouse, which was constructed in 2011. These facilities have a total square footage of 380,840 SF on 34.078-acres. In addition, the City has three (3) quasi-public facilities, totaling 20,749 SF of building area, on 7.367-acres, and one (1) State facility operated

by the Texas Department of Transportation (TXDOT) that has an estimated 17,163 SF of building area on five (5) acres of land.

The Rockwall Independent School District (RISD) currently has 15 schools and three (3) supporting facilities inside the city limits of the City of Rockwall. Of the 15 schools, there are ten (10) elementary schools, two (2) middle schools, two (2) freshman/9th grade centers, and one (1) high school. The ten (10) elementary schools have a total building area of ~849,806 SF and are situated on 144.384-acres of land; the two (2) middle schools have a total building area of ~415,846 SF and are situated on 67.22-acres of land; the two (2) freshman centers have a total building area of ~310,869 SF and are situated on 149.53-acres of land; and the high school has a total building square footage of 413,908 SF and is situated on 35.295-acres. There are two (2) special education facilities (*i.e. the Administration/Quest Academy and the Gene Burton College and Career Academy*) that have a total building area of ~227,041 SF and are situated on ~489.143-acres of land. The supporting facilities for the RISD have a total building square footage of 38,795 SF and are situated on 16.51-acres of land. This all equates to a total building area of ~2,255,143 SF on a total land area owned and operated by the RISD of 489.143-acres that is situated within the City's corporate limits.

In total, there are 688.098-acres of public facility land (*i.e. school, City, County, and State land*) with a total of 2,833,912 SF of building improvement. *Figure 6.3* shows a breakdown of all public facilities in the City of Rockwall. A map showing the location of all public facilities is depicted in *Map 6.3*. As a note, this map *only* shows land with public facilities and does *not* show all publicly owned land (*i.e. vacant lots, open space and parks are not included*).

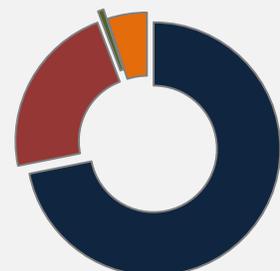
MAP INDEX

(1) MAP 6.1: MAP OF PUBLIC FACILITIES

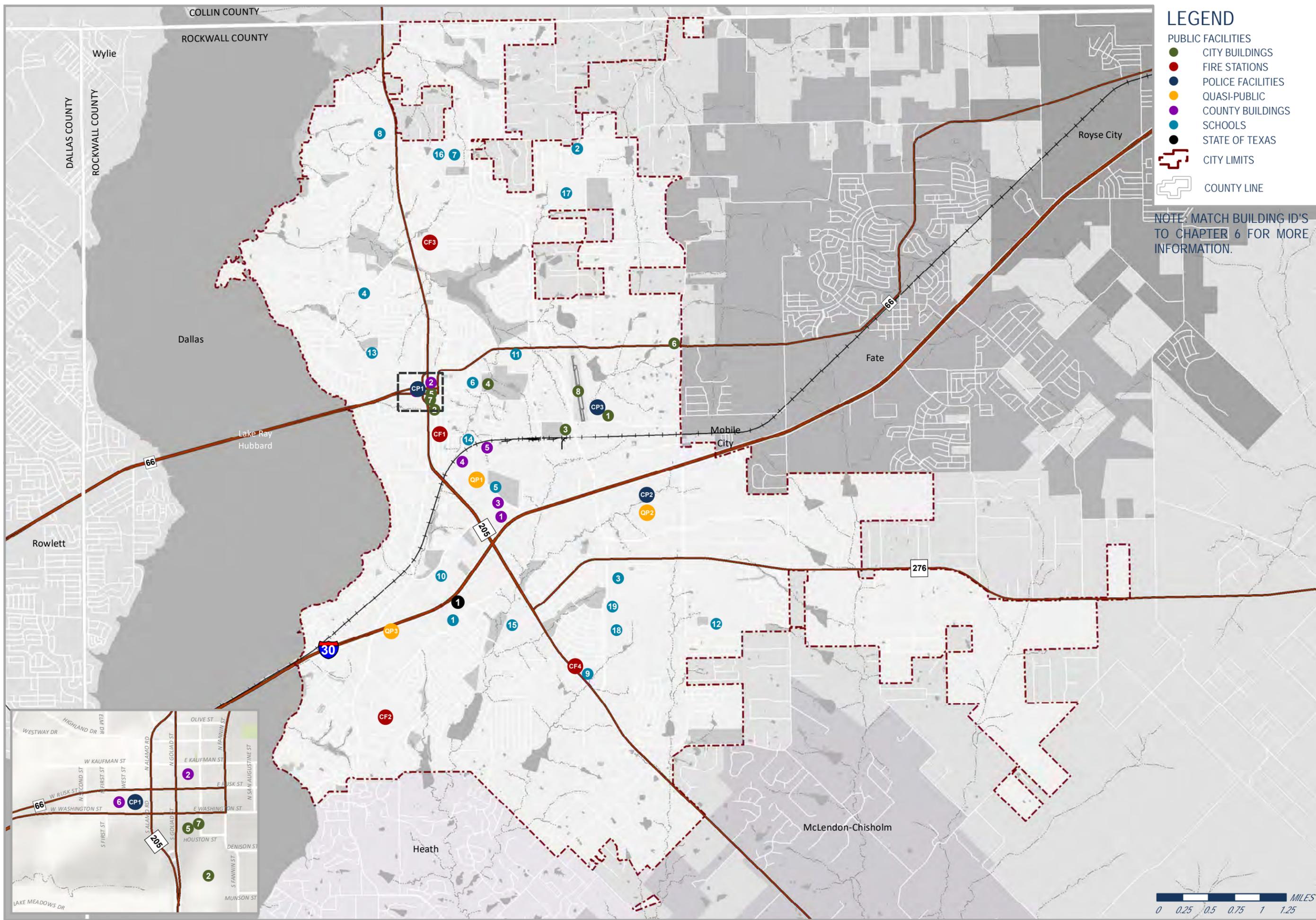


FIGURE 6.3: PUBLIC FACILITY LAND WITHIN THE CITY OF ROCKWALL

KEY: RED: ROCKWALL INDEPENDENT SCHOOL DISTRICT (RISD) [489.14-ACRES]; BLUE: CITY OF ROCKWALL [152.51-ACRES]; GREEN: STATE OF TEXAS [5.0-ACRES]; ORANGE: ROCKWALL COUNTY [34.078-ACRES]



SOURCE: CITY OF ROCKWALL GIS DIVISION



LEGEND

- PUBLIC FACILITIES
- CITY BUILDINGS
- FIRE STATIONS
- POLICE FACILITIES
- QUASI-PUBLIC
- COUNTY BUILDINGS
- SCHOOLS
- STATE OF TEXAS
- ▬ CITY LIMITS
- ▬ COUNTY LINE

NOTE: MATCH BUILDING ID'S TO CHAPTER 6 FOR MORE INFORMATION.

6.1

PUBLIC FACILITIES

CH.6: PUBLIC FACILITIES



7 PARKS, TRAILS, OPEN SPACE, AND EVENTS





HERO'S MEMORIAL PARK

THE CITY OF ROCKWALL -- *IN PARTNERSHIP WITH FIRST HELP* -- DEDICATED THE HERO'S MEMORIAL PARK ON SEPTEMBER 21, 2024. THE MEMORIAL IS DEDICATED TO THE MEMORY OF THE VETERANS AND FIRST RESPONDERS WHO DIED AS A RESULT OF SUICIDE, AND IS THE FIRST MONUMENT RECOGNIZING THESE HEROES IN THE NATION. THE MEMORIAL IS LOCATED ADJACENT TO THE PUBLIC BOAT LAUNCH ALONG W. RUSK STREET (SH-66).

PARKS

The City of Rockwall has over ~708.13-acres of public parks, 310.38-acres of future parks, and 74.39-acres of private parks. The City's public parks consist of 332.54-acres of *Community Parks*, 87.95-acres of *Greenbelts and Greenways*, 14.24-acres of *Mini-Parks*, 149.14-acres of *Neighborhood Parks*, 17.24-acres of *School Parks*, 26.28-acres of *Sports Complexes*, 73.40-acres of *Special Use Open Space*, and 7.34-acres of *Specialty Parks*. These are broken down as follows:

COMMUNITY PARKS

1 | ALMA WILLIAMS PARK

ADDRESS: North Country Lane/FM-1141
HOURS: TBD
ACREAGE: 44.96-Acres

2 | HARRY MYERS PARK

ADDRESS: 815 E. Washington Street
HOURS: 5:00 AM – 11:00 PM
ACREAGE: 66.99-Acres
FEATURES:

Sports Fields, Barbecue Grills, Disc Golf Course, Dog Park, Drinking Fountains, Fishing Pier, Natural Open Space, Pavilions, Picnic Stations, Playground Structure, Swings, Pond, Restrooms, Spray Grounds, Trails, Pool, and Pickleball Courts.

3 | THE PARK AT PHELPS LAKE

ADDRESS: 1325 Memorial Drive
HOURS: 5:00 AM – 11:00 PM
ACREAGE: 168.21-Acres
FEATURES:

Drinking Fountains, Fishing Pier, Kayak Launch, Natural Open Space, Pond, and Trails.

4 | BEN A. KLUTTS, SR. PARK

ADDRESS: FM-549/FM-1139
HOURS: TBD
ACREAGE: 52.38-Acres

GREENBELTS AND GREENWAYS

1 | HIGHLAND MEADOWS

ADDRESS: Mims Road/Sids Road
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 5.90-Acres

2 | LAGO VISTA

ADDRESS: Summer Lee Drive/Lakefront Trail
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 3.02-Acres

3 | LAKEVIEW SUMMIT

ADDRESS: N. Lakeshore Drive/Sutter Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 11.82-Acres

4 | LYNDEN PARK

ADDRESS: Tubbs Road/Glenhurst Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 12.06-Acres

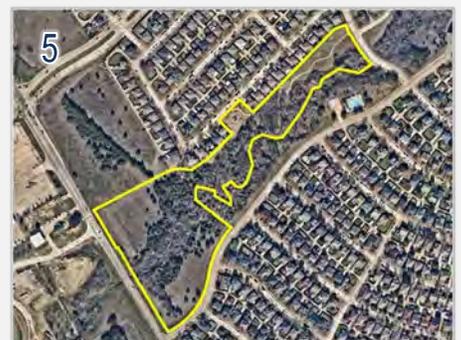
5 | MEADOW CREEK-FOX WOOD

ADDRESS: 1441 Foxwood Lane
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 16.78-Acres



COMMUNITY PARKS: A park that is larger than a neighborhood park, that provides service to several neighborhoods or specific sections of the community, and generally provides recreational opportunities not feasible in a neighborhood park.

GREENBELTS AND GREENWAYS: These areas are generally natural corridors along creeks/flood prone areas, which are typically undevelopable. These areas are used as a component to linking trails or other park facilities.



6 | MEADOW CREEK-MADISON

ADDRESS: 1430 Madison Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 17.27-Acres
FEATURES:

Playground and Trails

7 | TIMBERCREEK

ADDRESS: Timber Creek Subdivision
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 8.27-Acres

8 | WATERSTONE

ADDRESS: Murphy Drive/Damascus Road
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 12.83-Acres

MINI-PARKS

1 | CLARK STREET PARK

ADDRESS: 308 S. Clark Street
HOURS: 5:00 AM to 10:00 PM
ACREAGE: 1.32-Acres

2 | GREENES LAKE PARK

ADDRESS: 219 Cornelius Road
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 3.49-Acres
FEATURES:

Playgrounds and Trails

3 | TED CAIN'S CORNER PARK

ADDRESS: 203 N. San Jacinto Street
HOURS: 5:00 AM to 10:00 PM
ACREAGE: 0.05-Acres

4 | GLORIA WILLIAMS PARK

ADDRESS: 807 Davy Crockett
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 0.48-Acres
FEATURES:

Basketball Court, Picnic Stations, Playground Structure, Swings, and Swimming Pool.

5 | LOFLAND PARK

ADDRESS: 407 Kernodle Street
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 1.21-Acres
FEATURES:

Barbeque Grills, Pavilion, Picnic Stations, Playgrounds, Swings, and Trails.

6 | HENRY M. CHANDLER PARK

ADDRESS: FM-740/Henry M. Chandler Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 4.20-Acres
FEATURES:

Soccer Fields.

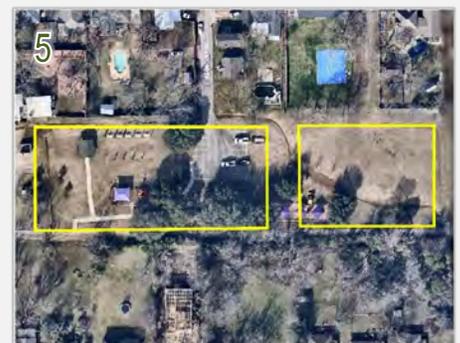
7 | JEWEL PARK

ADDRESS: 7123 Hunt Lane
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 3.49-Acres
FEATURES:

Picnic Stations, Playgrounds, and Swings.



MINI-PARKS: A *Mini-Park* is typically smaller than a *Neighborhood Park* and only serves a population of 500-2,500 residents. These types of parks typically have limited amenities (e.g. *fountains, playgrounds, benches, etc.*).



NEIGHBORHOOD PARKS

1 | THE PARK AT BREEZY HILL

ADDRESS: Breezy Hill Subdivision
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 61.97-Acres
FEATURES:

Barbeque Grills, Drinking Fountains, Fishing Pier, Natural Open Space, Playgrounds, Swings, Pond, and Trails.

2 | EMERALD BAY PARK

ADDRESS: 1816 Emerald Bay
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 11.85-Acres
FEATURES:

Barbeque Grills, Drinking Fountain, Fishing Pier, Picnic Stations, Playgrounds, Swings, Pond, and Trails.

3 | THE PARK AT FOX CHASE

ADDRESS: 4475 Tubbs Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 9.77-Acres
FEATURES:

Basketball Court, Barbeque Grills, Drinking Fountains, Fishing Pier, Pavilion, Picnic Stations, Playgrounds, Swings, Pond, Spray Grounds, and Trails.

4 | THE PARK AT HICKORY RIDGE

ADDRESS: FM-740/Henry M. Chandler
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 25.45-Acres
FEATURES:

Basketball Court, Drinking Fountains, Natural Open Space, Pavilion, Picnic Stations, Playgrounds, Pond, Spray Grounds, and Trails.

5 | WINDMILL RIDGE PARK

ADDRESS: 139 Westwood Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 8.48-Acres
FEATURES:

Barbeque Grills, Drinking Fountain, Picnic Stations, and Playgrounds.

6 | THE PARK AT TERRACINA

ADDRESS: 2715 Guadalupe Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 8.22-Acres
FEATURES:

Drinking Fountains, Pavilion, Playgrounds, and Trails.

7 | THE PARK AT STONE CREEK

ADDRESS: Featherstone Drive & Hays Road
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 11.50-Acres
FEATURES:

Barbeque Grills, Drinking Fountains, Fishing Pier, Natural Open Space, Pavilion, Picnic Stations, Playgrounds, Swings, Pond, and Trails.

8 | SHORES PARK

ADDRESS: 2302 Shores Boulevard
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 11.90-Acres
FEATURES:

Multipurpose Open Space, Basketball Court, Natural Open Space, Drinking Fountains,



NEIGHBORHOOD PARKS: A highly amenitized park that generally consists of a minimum of ten (10) acres and is situated in an existing subdivision. These are typically parks meant to service properties within a ½-mile distance or a population up to 10,000 residents.

| | | |
|--------|-----------------------------------|-------|
| 332.54 | ◀ ACRES OF COMMUNITY PARKS | |
| | ACRES OF PARK/SCHOOL ▶ | 17.24 |
| 87.95 | ◀ ACRES OF GREENBELTS/GREEN WAYS | |
| | ACRES OF SPORTS COMPLEX ▶ | 26.28 |
| 14.24 | ◀ ACRES OF MINI-PARKS | |
| | ACRES OF SPECIALTY PARKS ▶ | 73.40 |
| 149.14 | ◀ ACRES OF NEIGHBORHOOD PARKS | |
| | ACRES OF SPECIAL USE OPEN SPACE ▶ | 7.34 |

Picnic Stations, BBQ Grill, Benches, Playground Structure, Trails, Swings and Pavilion

PARK/SCHOOL

1 | FONTANNA PARK

ADDRESS: 300 Fontanna Boulevard
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 5.23-Acres

2 | DALTON RANCH PARK

ADDRESS: FM-552/Tannerson Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 2.01-Acres

3 | NORTHSHORE PARK

ADDRESS: 609 Highland Drive
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 10.00-Acres
FEATURES:

Basketball Court, Natural Open Space, Benches, Picnic Stations, Playground Structure, and Swings

SPORTS COMPLEXES

1 | LEON TUTTLE ATHLETIC COMPLEX

ADDRESS: 1600 Airport Road
HOURS: 5:00 AM – 11:00 PM
ACREAGE: 18.65-Acres
FEATURES: Eight (8) Baseball Fields

2 | YELLOW JACKET PARK

ADDRESS: Yellow Jacket Lane
HOURS: 5:00 AM – 10:00 PM
ACREAGE: 7.63-Acres
FEATURES:

Three (3) Baseball Fields, Playground with Shade Canopies, Music Play, Interactive Panels and Classic Playground Elements

SPECIALTY PARKS

1 | SH-66 BOAT RAMP/HERO'S MEMORIAL PARK

ADDRESS: 1120 W. Rusk Street
HOURS: 5:00 AM to 10:00 PM
ACREAGE: 0.52-Acres
FEATURES:

Benches, Natural Open Space, Drinking Fountains, Trails, a Pavilion, and three (3) Boat Ramps

2 | SQUABBLE CREEK MOUNTAIN BIKE TRAIL

ADDRESS: 1401 Dickson Lane
HOURS: 5:00 AM to 10:00 PM
ACREAGE: 48.16-Acres
FEATURES:

Trails, Picnic Table, Restrooms and Drinking Fountains

3 | PETTINGER NATURE PRESERVE

ADDRESS: 450 W. Quail Run Road
HOURS: 5:00 AM to 10:00 PM
ACREAGE: 24.72-Acres
FEATURES:

Nature Trails and Learning Stations



PARK/SCHOOL: The *Park/School* classification is typically used to describe a park that is directly adjacent to a public school. These parks provide equipment that is appropriate for school-aged children and include a large amount of open space.

SPORTS COMPLEXES: A *Sports Complex* typically provides both park facilities and ball fields/sports fields with bleachers. They are intended to be 40-acres to 150-acres and serve the entire City.



SPECIALTY PARKS: These parks are generally identified by a single use that falls into one of three categories: (1) Historical/Cultural/Social, (2) Recreational Facilities, and (3) Outdoor Recreation Facilities. These single uses include things like Marinas, Community Centers, Golf Courses, Stadiums, historical sites, etc.

SPECIAL USE OPEN SPACE: Open space that involves special amenities not found in other parks or open space. These are usually associated with adjacent developments and are used to host special City events.

SPECIAL USE OPEN SPACE

1 | THE HARBOR

ADDRESS: 2059 Summer Lee Drive
HOURS: 8:00 AM to 10:00 PM
ACREAGE: 7.34-Acres
FEATURES:

Shopping, Dining, Scenic Views, and Rental Facilities

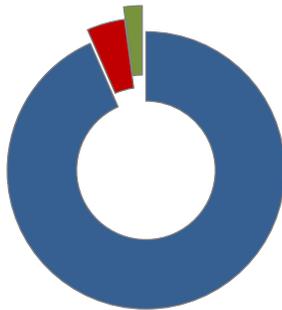


TRAILS

As was stated in *Chapter 2: Transportation and Infrastructure* of this document, the City currently has 259.06 linear miles (i.e. 1,367,836.80 linear feet) of on-street sidewalks, 12.28 linear miles (i.e. 64,838.40 linear feet) of park trails, and 5.95 linear miles (i.e. 31,416.00 linear feet) of mountain bike trails (see *Map 7.1 and Figure 7.1*).

FIGURE 7.1: SIDEWALKS AND TRAILS

KEY: BLUE = SIDEWALKS (259.06 LM); RED = PARK TRAILS (12.28 LM); GREEN = MOUNTAIN BIKE TRAILS (5.95 LM)



SOURCE: CITY OF ROCKWALL GIS DIVISION

JOHN KING BOULEVARD CONCEPT PLAN

A major component of the City's trail system is the proposed hike/bike trails that will run on either side of John King Boulevard. This includes 13.87 linear miles (i.e. 6.93 linear miles on each side of the road) or 73,223.00 linear feet of trails. Currently, the trail is 38.08% constructed with 3.94 linear miles of ten (10) foot concrete trail and 1.34 linear miles of eight (8) foot concrete trail. There are about 8.13 linear miles (i.e. 42,943.00 linear feet) left to be developed and 0.45 linear miles (i.e. 2,396.00 linear feet) of four (4) foot concrete sidewalk that needs to be

redeveloped. Also, incorporated in this plan are various trail rest stops, trailheads (which incorporate on-site parking and access to the trail), and city enhancement features intended to create a unique corridor that is central to the community (see *Map 7.4*). *Figure 7.2 through Figure 7.9* show some of the already constructed trailheads and trail rest stops along John King Boulevard.

FIGURES 7.2 & 7.3: TRAILHEAD AT THE BREEZY HILL SUBDIVISION



FIGURE 7.4: TRAIL REST STOP AT THE QUAIL HOLLOW SUBDIVISION



FIGURE 7.5: TRAIL REST STOP AT THE SADDLE STAR SUBDIVISION



FIGURE 7.6 & 7.7: TRAIL REST STOP/PLAYGROUND AT THE GIDEON GROVE SUBDIVISION



FIGURE 7.8 & 7.9: TRAILHEAD/REST STOP AT THE ROCKWALL DOWNES SUBDIVISION

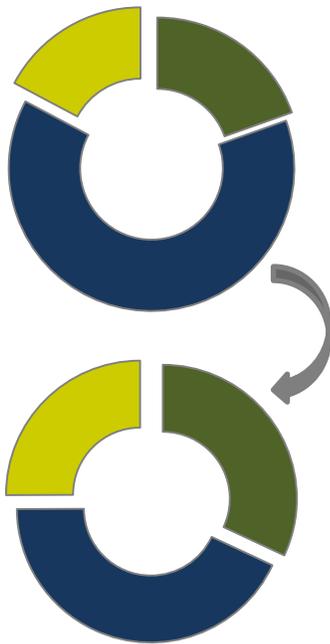


OPEN SPACE

There are currently 3,445.46-acres of open space and floodplain within the City (see *Map 7.3*). This represents an increase of 214.63-acres or an increase of 6.64% in open space and floodplain between 2017-2025. Of the 3,445.46-acres, 1,964.51-acres are considered to be dedicated open space (this includes 863.57-acres of floodplain that has also been dedicated as open space). This

represents an increase of 74.08% or 468.52-acres in the amount of open space dedicated between 2017-2025. In addition, the City has a total of 2,344.52-acres of floodplain, which has decreased -22.47% or a reduction of 560.80-acres from 2017-2025. This reduction or changes in the floodplain can be explained by [1] more accurate topography from the City's previous use of LIDAR, [2] Letters of Map Revision (LOMR) due to development, and [3] studies of existing unstudied floodplains. See *Figure 7.2* to see a breakdown of the City's open space and floodplain from 2017-2025.

FIGURE 7.10: OPEN SPACE AND FLOODPLAIN
KEY (2017 BELOW): BLUE = FLOODPLAIN (2,041.75-ACRES); GREEN = OPEN SPACE (632.42-ACRES);
YELLOW = OPEN SPACE AND FLOODPLAIN (556.66-ACRES).



KEY (2025 ABOVE): BLUE = FLOODPLAIN (2,344.52-ACRES); GREEN = OPEN SPACE (1,100.94-ACRES);
YELLOW = OPEN SPACE AND FLOODPLAIN (863.57-ACRES).

SOURCE: CITY OF ROCKWALL GIS DIVISION

COMMUNITY EVENTS

The City of Rockwall was designated the *Free Live Music Capital of North Texas* and hosts a minimum of 100 free live music concerts per year. These include several prominent series and festivals, contributing to a vibrant local music scene. These series and festivals are:

CONCERT BY THE LAKE SERIES.

This series features weekly performances every Thursday from May through July at the Harbor. In 2025, the lineup includes 13 concerts, starting with the Rockwall

Philharmonic Orchestra on May 1st and concluding with the David Whiteman Band on July 31st.

SAN JACINTO PLAZA MUSIC SERIES.

Held in Downtown Rockwall, this series offers live music on Friday and Saturday evenings from May through October, totaling approximately 43 concerts. The 2025 season marks its 11th year, featuring both new artists and returning favorites.

FOUNDERS DAY FESTIVAL.

Celebrating Rockwall's 171st birthday on May 17, 2025, this festival includes multiple live music performances. The event features artists such as Will Jones, William Beckmann, and headliner Josh Turner.

HARMONY AT THE HARBOR.

An annual live music series that is held at the Harbor's amphitheater, a popular venue overlooking Lake Ray Hubbard. This event features bands from the Dallas/Fort Worth area performing on Thursday evenings in October.

In addition to the City's live music offerings, the City hosts a number of community events that include a Daddy Daughter Dance, Family Fun Fridays, an Easter Egg Hunt, an Independence Day Celebration, the Mother/Son Dance, the Rib Rub, Run and Roll, Scare on the Square, a Fishing Derby, Breakfast with Santa, and the Hometown Christmas Celebration. A calendar of the City's 2025 Events is as follows:

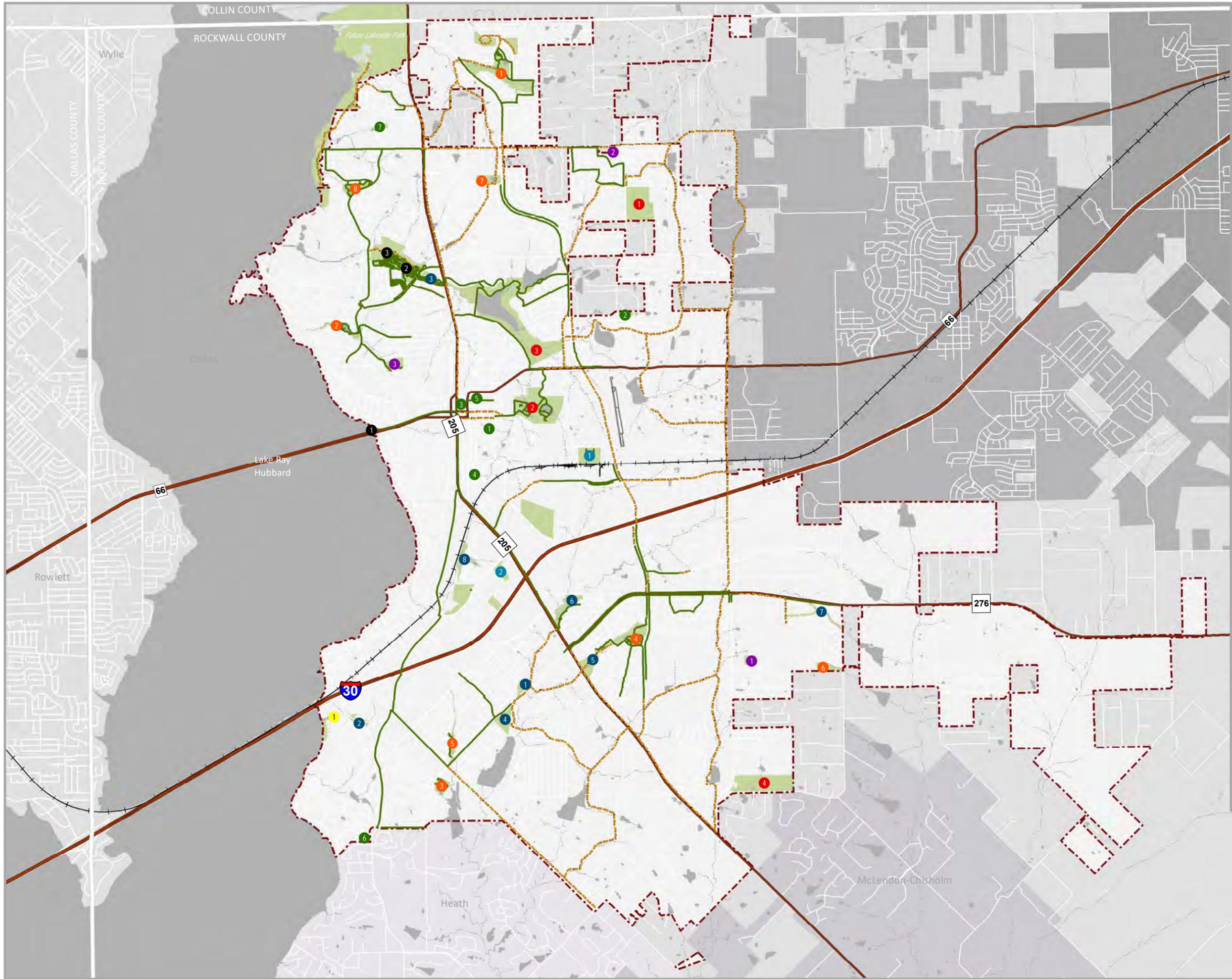
| DATE | EVENT |
|---------------------------|------------------------------|
| JANUARY 25 TH | DADDY DAUGHTER DANCE |
| MARCH 7 TH | FAMILY FUN FRIDAY |
| APRIL 5 TH | SNAP EASTER EGG HUNT |
| APRIL 12 TH | EGGSTRAVAGANZA |
| MAY 1 ST | CONCERT BY THE LAKE |
| MAY 8 TH | CONCERT BY THE LAKE |
| MAY 15 TH | CONCERT BY THE LAKE |
| MAY 17 TH | FOUNDER'S DAY |
| MAY 22 ND | CONCERT BY THE LAKE |
| MAY 29 TH | CONCERT BY THE LAKE |
| JUNE 5 TH | CONCERT BY THE LAKE |
| JUNE 12 TH | CONCERT BY THE LAKE |
| JUNE 19 TH | CONCERT BY THE LAKE |
| JUNE 26 TH | CONCERT BY THE LAKE |
| JULY 4 TH | INDEPENDENCE DAY CELEBRATION |
| JULY 17 TH | CONCERT BY THE LAKE |
| JULY 24 TH | CONCERT BY THE LAKE |
| JULY 25 TH | FAMILY FUN FRIDAY |
| JULY 31 ST | CONCERT BY THE LAKE |
| SEPTEMBER 6 TH | MOTHER/SON DANCE |
| OCTOBER 4 TH | RIB RUB, RUN, & ROLL |
| OCTOBER 9 TH | HARMONY AT THE HARBOR |
| OCTOBER 16 TH | HARMONY AT THE HARBOR |
| OCTOBER 23 RD | HARMONY AT THE HARBOR |
| OCTOBER 25 TH | SCARE ON THE SQUARE |
| NOVEMBER 8 TH | FISHING DERBY |
| NOVEMBER 14 TH | FAMILY FUN FRIDAY |
| DECEMBER 6 TH | HOMETOWN CHRISTMAS |
| DECEMBER 13 TH | BREAKFAST WITH SANTA |

PARKS, RECREATION AND OPEN SPACE MASTER PLAN

The Parks Department is currently in the process of preparing the five (5) year update for the Parks, Recreation and Open Space Master Plan. This update will look to add to the plan to ensure the City will strive to provide exceptional parks, trails and recreation amenities across the community for all the City's residents.

MAP INDEX

- (1) MAP 7.1: MAP ALL PARKS AND TRAILS
- (2) MAP 7.2: MAP OF PARKS DISTRICTS
- (3) MAP 7.3: MAP OF FLOODPLAINS & OPENSACE
- (4) MAP 7.4: JOHN KING BOULEVARD CONCEPT PLAN



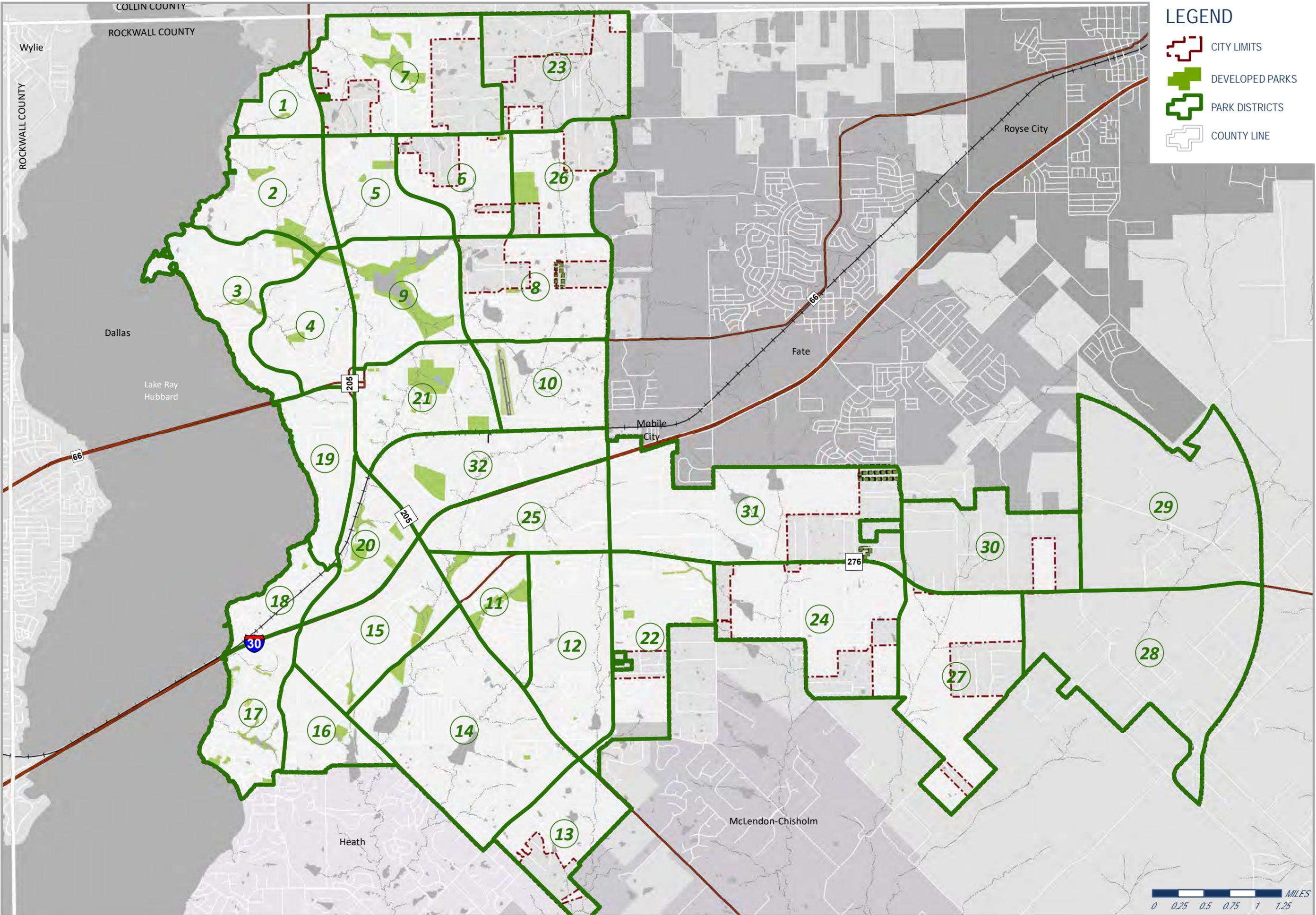
LEGEND

- CITY LIMITS
- PARKLAND
- DEVELOPED TRAILS
- FUTURE TRAILS
- COUNTY LINE

PARK DESCRIPTIONS

- COMMUNITY PARKS**
 - 1 ALMA WILLIAMS PARK
 - 2 HARRY MYERS PARK
 - 3 LAKES OF SQUABBLE CREEK PARK
 - 4 BEN A KLUTTS SR PARK
- GREENBELTS / GREENWAYS**
 - 1 HIGHLAND MEADOWS
 - 2 LAGO VISTA
 - 3 LAKEVIEW SUMMIT
 - 4 LYNDEN PARK
 - 5 MEADOW CREEK - FOXWOOD
 - 6 MEADOW CREEK - MADISON
 - 7 TIMBERCREEK
 - 8 WATERSTONE
- MINI-PARKS**
 - 1 CLARK STREET PARK
 - 2 GREENE LAKE PARK
 - 3 TED CAINS CORNER PARK
 - 4 GLORIA WILLIAMS PARK
 - 5 LOFLAND PARK
 - 6 HENRY M CHANDLER PARK
 - 7 JEWEL PARK
- NEIGHBORHOOD PARKS**
 - 1 THE PARK AT BREEZY HILL
 - 2 THE PARK AT EMERALD BAY
 - 3 THE PARK AT FOX CHASE
 - 4 THE PARK AT HICKORY RIDGE
 - 5 WINDMILL RIDGE PARK
 - 6 THE PARK AT TERRACINA
 - 7 THE PARK AT STONE CREEK
 - 8 SHORES PARK
- PARK / SCHOOL**
 - 1 NORTHSHORE PARK
 - 2 DALTON RANCH PARK
 - 3 FONTANNA PARK
- SPORTS COMPLEXES**
 - 1 LEON TUTTLE ATHLETIC COMPLEX
 - 2 YELLOW JACKET PARK
- SPECIALTY PARKS**
 - 1 SH-66 BOAT RAMP
 - 2 SQUABBLE CREEK MOUNTAIN BIKE TRAIL
 - 3 PETTINGER NATURE PRESERVE
- SPECIAL-USE PARKS**
 - 1 THE HARBOR



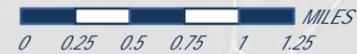


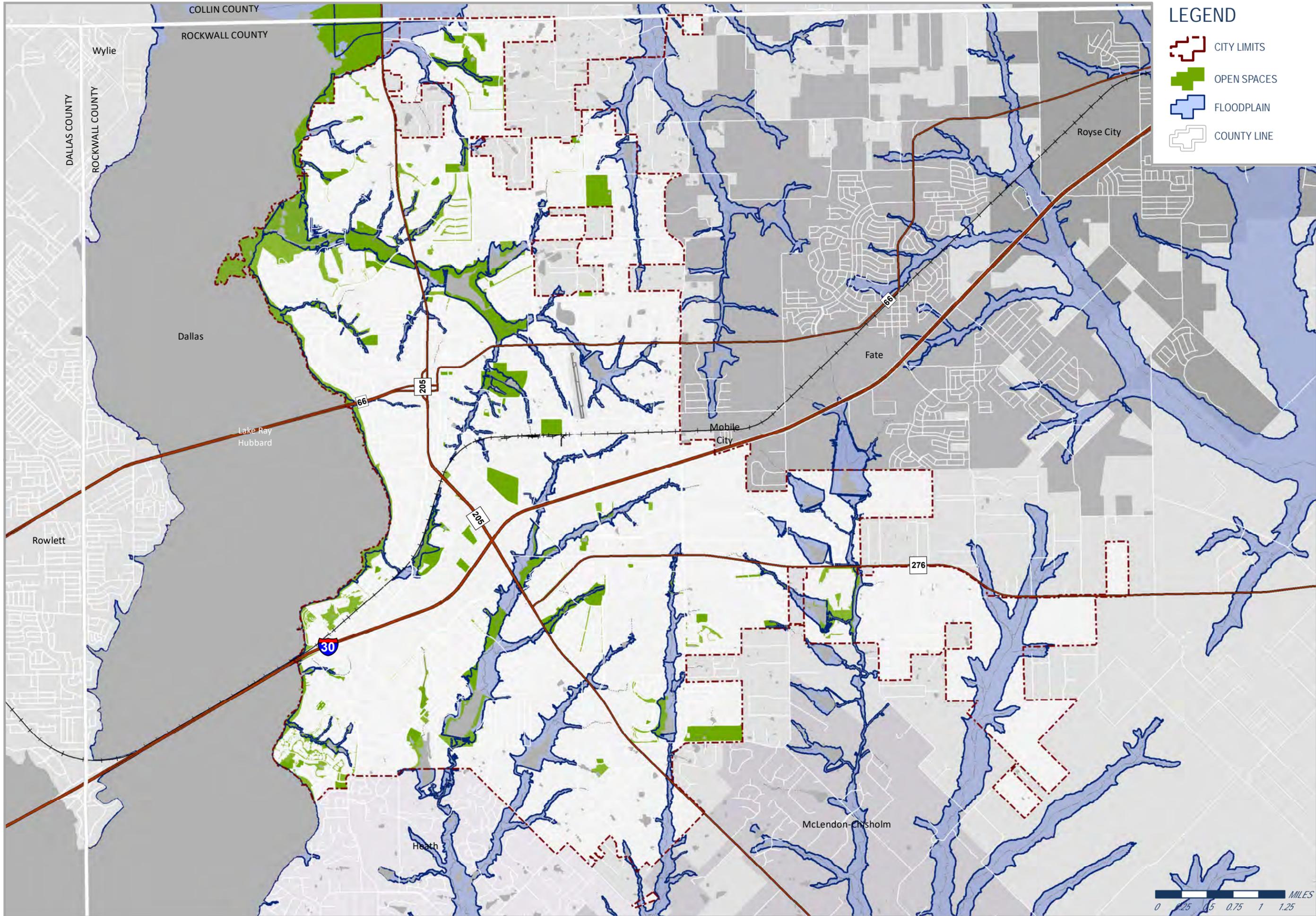
LEGEND

-  CITY LIMITS
-  DEVELOPED PARKS
-  PARK DISTRICTS
-  COUNTY LINE

PARK DISTRICTS

CH 7: PARKS, TRAILS, AND OPEN SPACE





LEGEND

-  CITY LIMITS
-  OPEN SPACES
-  FLOODPLAIN
-  COUNTY LINE

FLOODPLAINS AND OPEN SPACE

CH 7: PARKS, TRAILS, AND OPEN SPACE





CITY OF ROCKWALL

PLANNING AND ZONING DEPARTMENT MEMORANDUM

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
Planning and Zoning Commission
Comprehensive Plan Advisory Committee (CPAC)

FROM: Ryan Miller, *Director of Planning and Zoning*

CC: Mary Smith, *City Manager*
Joey Boyd, *Assistant City Manager*

DATE: April 21, 2025

SUBJECT: ESRI Tapestry Segmentation Report

In 2017, the City of Rockwall began the process of updating the City's Comprehensive Plan. As part of this process, staff utilized a tool created by ESRI -- a *Geographic Information Systems (GIS) company that specializes in software development and demographic data* -- to better understand the City's various stakeholders and their market profiles. The *Tapestry Segmentation Tool* creates a psychographic profile of the community. A psychographic profile is essentially an understanding and classification of people in the community based on socioeconomic data and psychological criteria (e.g. *attitudes, aspirations, etc.*). This method is typically used in market research for consumer goods and services, but -- *for the City's purposes* -- it helps paint a picture of the City's various market segments and constituents. This information will help City staff, the City Council, various boards and commissions, and the Comprehensive Plan Advisory Committee (CPAC) in understanding the City's population and in updating the OURHometown Vision 2040 Comprehensive Plan in the future.

WHAT IS TAPESTRY SEGMENTATION?

To begin, let us start with the question of *What is Tapestry Segmentation?* *Tapestry Segmentation* is a market segmentation system that classifies neighborhoods based on their socioeconomic and demographic compositions. A *Tapestry* is a system for classifying consumers and constituents using all the variables that can distinguish consumer behavior, from household characteristics such as income and family type to personal traits like age, education, or employment. Even housing choices can be used as an indicator of a person's behavior. Typically, to get this information a City or company would need to hire a retail market firm; however, through ESRI's *Tapestry Segmentation Tool*, this information has been made readily available to cities for planning purposes, and is included in the City's software package through ESRI.

ESRI'S TAPESTRY SEGEMENTATION TOOL METHODOLOGY

ESRI's *Tapestry Segmentation Tool* breaks down the entire population of the United States to the neighborhood level and identifies each neighborhood as one (1) of 67 unique population segments based on tapestry detail and diversity. Each of these market segments is grouped into one (1) of 14 *LifeMode Groups* and one (1) of six (6) *Urbanization Groups*. The *LifeMode Groups* are markets that have specific common experiences (e.g. *people born of the same generation, immigration status, etc.*) or a significant demographic trait (e.g. *income, ethnicity, marital status, etc.*). *Urbanization Groups* are markets that share similar geographies or locales (i.e. *urban, rural, etc.*).

CITY OF ROCKWALL'S TAPESTRY SEGEMENTATION REPORT SUMMARY, 2017 & 2024

In the *2017 Tapestry Segmentation Report*, the City of Rockwall was represented by 12 of the 67 market segments, eight (8) of the 14 *LifeMode Groups*, and five (5) out of six (6) of the *Urbanization Groups*. The five (5) main market segments that emerged from this report -- *making up a total of 70.22% of the City's population* -- were the [1] *Up and Coming Families* (20.82%), [2] *Boomburbs* (16.57%), [3] *Soccer Moms* (12.32%), [4] *Comfortable Empty Nesters* (10.85%), and [5] *In Style* (9.66%) segments. The *2024 Tapestry Segmentation Report* shows that the City's market segments have shifted and become more diverse, with the five (5) main market segments changing and only representing 63.16% of the City's population. These segments are: [1] *Up and Coming Families* (16.77%), [2] *Boomburbs* (14.90%), [3] *Savvy Suburbanites* (11.41%), [4] *Retirement Communities* (10.780%), [4] *Workday Drive* -- formerly *Soccer Moms* -- (9.30%), and [5] *In Style* (9.27%). Staff has summarized the changes

to the City's market segments, *LifeMode Groups*, and *Urbanization Groups* in Tables 1, 2 & 3 and provided a summary of the characteristics of each of *LifeMode* and *Urbanization Group* below. In addition, staff has provided the 2024 *Segmentation Report* and the psychographic profiles of each of the City's *Tapestry Market Segments* attached to this memorandum. Staff also annotated the changes from 2017 to 2024 for each of the City's *Tapestry Market Segments*.

SUMMARY OF LIFEMODE GROUPS CHARACTERISTICS

LifeMode Groups represent markets that share a common experience (e.g. born in the same generation or immigration from another country) or a significant demographic trait, such as affluence. The characteristics of the *LifeMode Groups* that are present in the City of Rockwall are as follows:

(1) **AFFLUENT ESTATES:** 24.66% > 29.33% [*Established wealth – educated, well-travelled married couples*]

- This group is typically married couples that have established wealth, are well educated, and well-traveled.
- This group is accustomed to *MORE*.
- Almost all people in this group are homeowners (~90.00%) with ~70.00% having mortgages.
- Married couples' families with children ranging from grade-school aged to college.
- This group expects *QUALITY* and will invest in time-saving services.
- This group participates actively in their communities.
- This group is active in sports and is an enthusiastic traveler.

NOTE: This *LifeMode Segment* incorporates the following *Tapestry Market Segments*: [1] *Top Tier*, [2] *Professional Pride*, [3] *Boomburbs*, [4] *Savvy Suburbanites*, and [5] *Exurbanites*.

(2) **SPROUTING EXPLORERS:** 23.37% > 16.77% [*Established diversity – young, Hispanic homeowners with families*]

NOTE: This group was previously named *Ethnic Enclave* in 2017.

- This group is composed of a population that is described as being diverse and young. They also include a disproportionately high number of Hispanic homeowners with families.
- Typically, this group is composed of multilingual and multigenerational households that feature children that represent the second, third, or fourth generation of Hispanic families.
- The neighborhoods that this group resides in are composed of single-family, owner-occupied homes built at the City's edge, primarily built after 1980.
- This group is characterized as hard working, optimistic with most residents aged 25 years or older that have a high school diploma and some college education.
- This group focuses on shopping and leisure and place an emphasis on children and family. Their main purchases are baby and children's products from shoes to toys, games, and local trips to theme parks, water parks, or the zoo.
- These residents favor Hispanic programs on radio or television. Children enjoy playing video games on personal computers, handheld or console devices.
- Many households have domestic pets.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Up and Coming Families*, [2] *Urban Villages*, [3] *American Dreamers*, [4] *Barrios Urbanos*, [5] *Valley Growers*, and [6] *Southwestern Families*.

(3) **GENXURBAN:** 20.51% > 17.93% [*Gen X in middle age; families with fewer kids and a mortgage*]

TABLE 1: TAPESTRY SEGMENTS

INCREASE OVER 2017; DECREASE OVER 2017

| TAPESTRY SEGMENT | % OF HOUSEHOLD | |
|----------------------------|----------------|--------|
| | 2017 | 2024 |
| UP AND COMING FAMILIES | 20.82% | 16.77% |
| BOOMBURBS | 16.57% | 14.90% |
| WORKDAY DRIVE | 12.32% | 9.30% |
| COMFORTABLE EMPTY NESTERS | 10.85% | 8.66% |
| IN STYLE | 9.66% | 9.27% |
| BRIGHT YOUNG PROFESSIONALS | 6.72% | 6.46% |
| GREEN ACRES | 6.54% | 6.38% |
| SAVVY SUBURBANITES | 4.93% | 11.41% |
| DOWN THE ROAD | 3.44% | 3.05% |
| SOUTHWESTERN FAMILIES | 2.55% | - |
| PROFESSIONAL PRIDE | 2.45% | 2.21% |
| RETIREMENT COMMUNITIES | 2.43% | 10.78% |
| EXURBANITES | 0.71% | 0.81% |

NOTE: THE SOCCER MOMS CATEGORY FROM 2017 BECAME WORKDAY DRIVE CATEGORY IN 2023.

TABLE 2: LIFEMODES GROUPS

INCREASE OVER 2017; DECREASE OVER 2017

| LIFEMODE GROUPS | % OF HOUSEHOLD | |
|---------------------|----------------|--------|
| | 2017 | 2024 |
| AFFLUENT ESTATES | 24.66% | 29.33% |
| SPROUTING EXPLORERS | 23.37% | 16.77% |
| GENXURBAN | 20.51% | 17.93% |
| FAMILY LANDSCAPE | 12.32% | 9.30% |
| MIDDLE GROUND | 6.72% | 6.46% |
| COZY COUNTRY LIVING | 6.54% | 6.38% |
| RUSTIC OUTPOSTS | 3.44% | 3.05% |
| SENIOR STYLES | 2.43% | 10.78% |

NOTE: THE ETHNIC ENCLAVE CATEGORY FROM 2017 BECAME SPROUTING EXPLORERS CATEGORY IN 2023.

TABLE 3: URBANIZATION GROUPS

INCREASE OVER 2017; DECREASE OVER 2017

| URBANIZATION GROUPS | % OF HOUSEHOLDS | |
|---------------------|-----------------|--------|
| | 2017 | 2024 |
| SUBURBAN PERIPHERY | 68.70% | 64.07% |
| METRO CITIES | 12.10% | 20.05% |
| URBAN PERIPHERY | 9.30% | 6.46% |
| RURAL | 6.50% | 6.38% |
| SEMI-RURAL | 3.40% | 3.05% |

- This group is made up people from the *Generation X* age group. People are middle aged, married, with families and fewer kids. This group typically has a mortgage.
- This is the second largest *Tapestry Group* with a growing number of retirees.
- About one-fifth (1/5) of the people in this group are residents that are 65 years of age or older. About one-fourth (1/4) of the households are already drawing retirement income.
- This group typically owns older single-family homes in urban areas and have two (2) vehicles.
- Residents from this group typically work and live in the same county and favor shorter commute times.
- This group invests wisely, are well insured, and comfortable banking online or in person.
- This group is characterized as *NEWS JUNKIES* (i.e. they read daily newspapers, watch news on television, and go online for news).
- People in this group enjoy reading, photo album/scrapbooking, playing board games and cards, doing crossword puzzles, going to museums and rock concerts, dining out, and walking for exercise.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Comfortable Empty Nesters*, [2] *In-Style*, [3] *Parks and Rec*, [4] *Rustbelt Traditions* and [5] *Midlife Constants*.

(4) SENIOR STYLES: 2.43% > 10.78% [*Senior lifestyles reveal the effect of saving for retirement*]

- People of this group reveal the effects of saving for retirement.
- Households in the segment are commonly married empty nesters or singles living alone. They typically live in single-family homes (including seasonal getaways), retirement communities, or age restricted apartments.
- Seniors that are more affluent, and travel and relocate to warmer climates. Seniors that are less affluent are settled in their communities and are working towards full-time retirement.
- While cell phones are popular in this segment they will also typically continue to have landline telephones.
- Many people in this group still prefer print media over digital content. They are avid readers of newspapers and stay current with regard to news.
- This group subscribe to cable television and prefer watching channels like FOX News, CNN, and the Weather Channel.
- These residents are large purchasers of vitamins and place an emphasis on health and exercise to increase longevity.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Silver and Gold*, [2] *Golden Years*, [3] *The Elders*, [4] *Senior Escapes*, [5] *Retirement Communities*, and [6] *Social Security Set*.

(5) FAMILY LANDSCAPES: 12.32% > 9.30% [*Successful younger families in newer housing*]

- This group is made up of successful young families in their first homes.
- It is a relatively *NON-DIVERSE* group composed of prosperous married-couple families, residing in suburban or semi-rural areas with low vacancy rates (second lowest in a *Tapestry Segments*).
- 80.00% of people in this segment are homeowners with mortgages. The homes are typically newer single-family homes, with median home values that are slightly higher than the remainder of the United States.
- Households typically have two (2) people working in the family. This group has the second highest labor force participation rate of all *LifeMode Groups* and a very low unemployment rate.
- People in this group are characterized as *DO-IT-YOURSELFERS* who work on home improvement projects, and typically spend time working outdoors on their lawn and gardens.
- This group is composed of sports enthusiasts, typically own newer sedans and SUVs, have dogs/are pet owners, and have savings accounts and plans.
- This group eats out frequently at fast food or family restaurants to accommodate their busy lifestyles.
- This group enjoys bowling, swimming, playing golf, playing video games, watching movies rented from streaming services or redbox, and taking trips to the zoo or theme parks.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Workday Drive*, [2] *Home Improvement*, and [3] *Middleburg*.

(6) MIDDLE GROUND: 6.72% > 6.46% [*Lifestyles of thirtysomethings*]

- This group is described as having the lifestyles of thirtysomethings, or being Millennials in the middle.
- People in this segment are typically single/married, renters/homeowners, and middle-class or working class (i.e. they are transitioning to middle age life modes).
- They live in a market mix of single-family homes, townhomes, and multi-family dwellings.
- The majority of people in the group attended college or attained a college degree.
- People in this segment are cell phone centric, using the device to listen to music, read the news, and get the latest updates on their favorite sports teams.

- This group is considered to be *ONLINE ALL THE TIME*, using the internet for entertainment (i.e. *stream media, download music, watching YouTube, finding dates, etc.*), social media (i.e. *Facebook, Twitter, LinkedIn, etc.*), shopping, and news.
- Leisure activities for people in this group include night life (i.e. *clubs, movies, etc.*), going to the beach/lake, and some travel and hiking.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *City Lights*, [2] *Emerald City*, [3] *Bright Young Professionals*, [4] *Downtown Melting Pot*, [5] *Front Porches*, [6] *Old and Newcomers*, and [7] *Hardscrabble Road*.

(7) COZY COUNTRY LIVING: 6.54% > 6.38% [*Empty nesters in bucolic settings*]

- This group is typically composed of *EMPTY NESTERS* in bucolic settings.
- This is the largest *Tapestry Group* with almost half of all households being located in the Midwest.
- The people in this group consist of homeowners that typically have pets and that reside in single-family dwellings in rural areas. Almost 30% of all people in this district have three (3) or more vehicles and auto loans.
- People of this group are described as being politically conservative and believe in the importance of buying American.
- This group typically owns trucks, motorcycles, and/or ATVs/UTVs.
- They prefer to eat at home, and consistently shop at discount stores (especially *Wal-Mart*). They also bank in person and spend very little time online.
- These households typically own every tool and piece of equipment imaginable to maintain their homes, vehicles, vegetable gardens, and lawns.
- People in this segment are more likely to listen to country music, watch auto racing on television, and play the lottery. They are also described as enjoying outdoor activities (e.g. *fishing, hunting, camping, boating, and even bird watching*).

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Green Acres*, [2] *Salt of the Earth*, [3] *The Great Outdoors*, [4] *Prairie Living*, [5] *Rural Resort Dwellers*, and [6] *Heartland Communities*.

(8) RUSTIC OUTPOST: 3.44% > 3.05% [*Country life with older families, older homes*]

- This group is characterized by country life with older families in older homes.
- People in this group depend on manufacturing, retail and healthcare, mining, and agricultural jobs.
- This group typically has low labor force participation in skilled and service occupations.
- Housing in this group is typically affordable, older single-family or mobile homes. Vehicle ownership is essential in this group.
- Residents in this segment typically live within their means, shop at discount stores, and maintain their own vehicles -- *which are typically used* -- and homes.
- People in this group are characterized as outdoor enthusiasts, who grow their own vegetables, love their pets, and enjoy hunting and fishing.
- Technology is typically cost prohibitive and complicated for this group. They tend to pay bills in person, use the yellow pages, read the newspaper, and mail-order books.

NOTE: This *LifeMode Group* incorporates the following *Tapestry Market Segments*: [1] *Southern Satellites*, [2] *Rooted Rural*, [3] *Diners and Miners*, [4] *Down the Road*, and [5] *Rural Bypasses*.

SUMMARY OF URBANIZATION GROUPS CHARACTERISTICS

Tapestry groups are also available as *Urbanization Groups*, in which markets share similar locales, from the urban canyons of the largest cities to the rural lanes of villages or farms. The characteristics of the City of Rockwall's *Urbanization Groups* are as follows:

(1) SUBURBAN: 68.65% > 64.07% [*Affluence in the suburbs; married couple-families; longer commutes*]

- This group is characterized by *Urban Expansion*, affluence in the suburbs, and/or city-by-commute.
- This area is the most populous and fastest growing areas of all the *Urbanization Groups*, and includes nearly a one-third (1/3) of the nation's population.
- Commuters in this group value low-density living, but demand close proximity to jobs, entertainment, and amenities of the urban center.
- People in this group are typically well-educated, in two (2) income households and accept longer than average commute times to raise their children in family friendly neighborhoods. Many are heavily mortgaged in newly built, single-family homes.
- Older homeowners in these areas have either retired in place, downsized, or purchased a seasonal home.
- Suburbanites are the most affluent group in this segment and are characterized as hardworking and striving for bright fulfilling lives.
- Residents in the *Urbanization Group* invest for their futures, insure themselves against unforeseen circumstances, but also enjoy the fruits of their labor.

NOTE: This *Urbanization Group* incorporates the following *Tapestry Market Segments* specific to the City of Rockwall: [1] *Boomburbs*, [2] *Workday Drive*, [3] *Comfortable Empty Nesters*, [4] *Up and Coming Families*, [5] *Savvy Suburbanites*, [6] *Professional Pride*, and [7] *Exurbanites*.

(2) METRO CITIES: 12.09% > 20.05% [*Affordable city life, including smaller metros, satellite cities*]

- This *Urbanization Group* is characterized by an affordable City life that includes smaller metropolitan cities or satellite cities that feature a mix of single-family and multi-family housing.
- Groups under this category are seeking affordable living opportunities in or close to cities. These are usually multi-family buildings that range from mid- to high-rise apartments. The average monthly rent and home value is typically below the housing average for the United States.
- Consumers in the group include college students, affluent *Generation X* couples, and retirement communities.
- Student loans are more common than mortgages in this group. In addition, debit cards are used more commonly than credit cards by this group.
- Residents in this group share an interest in city life and its amenities and activities (e.g. *dancing and clubbing, museums, and concerts*).
- These residents also favor convenience and mobility.
- Many residents rely on the internet for entertainment (e.g. *download music, stream television, play online games, etc.*) and as a resource (e.g. *job searches*).

NOTE: This *Urbanization Group* incorporates the following *Tapestry Market Segments* specific to the City of Rockwall: [1] *In Style*, and [2] *Retirement Communities*.

(3) URBAN PERIPHERY: 9.27% > 6.46% [*City life for starting families with single-family homes*]

- This group features starting families in neighborhoods that are on the fringe of major cities.
- Housing in this group usually consists of the earliest suburbs, being single-family homes primarily built prior to the 1970's; however, there are some apartments that fall into this category.
- People in this group are typically young, diverse families.
- Homeowners in this group live close to the city and have a lower than average vacancy rate.
- Leisure for people in this group consists of a focus on children (e.g. *visits to theme parks or water parks*), sports (e.g. *soccer, basketball, baseball, and etc.*) and movies.
- The spending habits of people in this segment emphasizes children with the majority of purchases relating to clothing, toys, and baby products.
- Parents of younger children in this group favor family restaurants and fast food.
- Smartphones are essential for social contact, shopping, and music for people in this group.

NOTE: This *Urbanization Group* incorporates the following *Tapestry Market Segments* specific to the City of Rockwall: [1] *Bright Young Professionals*

(4) RURAL: 6.54% > 6.38% [*Country living with older families; low density; and low diversity*]

- This group features country living and single-family homes with acreage, farms, and rural resort areas.
- Areas in this group have very low population densities with typically less than 50 people per square mile.
- Over half of all households in this group are occupied by persons 55 years and older. Many are married couples without children at home.
- This is the least diverse *Urbanization Group* with over 80.00% of the group being *Non-Hispanic White*.
- People in this group are mostly homeowners (~80.00%), but rentals are affordable in single-family and mobile homes areas.
- Trips to the store and work are typically longer for people in this group.
- People in this group typically drive alone in a truck or SUV, and are more likely to listen to country music on the radio.
- This group is dominated by *Blue-Collar* jobs that include manufacturing, agriculture, mining, and construction.
- Many people in this group are self-employed, retired, or receive income from social security.
- Satellite television and landline phone lines are typical in this group.
- This group tends to be more conservative in the spending and belief practices.
- This group is characterized as being comfortable and established, and not likely to move.

NOTE: This *Urbanization Group* incorporates the following *Tapestry Market Segments* specific to the City of Rockwall: [1] *Green Acres*.

(5) SEMI RURAL: 3.44% > 3.05% [*Small town living, families, low density, and low diversity*]

- This *Urbanization Group* has the most affordable housing of all the groups. It is typically situated in smaller towns and communities that are located throughout the country.

- Housing in this group consists of single-family and mobile homes that are typically within reach of some amenities.
- People in these areas embrace a quiet, country lifestyle that usually includes country music and hunting.
- People in this group prefer domestic products and vehicles (*especially trucks*).
- Residents typically shop at large department and discount stores (*e.g. Wal-Mart*).
- People in these areas typically prefer fast food and dinner mixes/kits as opposed to fine dining.
- Many people in these areas are employed in the agricultural, fishing or forestry professions.
- Vacations for people in this group are typically spent with family as opposed to travel or vacation destinations.
- When services are needed this group will typically use the yellow pages to meet their needs.

NOTE: This *Urbanization Group* incorporates the following *Tapestry Market Segments* specific to the City of Rockwall: [1] *Down the Road*.

SUMMARY AND FINDINGS

While many of these profiles appear to be common sensical and/or stereotypical, they do paint an accurate picture of the City's various socioeconomic and consumer groups. They also appear to be consistent with the 2020 Census Data and the growth trends observed by staff over the last eight (8) years. For example, the 2024 Segmentation Report indicates that since 2017, the Retirement Communities tapestry segment of the City's population has grown from 2.43% to 10.78%. This is consistent with the age restricted single-family and multi-family housing projects that have been completed during this time period. Some of these new projects include the Ladera Subdivision (118 age restricted single-family home units), the Standard Subdivision (52 age restricted single-family units), the Alders at Rockwall (144 age restricted multi-family units), and the Village Green (28 assisted living units). Another example of the Tapestry Segmentation Report's accuracy can be seen in the increase in Savvy Suburbanites (4.93% > 11.41%) tapestry segments, which may correlate to the professional offices and multi-family units that have been completed in the City's Harbor District over the last six (6) years. In addition, the US Census Bureau's data shows, that since 2010, the City of Rockwall's population has become more diverse with regard to race and ethnicity. This appears to be consistent with the decreases observed in the Up and Coming Families (20.82% > 16.77%), Boomburbs (16.57% > 14.90%), Workday Drive (12.32% > 9.30%), and Comfortable Empty Nesters (10.85% > 8.66%) tapestry segments. Additionally, the 2020 US Census shows a decrease in the percentage of population identifying as Hispanic or Latino (16.6% in 2010 > 15.60% in 2020), which is consistent in the decrease in the Sprouting Explorers tapestry segment (23.37% > 16.77%).

PROJECTED TRENDS

Looking forward at potential trends -- and based on the trends observed from the 2000, 2010, and 2020 US Census Data and by City staff over this same time period --, it is anticipated that the City's population will continue to become more diverse, and that the City will continue to attract a mix of affluent households and young families. With the final waves of the baby boomer population entering into retirement and the first waves of the Generation X population exiting the workforce, it is not inconceivable to project the Retirement Communities, Savvy Suburbanites, Exurbanites, and Green Acres tapestry segments will grow moving forward. Based on this projection, the Affluent Estates and Senior Styles LifeMode Groups should also be anticipated to grow in the future. The Affluent Estates LifeMode Group should also be anticipated to grow through the Savvy Suburbanites tapestry segment due to the increases in the cost of new single-family homes in the City, which increased by 184.17% (*i.e. Average Housing Value: \$271,632.00 in January 2017 to \$500,254.00 in March 2023; Texas A&M Texas Real Estate Research Center*) over the six (6) year period from 2017-2023. This trend should inversely shrink the Down the Road tapestry segment, which will also decrease the Rustic Outposts LifeMode Group and the Semi-Rural Urbanization Group. With this being said, the Rural Urbanization Group is projected to increase due to the lack of wastewater infrastructure existing along SH-276. It is projected that the

TABLE 4: LIFEMODES GROUPS PROJECTED TREND

(1) INCREASE OVER 2017; DECREASE OVER 2017
 (2) ↑: INCREASE IN THE FUTURE; ↓: DECREASE IN THE FUTURE; = STAY THE SAME IN THE FUTURE

| LIFEMODE GROUPS | % OF HOUSEHOLD | | |
|---------------------|----------------|--------|-------|
| | 2017 | 2024 | TREND |
| AFFLUENT ESTATES | 24.66% | 29.33% | ↑ |
| SPROUTING EXPLORERS | 23.37% | 16.77% | = |
| GENXURBAN | 20.51% | 17.93% | = |
| FAMILY LANDSCAPE | 12.32% | 9.30% | ↑ |
| MIDDLE GROUND | 6.72% | 6.46% | ↓ |
| COZY COUNTRY LIVING | 6.54% | 6.38% | ↓ |
| RUSTIC OUTPOSTS | 3.44% | 3.05% | ↓ |
| SENIOR STYLES | 2.43% | 10.78% | ↑ |

NOTE: THE ETHNIC ENCLAVE CATEGORY FROM 2017 BECAME SPROUTING EXPLORERS CATEGORY IN 2023.

TABLE 5: URBANIZATION GROUPS PROJECTED TREND

(3) INCREASE OVER 2017; DECREASE OVER 2017
 (4) ↑: INCREASE IN THE FUTURE; ↓: DECREASE IN THE FUTURE; = STAY THE SAME IN THE FUTURE

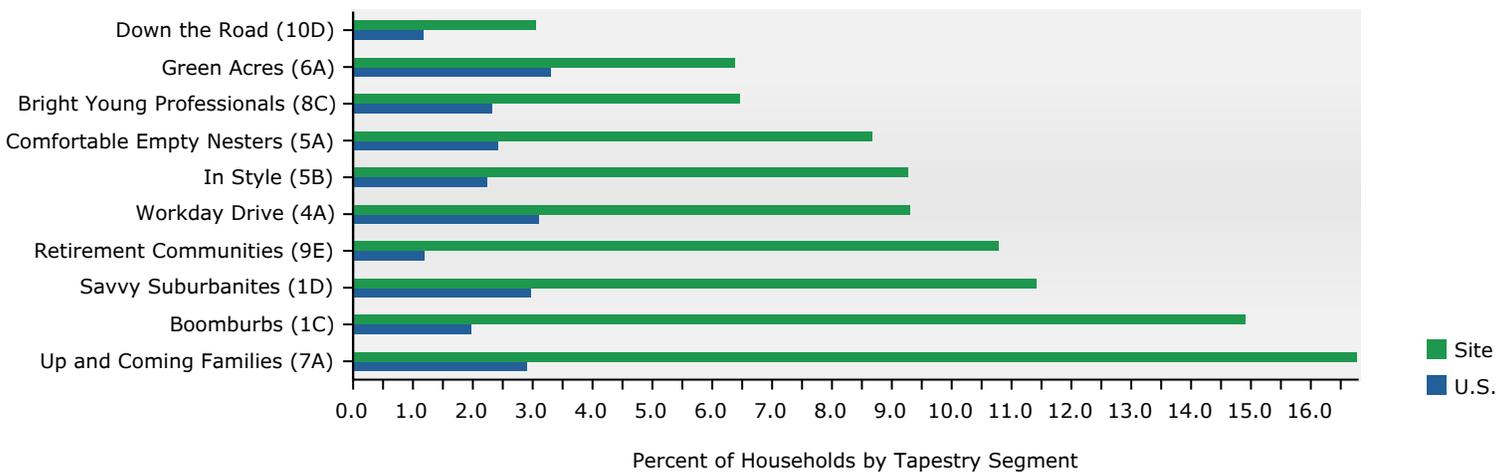
| URBANIZATION GROUPS | % OF HOUSEHOLDS | | |
|---------------------|-----------------|--------|-------|
| | 2017 | 2024 | TREND |
| SUBURBAN | 68.65% | 64.07% | = |
| METRO CITIES | 12.09% | 20.05% | ↑ |
| URBAN PERIPHERY | 9.27% | 6.46% | ↓ |
| RURAL | 6.54% | 6.38% | ↑ |
| SEMI-RURAL | 3.44% | 3.05% | ↓ |

large tracts in this area of the City, will develop with large estate lots utilizing *On-Site Sanitary Sewer (OSSF)* systems (*i.e. septic systems*). Finally, the high educational attainment achieved by the Rockwall Independent School District (RISD), should continue to make this a sought-after school district. This should lead to increases in the *Family Landscape LifeMode Group* and help maintain the percentage of the population that is in the *Suburban Urbanization Group*.

Top Twenty Tapestry Segments

| Rank | Tapestry Segment | 2024 Households | | 2024 U.S. Households | | Index |
|-----------------|---------------------------------|-----------------|--------------------|----------------------|--------------------|------------|
| | | Percent | Cumulative Percent | Percent | Cumulative Percent | |
| 1 | Up and Coming Families (7A) | 16.8% | 16.8% | 2.9% | 2.9% | 577 |
| 2 | Boomburbs (1C) | 14.9% | 31.7% | 2.0% | 4.9% | 755 |
| 3 | Savvy Suburbanites (1D) | 11.4% | 43.1% | 3.0% | 7.8% | 385 |
| 4 | Retirement Communities (9E) | 10.8% | 53.9% | 1.2% | 9.0% | 909 |
| 5 | Workday Drive (4A) | 9.3% | 63.2% | 3.1% | 12.1% | 300 |
| Subtotal | | 63.2% | | 12.2% | | |
| 6 | In Style (5B) | 9.3% | 72.4% | 2.2% | 14.4% | 416 |
| 7 | Comfortable Empty Nesters (5A) | 8.7% | 81.1% | 2.4% | 16.8% | 359 |
| 8 | Bright Young Professionals (8C) | 6.5% | 87.6% | 2.3% | 19.1% | 279 |
| 9 | Green Acres (6A) | 6.4% | 93.9% | 3.3% | 22.4% | 193 |
| 10 | Down the Road (10D) | 3.0% | 97.0% | 1.2% | 23.6% | 260 |
| Subtotal | | 33.9% | | 11.4% | | |
| 11 | Professional Pride (1B) | 2.2% | 99.2% | 1.6% | 25.2% | 134 |
| 12 | Exurbanites (1E) | 0.8% | 100.0% | 1.9% | 27.1% | 42 |
| Subtotal | | 3.0% | | 3.5% | | |
| Total | | 100.0% | | 27.1% | | 369 |

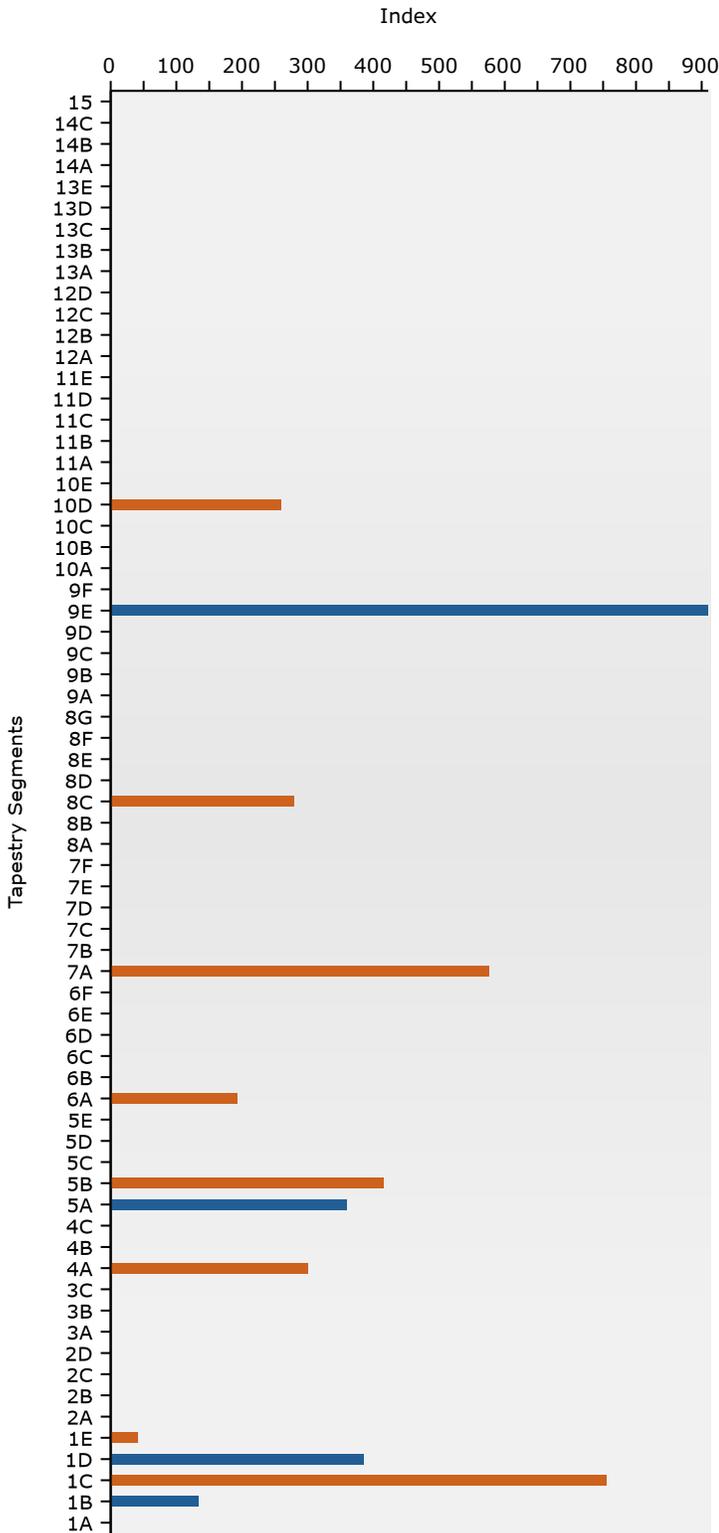
Top Ten Tapestry Segments Site vs. U.S.



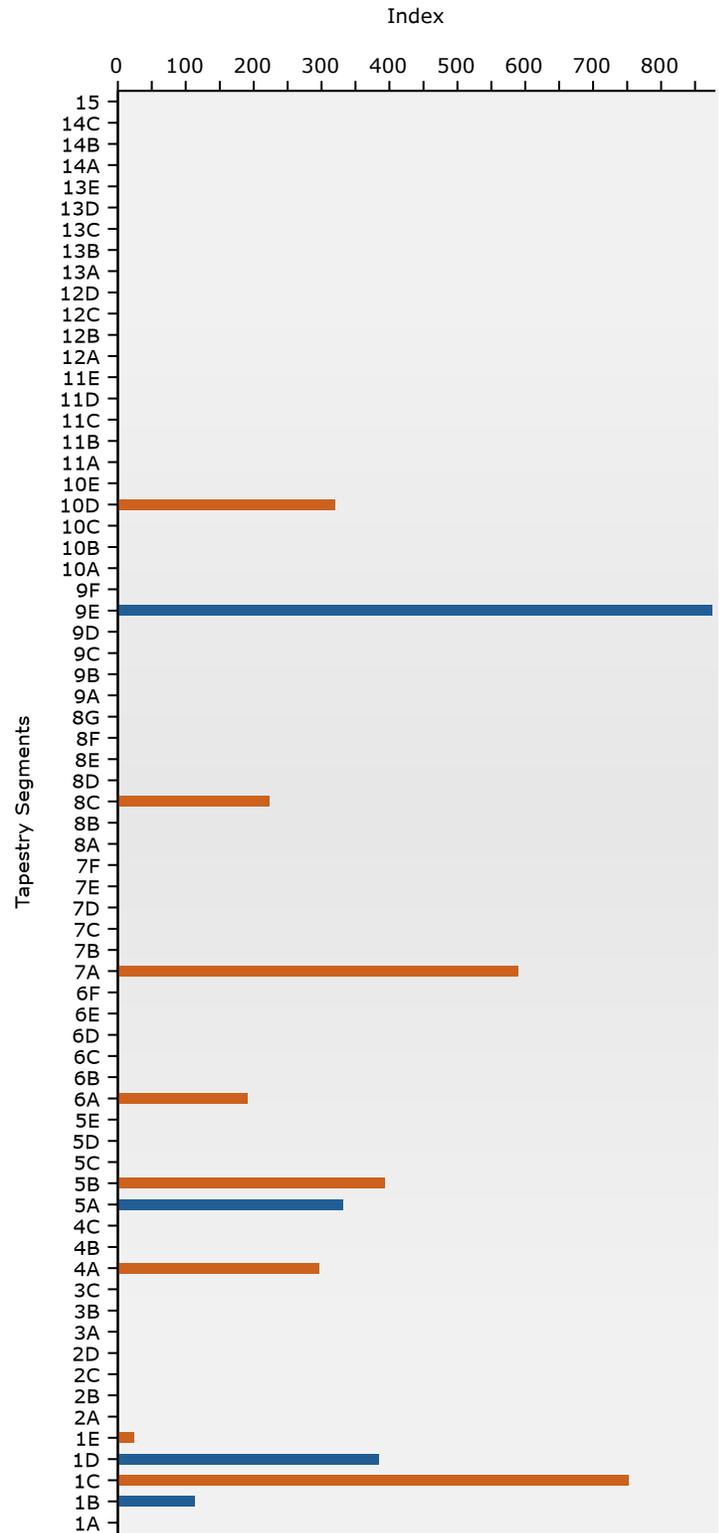
Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri

2024 Tapestry Indexes by Households



2024 Tapestry Indexes by Total Population 18+



Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri

| Tapestry LifeMode Groups | 2024 Households | | | 2024 Adult Population | | |
|---------------------------------|-----------------|--------------|------------|-----------------------|--------------|------------|
| | Number | Percent | Index | Number | Percent | Index |
| Total: | 18,587 | 100.0% | | 39,201 | 100.0% | |
| 1. Affluent Estates | 5,452 | 29.3% | 290 | 12,293 | 31.4% | 287 |
| Top Tier (1A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Professional Pride (1B) | 411 | 2.2% | 134 | 829 | 2.1% | 114 |
| Boomburbs (1C) | 2,770 | 14.9% | 755 | 6,464 | 16.5% | 753 |
| Savvy Suburbanites (1D) | 2,121 | 11.4% | 385 | 4,814 | 12.3% | 385 |
| Exurbanites (1E) | 150 | 0.8% | 42 | 186 | 0.5% | 24 |
| 2. Upscale Avenues | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Urban Chic (2A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Pleasantville (2B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Pacific Heights (2C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Enterprising Professionals (2D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 3. Uptown Individuals | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Laptops and Lattes (3A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Metro Renters (3B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Trendsetters (3C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 4. Family Landscapes | 1,729 | 9.3% | 118 | 3,874 | 9.9% | 120 |
| Workday Drive (4A) | 1,729 | 9.3% | 300 | 3,874 | 9.9% | 297 |
| Home Improvement (4B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Middleburg (4C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 5. GenXurban | 3,333 | 17.9% | 161 | 6,328 | 16.1% | 151 |
| Comfortable Empty Nesters (5A) | 1,610 | 8.7% | 359 | 3,119 | 8.0% | 332 |
| In Style (5B) | 1,723 | 9.3% | 416 | 3,209 | 8.2% | 393 |
| Parks and Rec (5C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rustbelt Traditions (5D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Midlife Constants (5E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 6. Cozy Country Living | 1,186 | 6.4% | 54 | 2,543 | 6.5% | 56 |
| Green Acres (6A) | 1,186 | 6.4% | 193 | 2,543 | 6.5% | 192 |
| Salt of the Earth (6B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| The Great Outdoors (6C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Prairie Living (6D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rural Resort Dwellers (6E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Heartland Communities (6F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 7. Sprouting Explorers | 3,117 | 16.8% | 223 | 7,279 | 18.6% | 213 |
| Up and Coming Families (7A) | 3,117 | 16.8% | 577 | 7,279 | 18.6% | 589 |
| Urban Villages (7B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Urban Edge Families (7C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Forging Opportunity (7D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Farm to Table (7E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Southwestern Families (7F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |

Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri

| Tapestry LifeMode Groups | 2024 Households | | | 2024 Adult Population | | |
|----------------------------------|-----------------|--------------|------------|-----------------------|-------------|------------|
| | Number | Percent | Index | Number | Percent | Index |
| Total: | 18,587 | 100.0% | | 39,201 | 100.0% | |
| 8. Middle Ground | 1,201 | 6.5% | 60 | 1,864 | 4.8% | 47 |
| City Lights (8A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Emerald City (8B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Bright Young Professionals (8C) | 1,201 | 6.5% | 279 | 1,864 | 4.8% | 223 |
| Downtown Melting Pot (8D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Front Porches (8E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Old and Newcomers (8F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Hometown Heritage (8G) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 9. Senior Styles | 2,003 | 10.8% | 186 | 3,547 | 9.0% | 177 |
| Silver & Gold (9A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Golden Years (9B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| The Elders (9C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Senior Escapes (9D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Retirement Communities (9E) | 2,003 | 10.8% | 909 | 3,547 | 9.0% | 876 |
| Social Security Set (9F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 10. Rustic Outposts | 566 | 3.0% | 38 | 1,473 | 3.8% | 48 |
| Southern Satellites (10A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rooted Rural (10B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Economic BedRock (10C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Down the Road (10D) | 566 | 3.0% | 260 | 1,473 | 3.8% | 320 |
| Rural Bypasses (10E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 11. Midtown Singles | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| City Strivers (11A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Young and Restless (11B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Metro Fusion (11C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Set to Impress (11D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| City Commons (11E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 12. Hometown | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Family Foundations (12A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Traditional Living (12B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Small Town Sincerity (12C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Modest Income Homes (12D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 13. Next Wave | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Diverse Convergence (13A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Family Extensions (13B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| NeWest Residents (13C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Fresh Ambitions (13D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| High Rise Renters (13E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 14. Scholars and Patriots | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Military Proximity (14A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| College Towns (14B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Dorms to Diplomas (14C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Unclassified (15) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |

Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri

| Tapestry Urbanization Groups | 2024 Households | | | 2024 Adult Population | | |
|----------------------------------|-----------------|--------------|------------|-----------------------|--------------|------------|
| | Number | Percent | Index | Number | Percent | Index |
| Total: | 18,587 | 100.0% | | 39,201 | 100.0% | |
| 1. Principal Urban Center | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Laptops and Lattes (3A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Metro Renters (3B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Trendsetters (3C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Downtown Melting Pot (8D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| City Strivers (11A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| NeWest Residents (13C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Fresh Ambitions (13D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| High Rise Renters (13E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 2. Urban Periphery | 1,201 | 6.5% | 39 | 1,864 | 4.8% | 27 |
| Pacific Heights (2C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rustbelt Traditions (5D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Urban Villages (7B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Urban Edge Families (7C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Forging Opportunity (7D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Southwestern Families (7F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| City Lights (8A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Bright Young Professionals (8C) | 1,201 | 6.5% | 279 | 1,864 | 4.8% | 223 |
| Metro Fusion (11C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Family Foundations (12A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Modest Income Homes (12D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Diverse Convergence (13A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Family Extensions (13B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 3. Metro Cities | 3,726 | 20.0% | 111 | 6,756 | 17.2% | 103 |
| In Style (5B) | 1,723 | 9.3% | 416 | 3,209 | 8.2% | 393 |
| Emerald City (8B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Front Porches (8E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Old and Newcomers (8F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Hometown Heritage (8G) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Retirement Communities (9E) | 2,003 | 10.8% | 909 | 3,547 | 9.0% | 876 |
| Social Security Set (9F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Young and Restless (11B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Set to Impress (11D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| City Commons (11E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Traditional Living (12B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| College Towns (14B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Dorms to Diplomas (14C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |

Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri

| Tapestry Urbanization Groups | 2024 Households | | | 2024 Adult Population | | |
|---------------------------------|-----------------|--------------|------------|-----------------------|--------------|------------|
| | Number | Percent | Index | Number | Percent | Index |
| Total: | 18,587 | 100.0% | | 39,201 | 100.0% | |
| 4. Suburban Periphery | 11,908 | 64.1% | 198 | 26,565 | 67.8% | 202 |
| Top Tier (1A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Professional Pride (1B) | 411 | 2.2% | 134 | 829 | 2.1% | 114 |
| Boomburbs (1C) | 2,770 | 14.9% | 755 | 6,464 | 16.5% | 753 |
| Savvy Suburbanites (1D) | 2,121 | 11.4% | 385 | 4,814 | 12.3% | 385 |
| Exurbanites (1E) | 150 | 0.8% | 42 | 186 | 0.5% | 24 |
| Urban Chic (2A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Pleasantville (2B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Enterprising Professionals (2D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Workday Drive (4A) | 1,729 | 9.3% | 300 | 3,874 | 9.9% | 297 |
| Home Improvement (4B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Comfortable Empty Nesters (5A) | 1,610 | 8.7% | 359 | 3,119 | 8.0% | 332 |
| Parks and Rec (5C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Midlife Constants (5E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Up and Coming Families (7A) | 3,117 | 16.8% | 577 | 7,279 | 18.6% | 589 |
| Silver & Gold (9A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Golden Years (9B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| The Elders (9C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Military Proximity (14A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 5. Semirural | 566 | 3.0% | 32 | 1,473 | 3.8% | 41 |
| Middleburg (4C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Heartland Communities (6F) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Farm to Table (7E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Senior Escapes (9D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Down the Road (10D) | 566 | 3.0% | 260 | 1,473 | 3.8% | 320 |
| Small Town Sincerity (12C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| 6. Rural | 1,186 | 6.4% | 39 | 2,543 | 6.5% | 40 |
| Green Acres (6A) | 1,186 | 6.4% | 193 | 2,543 | 6.5% | 192 |
| Salt of the Earth (6B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| The Great Outdoors (6C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Prairie Living (6D) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rural Resort Dwellers (6E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Southern Satellites (10A) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rooted Rural (10B) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Economic BedRock (10C) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Rural Bypasses (10E) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |
| Unclassified (15) | 0 | 0.0% | 0 | 0 | 0.0% | 0 |

Data Note: This report identifies neighborhood segments in the area, and describes the socioeconomic quality of the immediate neighborhood. The index is a comparison of the percent of households or Total Population 18+ in the area, by Tapestry segment, to the percent of households or Total Population 18+ in the United States, by segment. An index of 100 is the US average.

Source: Esri



LifeMode Group: Sprouting Explorers

Up and Coming Families

7A

Households: 2,901,200

Average Household Size: 3.12

Median Age: 31.4

Median Household Income: \$72,000

WHO ARE WE?

Up and Coming Families is a market in transition—residents are younger and more mobile than the previous generation. They are ambitious, working hard to get ahead, and willing to take some risks to achieve their goals. The recession has impacted their financial well-being, but they are optimistic. Their homes are new; their families are young. And this is one of the fastest-growing markets in the country.

OUR NEIGHBORHOOD

- New suburban periphery: new families in new housing subdivisions.
- Building began in the housing boom of the 2000s and continues in this fast-growing market.
- Single-family homes with a median value of \$194,400 and a lower vacancy rate.
- The price of affordable housing; longer commute times (Index 217).

SOCIOECONOMIC TRAITS

- Education: 67% have some college education or degree(s).
- Hardworking labor force with a participation rate of 71% (Index 114).
- Most households (61%) have two or more workers.
- Careful shoppers, aware of prices, willing to shop around for the best deals and open to influence by others' opinions.
- Seek the latest and best in technology.
- Young families still feathering the nest and establishing their style.



Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.

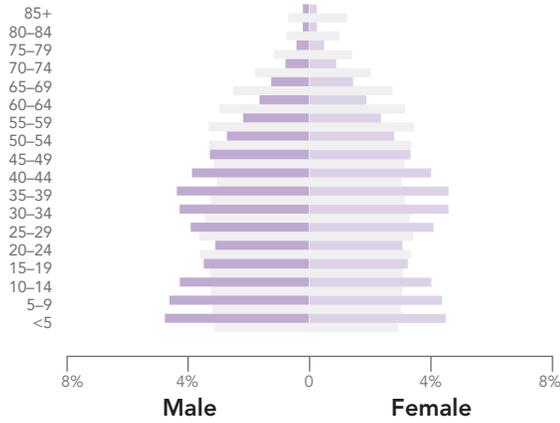
Up and Coming Families



AGE BY SEX (Esri data)

Median Age: **31.4** US: 38.2

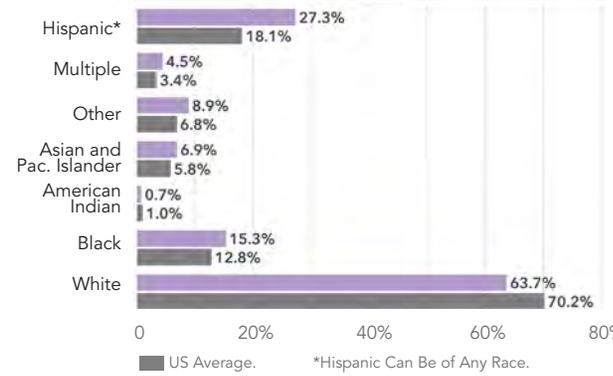
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **73.9** US: 64.0



INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

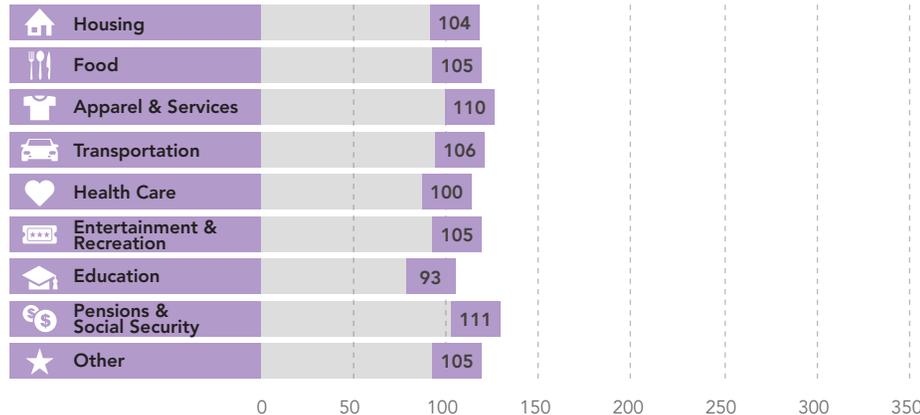


Median Net Worth



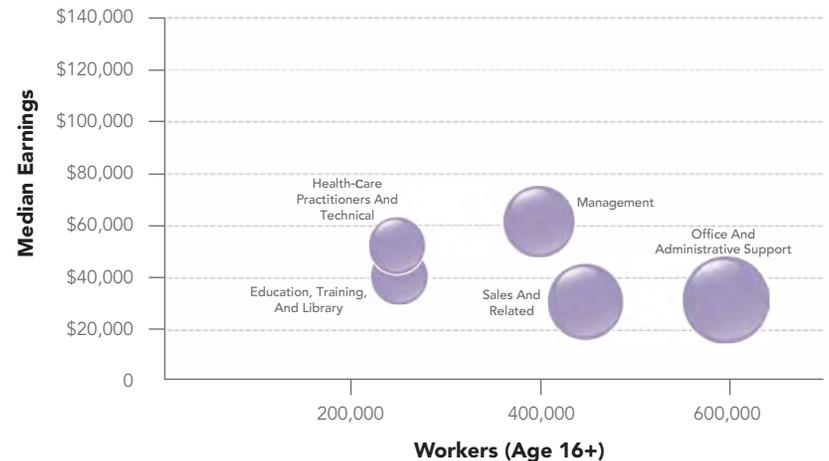
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Rely on the internet for entertainment, information, shopping, and banking.
- Prefer imported SUVs or compact cars, late models.
- Carry debt from credit card balances to student loans and mortgages but also maintain retirement plans and make charitable contributions.
- Busy with work and family; use home and landscaping services to save time.
- Find leisure in family activities, movies at home, trips to theme parks or the zoo, and sports, from golfing and weight lifting to taking a jog or run.

HOUSING

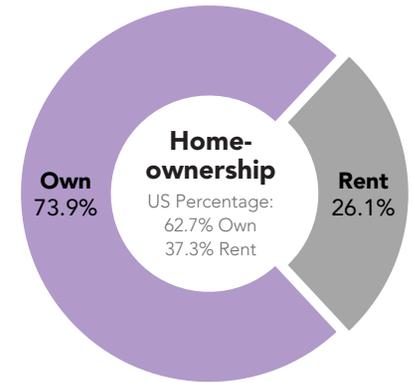
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family

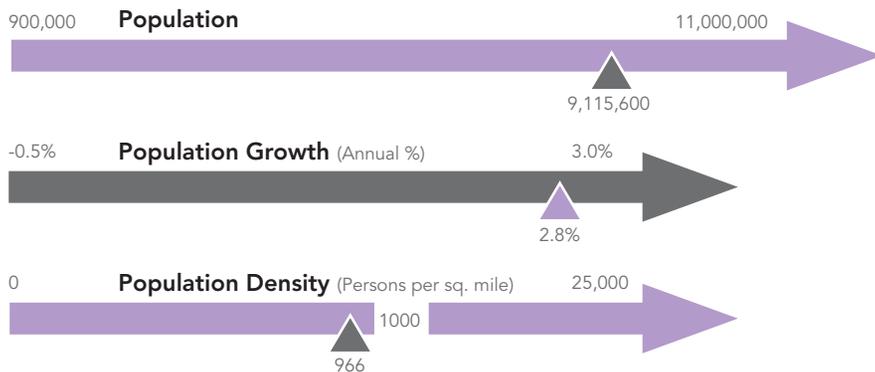
Median Value:
\$194,400

US Median: \$207,300



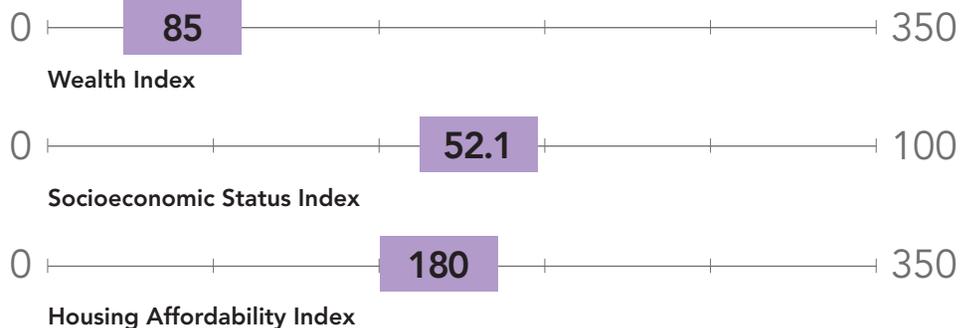
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.

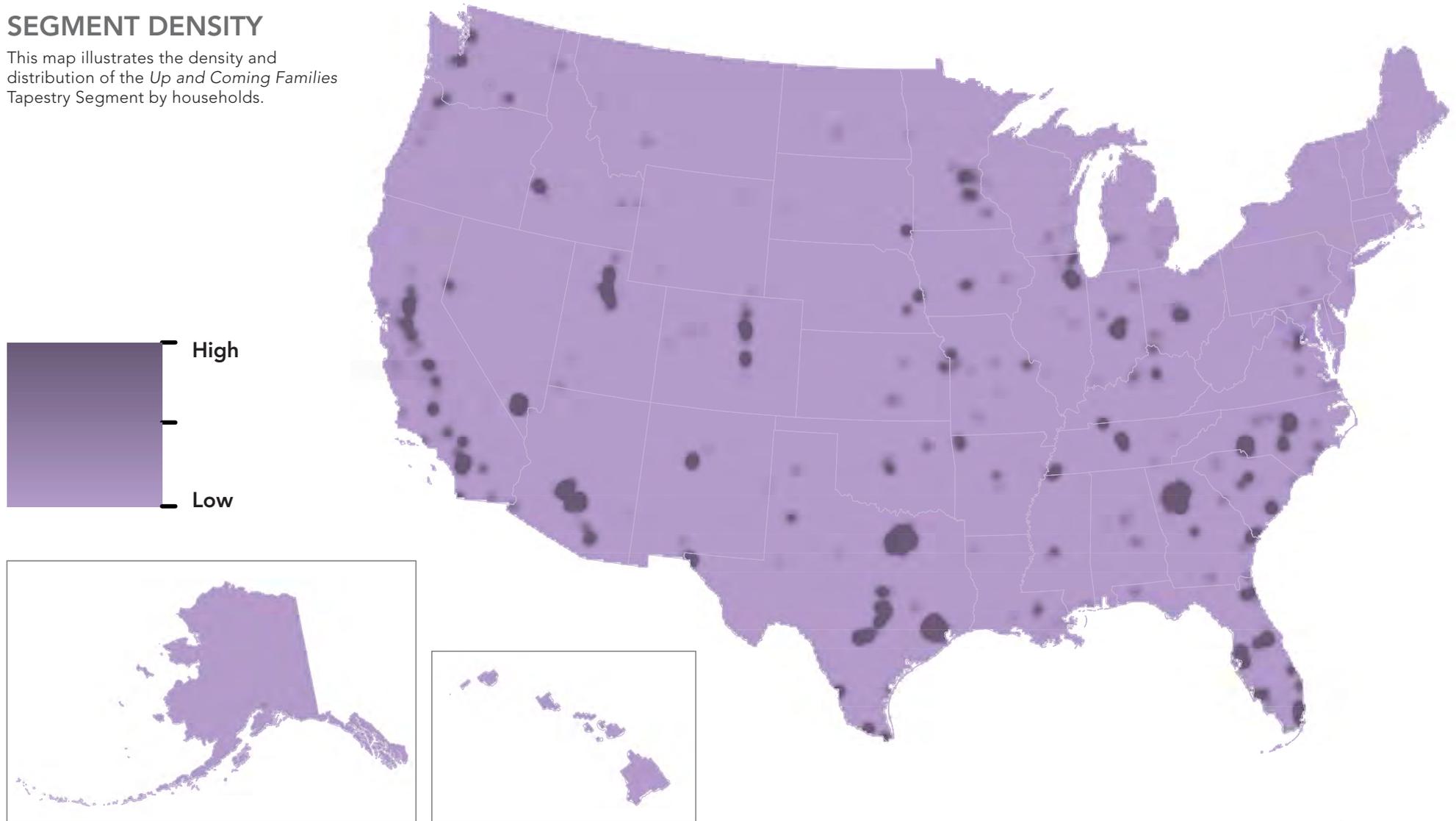


Up and Coming Families



SEGMENT DENSITY

This map illustrates the density and distribution of the *Up and Coming Families* Tapestry Segment by households.



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LifeMode Group: Affluent Estates

Boomburbs



Households: 2,004,400

Average Household Size: 3.25

Median Age: 34.0

Median Household Income: \$113,400

WHO ARE WE?

This is the new growth market, with a profile similar to the original: young professionals with families that have opted to trade up to the newest housing in the suburbs. The original *Boomburbs* neighborhoods began growing in the 1990s and continued through the peak of the housing boom. Most of those neighborhoods are fully developed now. This is an affluent market but with a higher proportion of mortgages. Rapid growth still distinguishes the *Boomburbs* neighborhoods, although the boom is more subdued now than it was 10 years ago. So is the housing market. Residents are well-educated professionals with a running start on prosperity.

OUR NEIGHBORHOOD

- Growth markets are in the suburban periphery of large metropolitan areas.
- Young families are married with children (Index 220); average household size is 3.25.
- Homeownership is 84% (Index 134), with the highest rate of mortgages, 71.5% (Index 173).
- Primarily single-family homes in new neighborhoods, 66% built since 2000 (Index 441).
- Median home value is \$350,000 (Index 169).
- Lower housing vacancy rate at 3.7%.
- The cost of affordable new housing comes at the expense of one of the longest commutes to work, over 30 minutes average, including a disproportionate number (33.6%) commuting across county lines (Index 141).

SOCIOECONOMIC TRAITS

- Well-educated young professionals, 55% are college graduates (Index 178).
- High labor force participation at 71.3% (Index 114); most households have more than two workers (Index 124).
- Longer commute times from the suburban growth corridors have created more home workers (Index 156).
- Well connected, own the latest devices and understand how to use them efficiently; biggest complaints—too many devices and too many intrusions on personal time.
- Financial planning is well under way for these professionals.



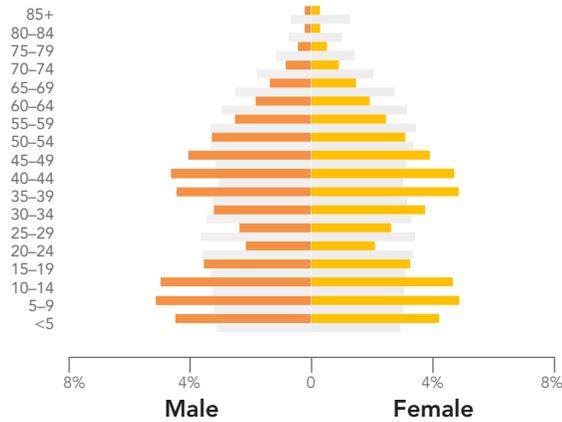
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **34.0** US: 38.2

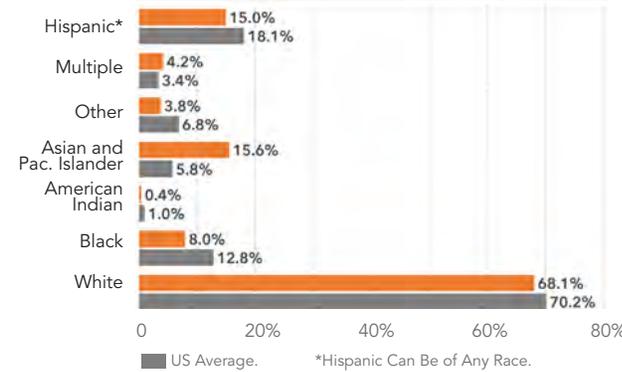
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **63.2** US: 64.0



INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

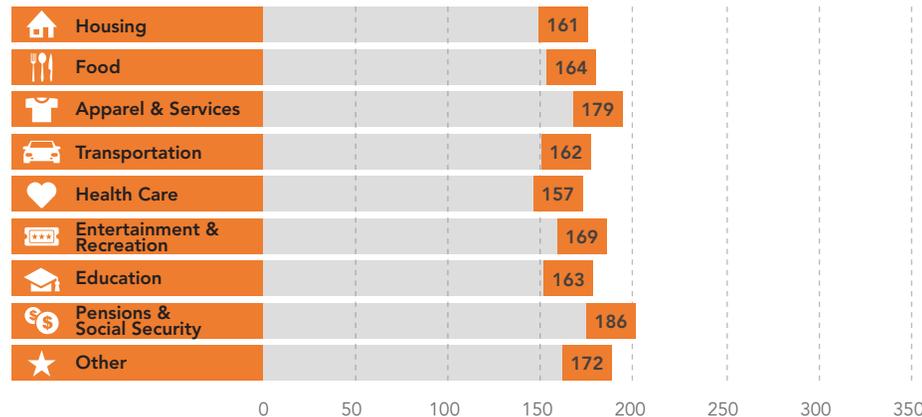


Median Net Worth



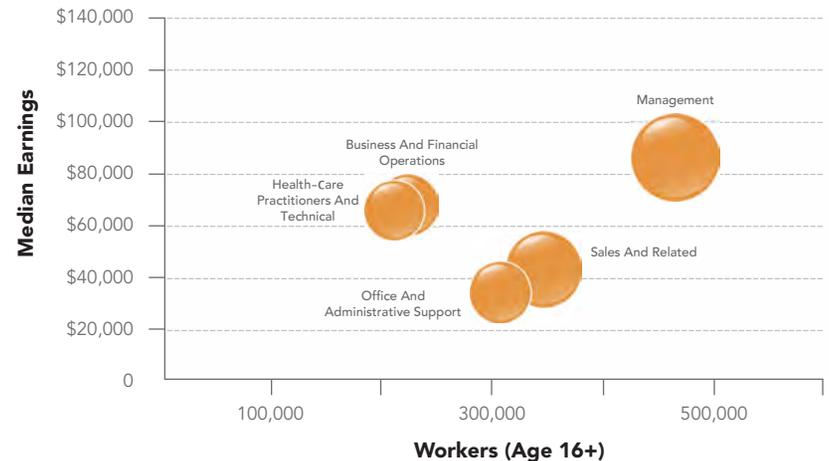
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- *Boomburbs* residents prefer late model imports, primarily SUVs, and also luxury cars and minivans.
- This is one of the top markets for the latest in technology, from smartphones to tablets to internet connectable televisions.
- Style matters in the *Boomburbs*, from personal appearance to their homes. These consumers are still furnishing their new homes and already remodeling.
- Enjoy gardening but more often contract for home services.
- Physical fitness is a priority, including club memberships and home equipment.
- Leisure includes a range of activities from sports (hiking, bicycling, swimming, golf) to visits to theme parks or water parks.
- Residents are generous supporters of charitable organizations.

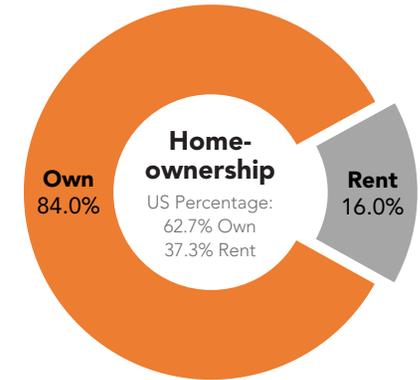
HOUSING

Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



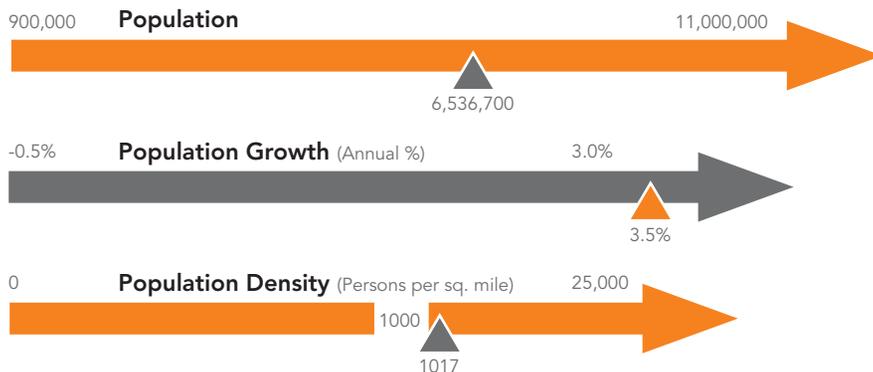
Typical Housing:
Single Family

Median Value:
\$350,000
US Median: \$207,300



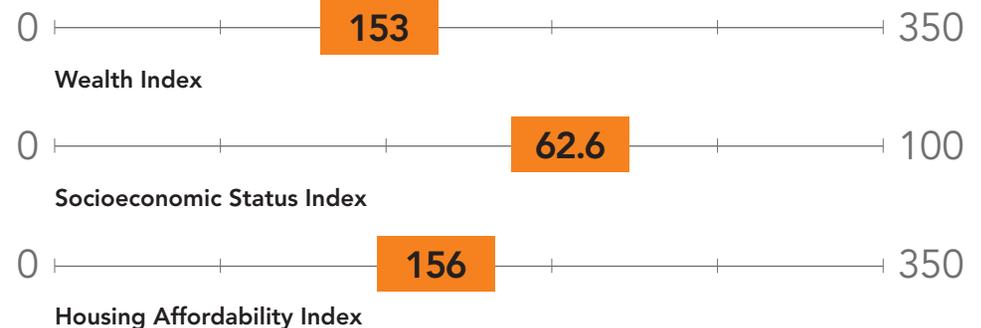
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.



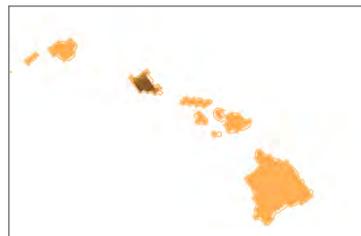
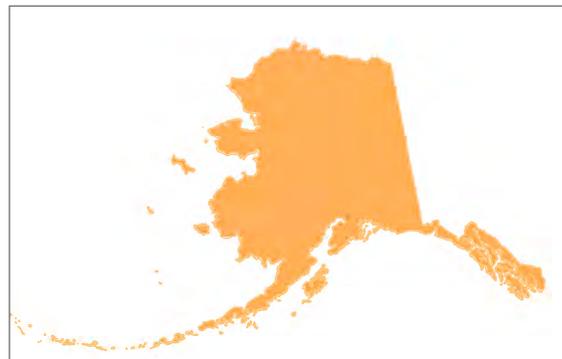
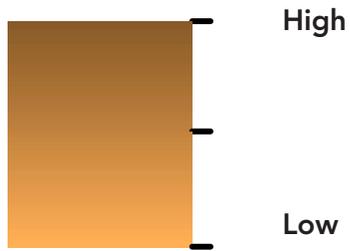
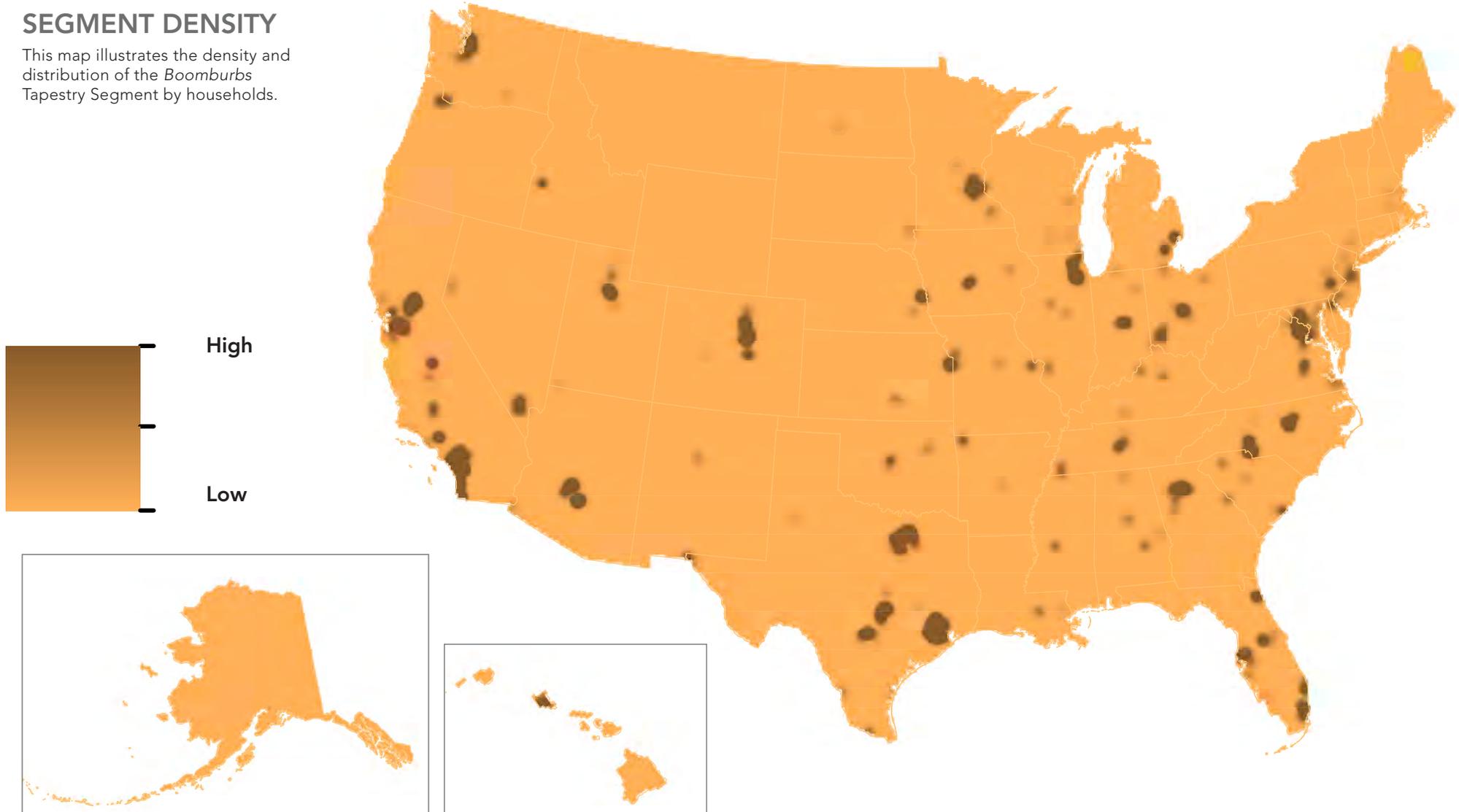


Boomburbs



SEGMENT DENSITY

This map illustrates the density and distribution of the *Boomburbs* Tapestry Segment by households.



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LifeMode Group: Affluent Estates

Savvy Suburbanites



Households: 3,664,200

Average Household Size: 2.85

Median Age: 45.1

Median Household Income: \$108,700

WHO ARE WE?

Savvy Suburbanites residents are well educated, well read, and well capitalized. Families include empty nesters and empty nester wannabes, who still have adult children at home. Located in older neighborhoods outside the urban core, their suburban lifestyle includes home remodeling and gardening plus the active pursuit of sports and exercise. They enjoy good food and wine, plus the amenities of the city's cultural events.

OUR NEIGHBORHOOD

- Established neighborhoods (most built between 1970 and 1990) found in the suburban periphery of large metropolitan markets.
- Married couples with no children or older children; average household size is 2.85.
- 91% owner occupied; 66% mortgaged (Index 160).
- Primarily single-family homes, with a median value of \$362,900 (Index 161).
- Low vacancy rate at 3.8%.

SOCIOECONOMIC TRAITS

- Education: 50.6% college graduates; 77.6% with some college education.
- Higher labor force participation rate at 67.9% (Index 109) with proportionately more 2-worker households at 62.2% (Index 120).
- Well-connected consumers that appreciate technology and make liberal use of it for everything from shopping and banking to staying current and communicating.
- Informed shoppers that do their research prior to purchasing and focus on quality.



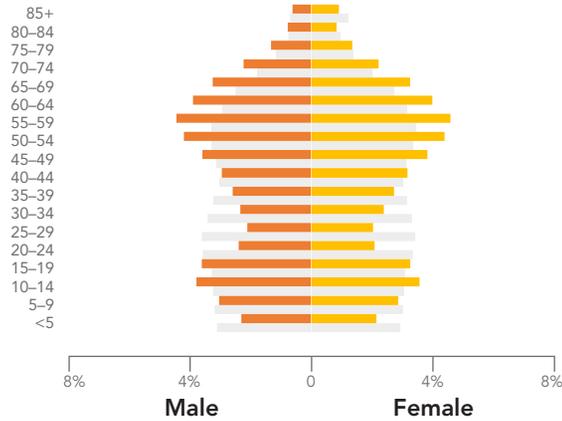
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: 45.1 US: 38.2

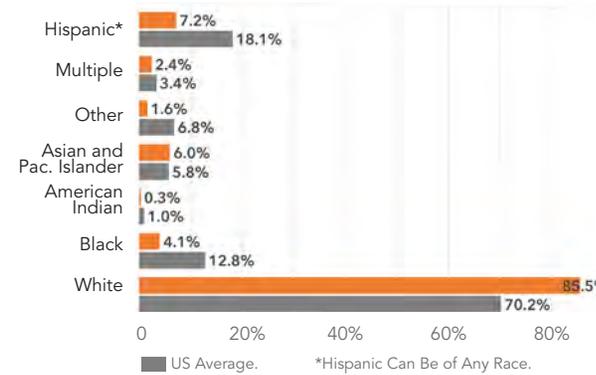
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: 36.2 US: 64.0



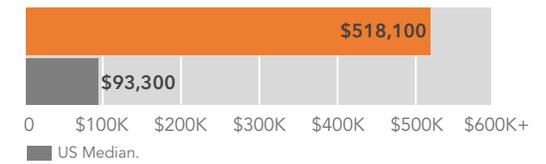
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

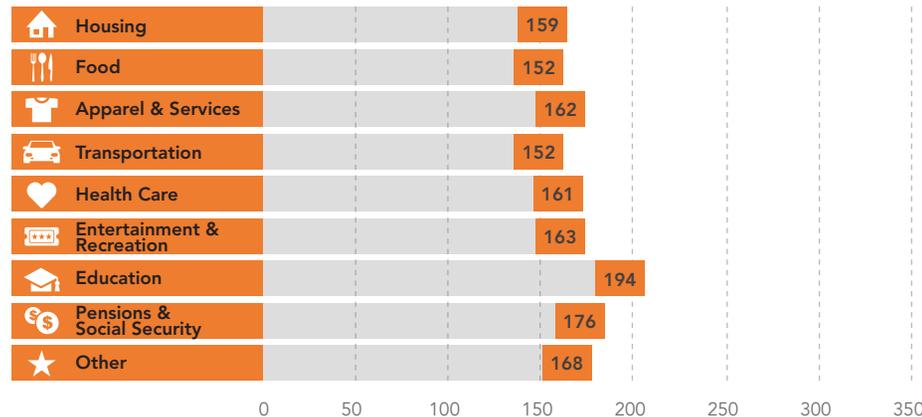


Median Net Worth



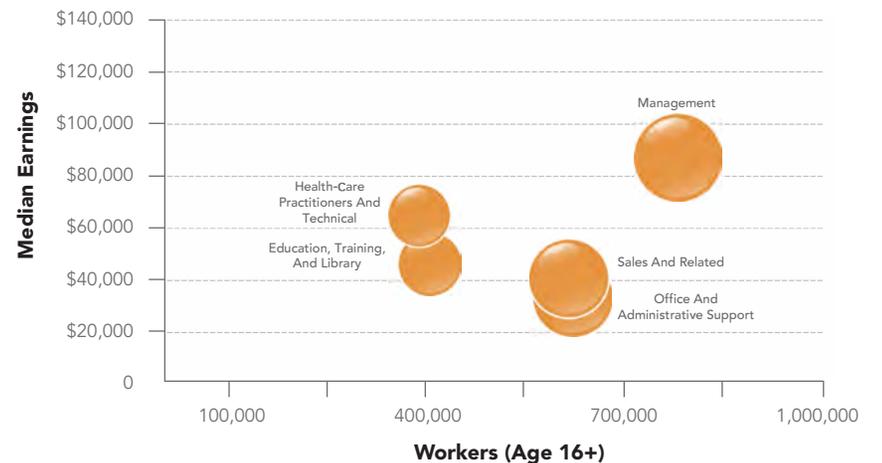
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Residents prefer late model, family-oriented vehicles: SUVs, minivans, and station wagons.
- Gardening and home remodeling are priorities, usually DIY. Riding mowers and power tools are popular, although they also hire contractors for the heavy lifting.
- There is extensive use of housekeeping and personal care services.
- Foodies: They like to cook and prefer natural or organic products.
- These investors are financially active, using a number of resources for informed investing. They are not afraid of debt; many households carry first and second mortgages, plus home equity credit lines.
- Physically fit, residents actively pursue a number of sports, from skiing to golf, and invest heavily in sports gear and exercise equipment.

HOUSING

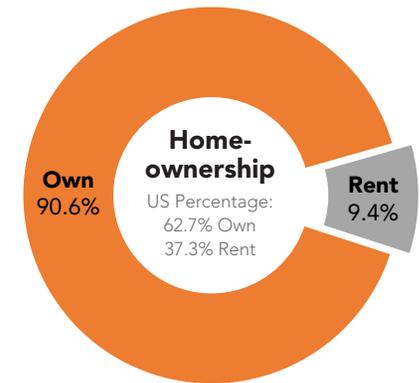
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
 Single Family

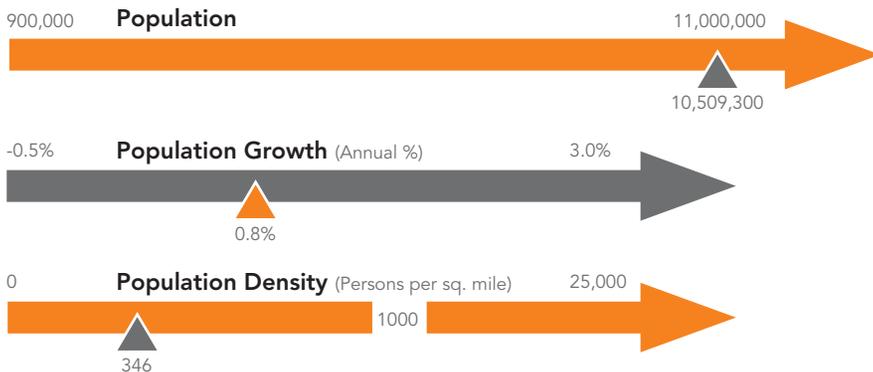
Median Value:
 \$362,900

US Median: \$207,300



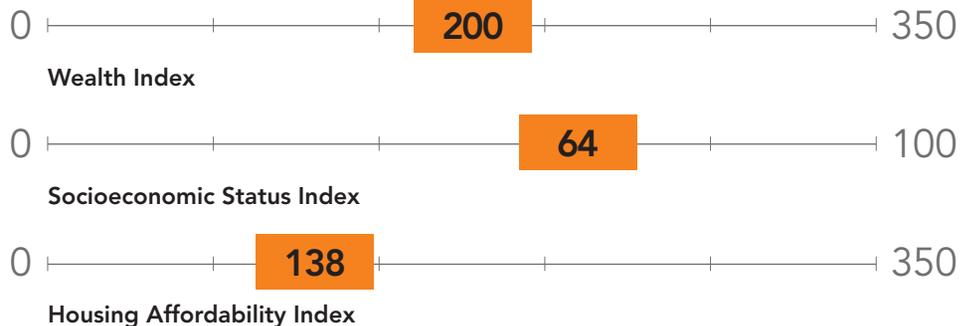
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

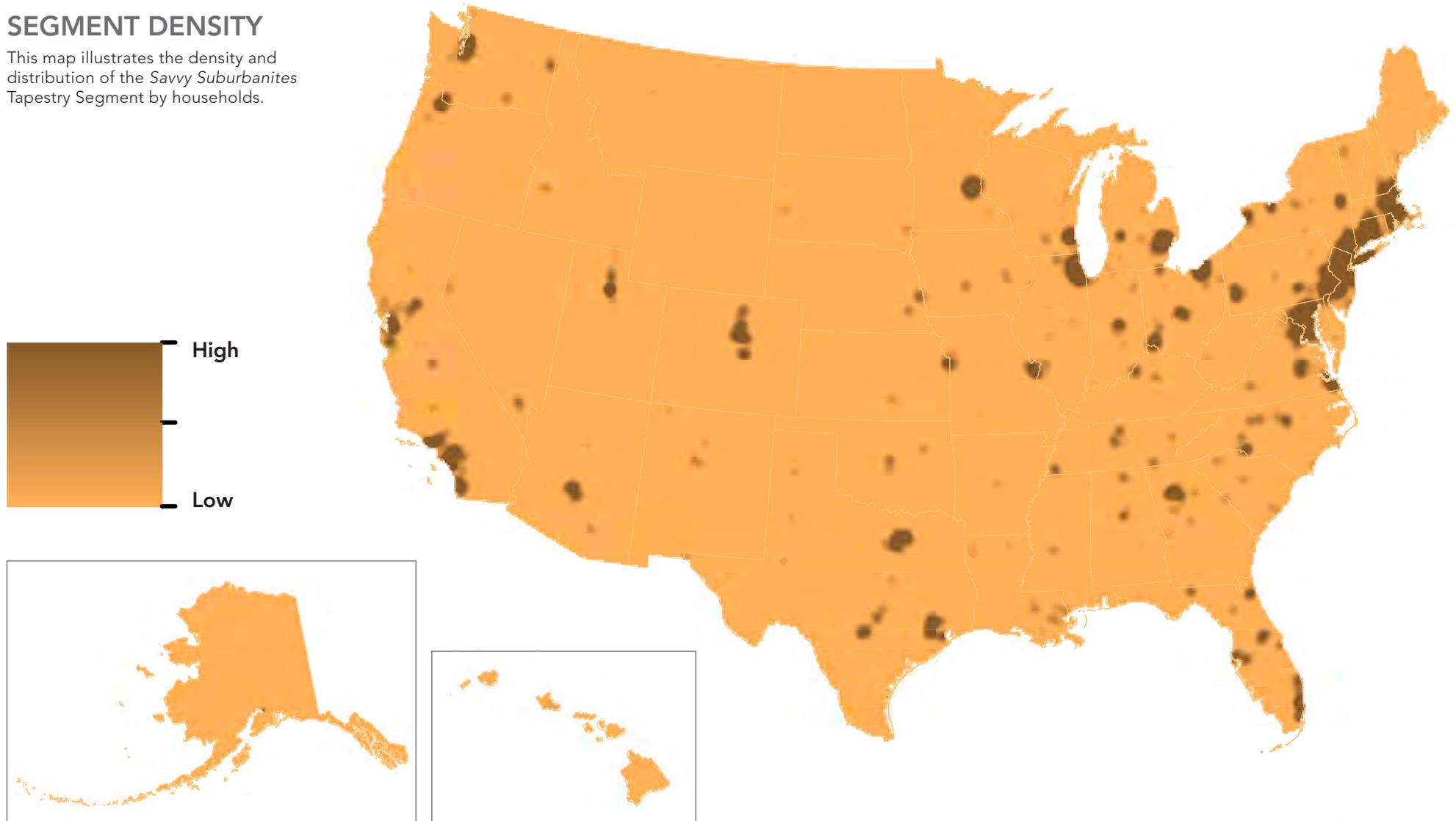
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Savvy Suburbanites* Tapestry Segment by households.



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LifeMode Group: Senior Styles

Retirement Communities



Households: 1,501,100

Average Household Size: 1.88

Median Age: 53.9

Median Household Income: \$40,800

WHO ARE WE?

Retirement Communities neighborhoods are evenly distributed across the country. They combine single-family homes and independent living with apartments, assisted living, and continuous care nursing facilities. Over half of the housing units are in multiunit structures, and the majority of residents have a lease. This group enjoys watching cable TV and stays up-to-date with newspapers and magazines. Residents take pride in fiscal responsibility and keep a close eye on their finances. Although income and net worth are below national averages, residents enjoy going to the movies, fishing, and taking vacations. While some residents enjoy cooking, many would rather dine out.

OUR NEIGHBORHOOD

- Much of the housing was built in the 1970s and 1980s—a mix of single-family homes and large multiunit structures that function at various levels of senior care.
- Small household size; many residents have outlived their partners and live alone.
- Over half of the homes are renter occupied.
- Average rent is slightly below the US average.
- Nearly one in five households has no vehicle.

SOCIOECONOMIC TRAITS

- Brand loyal, this segment will spend a little more for their favorite brands, but most likely they will have a coupon.
- Frugal, they pay close attention to finances.
- They prefer reading magazines over interacting with computers.
- They are health conscious and prefer name-brand drugs.



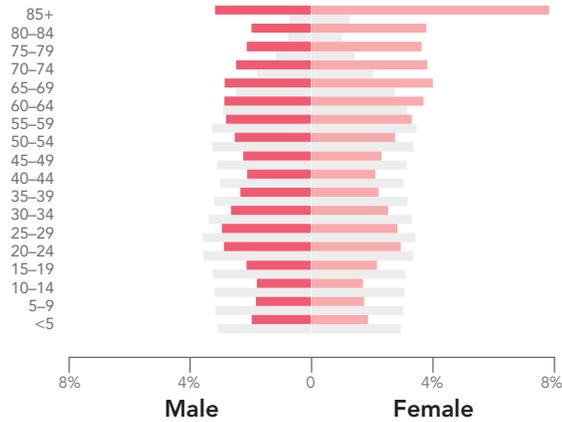
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **53.9** US: 38.2

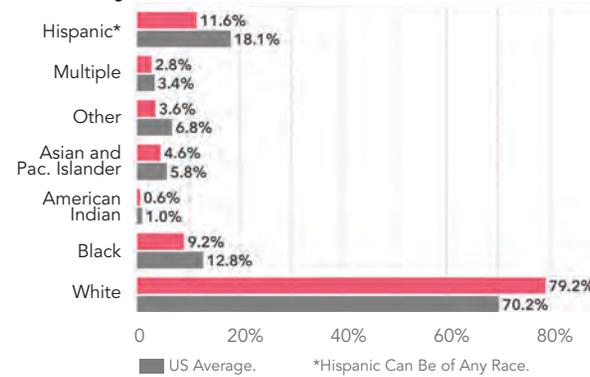
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **49.4** US: 64.0



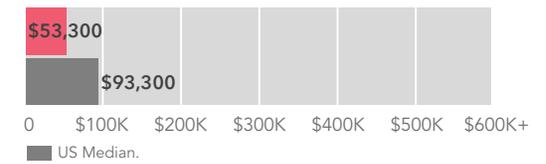
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

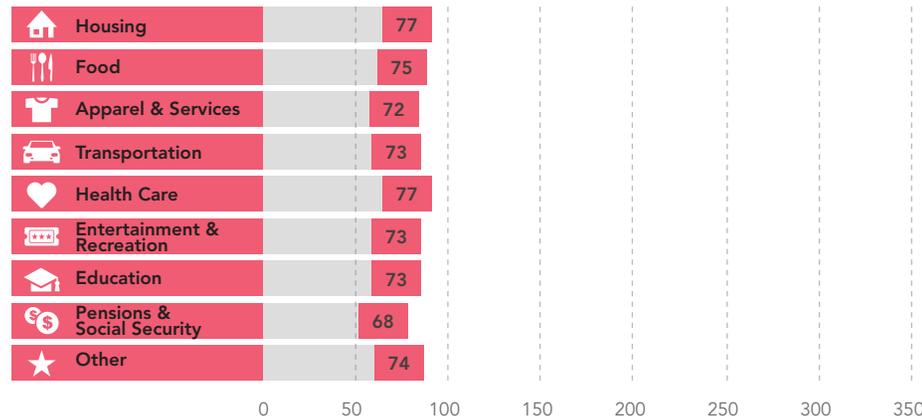


Median Net Worth



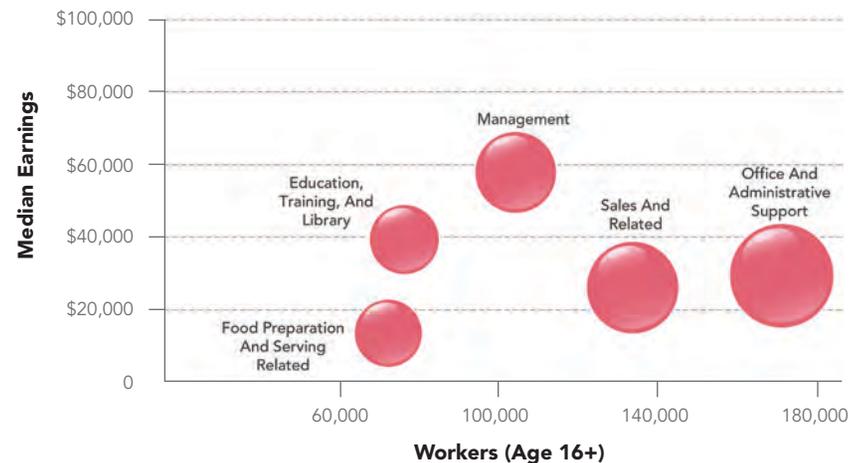
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Enjoy hard-cover books, book clubs, crossword puzzles, and Sudoku.
- Contribute to political organizations and other groups.
- Entertainment preferences: playing cards and reading books.
- Prefer watching cable TV, including premium channels like HBO and Cinemax.
- Like to travel—including visits to foreign countries.
- Shop at diverse, large retail chains like Sears, Family Dollar, Target, and Walmart for convenience.

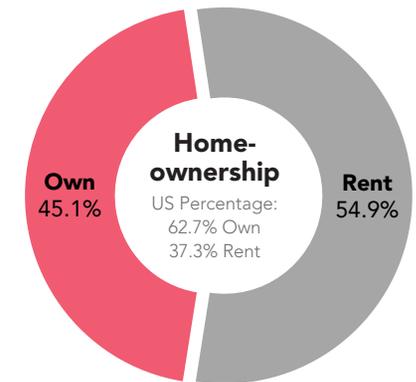
HOUSING

Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



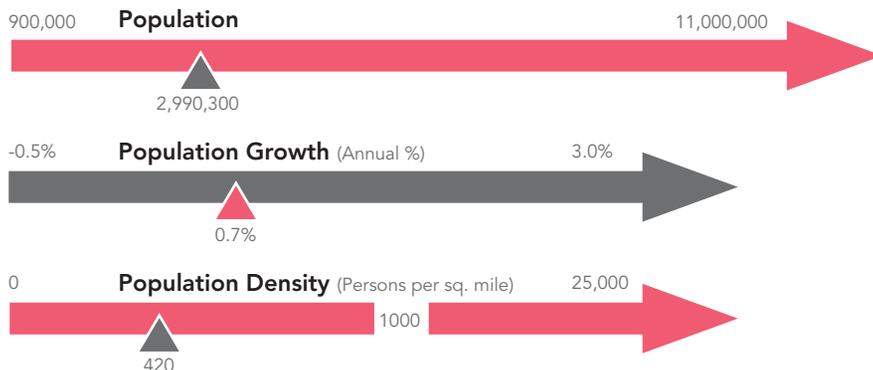
Typical Housing:
Multiunits; Single Family

Average Rent:
\$1,013
US Average: \$1,038



POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.



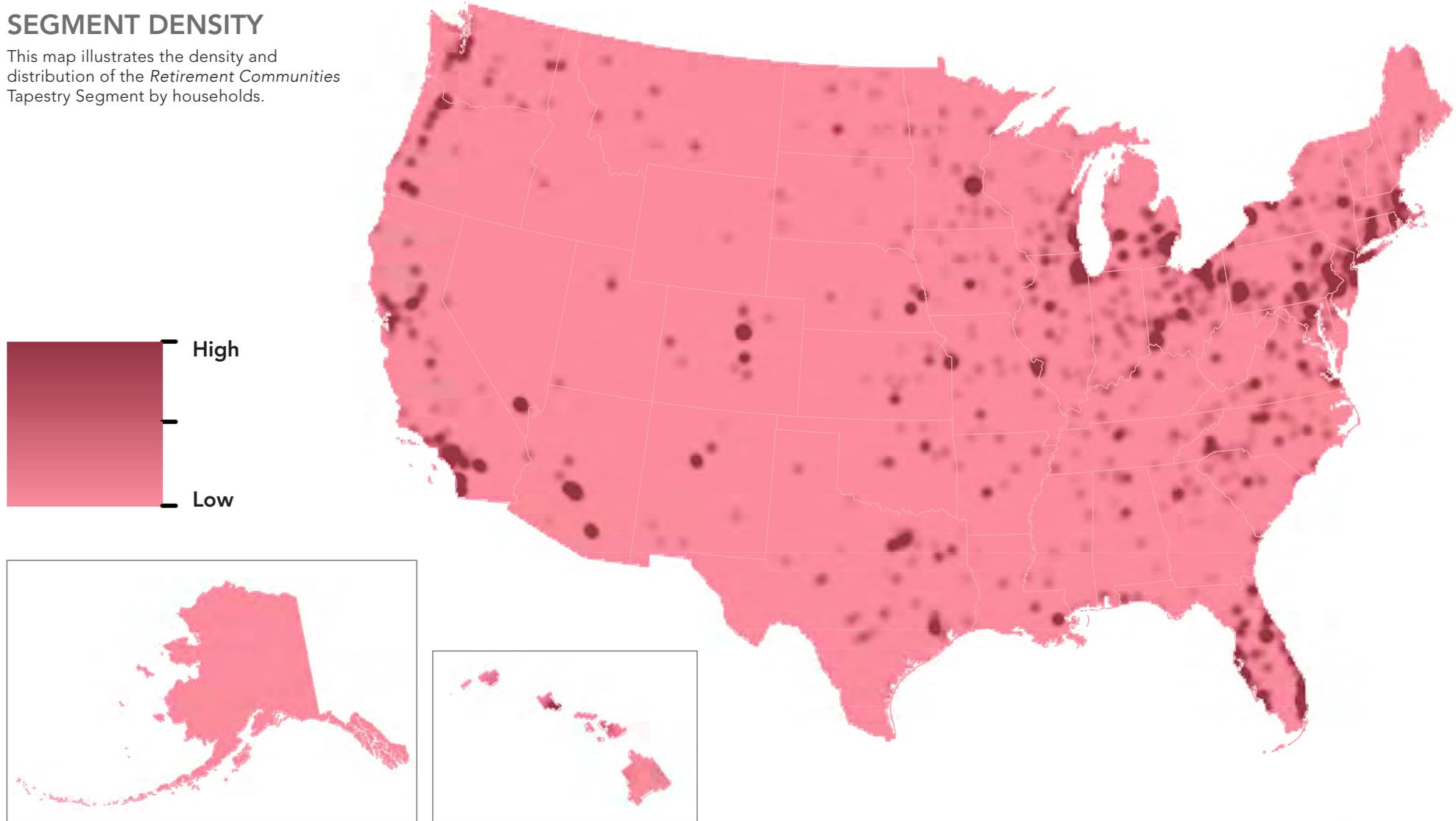


Retirement Communities



SEGMENT DENSITY

This map illustrates the density and distribution of the *Retirement Communities* Tapestry Segment by households.



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LifeMode Group: Family Landscapes

Workday Drive

4A

Households: 3,541,300

Average Household Size: 2.97

Median Age: 37.0

Median Household Income: \$90,500

WHO ARE WE?

Workday Drive is an affluent, family-oriented market with a country flavor. Residents are partial to new housing away from the bustle of the city but close enough to commute to professional job centers. Life in this suburban wilderness offsets the hectic pace of two working parents with growing children. They favor time-saving devices, like banking online or housekeeping services, and family-oriented pursuits.

OUR NEIGHBORHOOD

- *Workday Drive* residents prefer the suburban periphery of metropolitan areas.
- Predominantly single family, homes are in newer neighborhoods, 34% built in the 1990s (Index 236), 31% built since 2000.
- Owner-occupied homes have high rate of mortgages at 68% (Index 164) and low rate vacancy at 4%.
- Median home value is \$257,400.
- Most households are married couples with children; average household size is 2.97.
- Most households have two or three vehicles; long travel time to work including a disproportionate number commuting from a different county (Index 132).

SOCIOECONOMIC TRAITS

- Education: 40.5% college graduates; more than 72% with some college education.
- High labor force participation rate at 71%; two out of three households include two plus workers (Index 124).
- Connected, with a host of wireless devices—anything that enables convenience, like banking, paying bills, or even shopping online.
- Well insured and invested in a range of funds, from savings accounts or bonds to stocks.
- Carry a higher level of debt, including first (Index 149) and second mortgages (Index 154) and auto loans (Index 149).



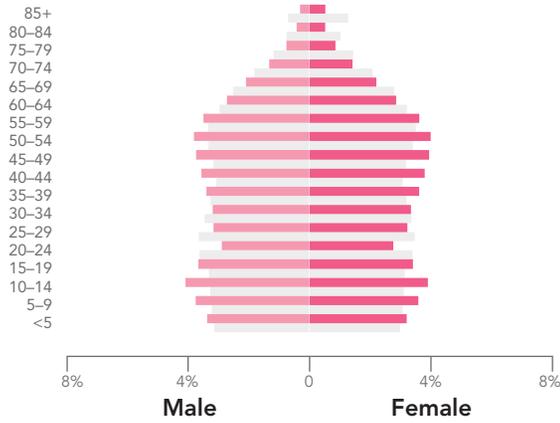
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **37.0** US: 38.2

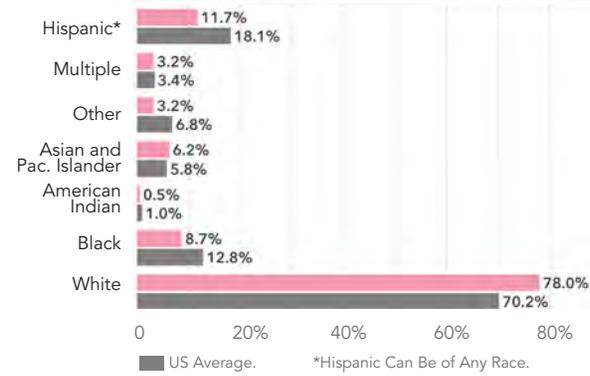
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **50.8** US: 64.0



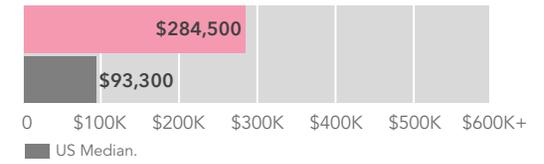
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

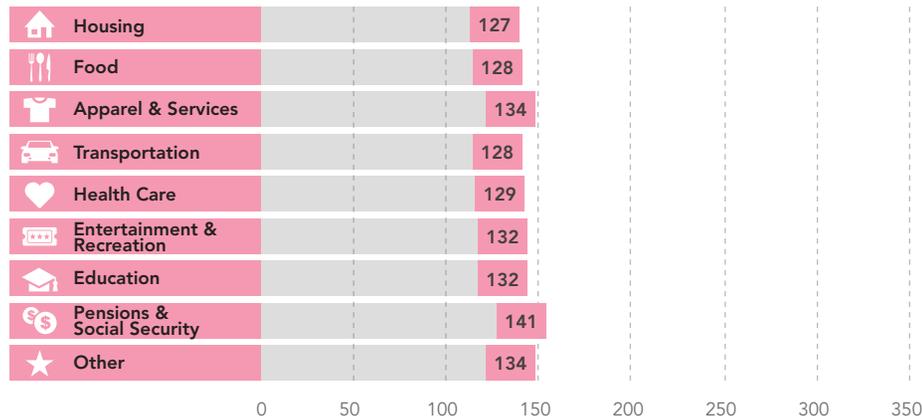


Median Net Worth



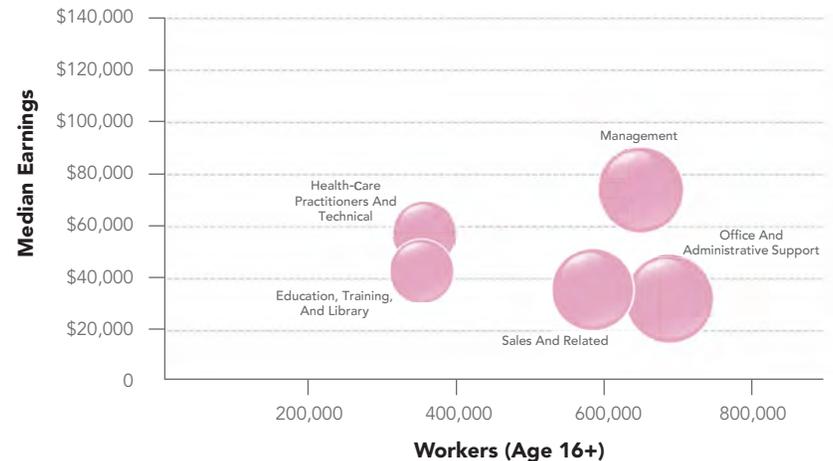
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Most households own at least two vehicles; the most popular types are minivans and SUVs.
- Family-oriented purchases and activities dominate, like four plus televisions (Index 154), movie purchases or rentals, children’s apparel and toys, and visits to theme parks or zoos.
- Outdoor activities and sports are characteristic of life in the suburban periphery. They attend sporting events, as well as participate in them like bicycling, jogging, golfing, and boating.
- Home maintenance services are frequently contracted, but these families also like their gardens and own the tools for minor upkeep, like lawn mowers, trimmers, and blowers.

HOUSING

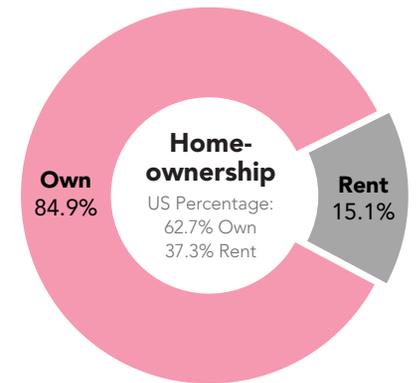
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau’s American Community Survey.



Typical Housing:
Single Family

Median Value:
\$257,400

US Median: \$207,300



POPULATION CHARACTERISTICS

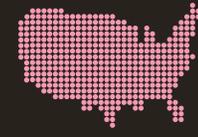
Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

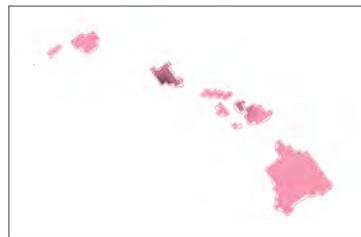
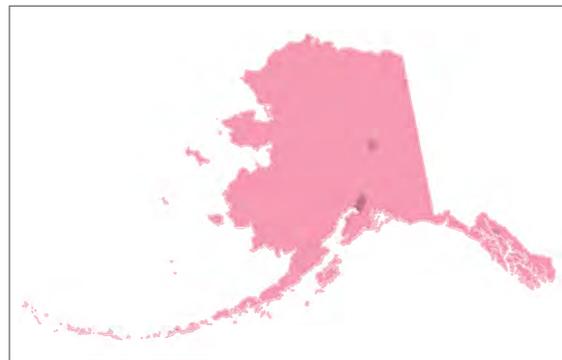
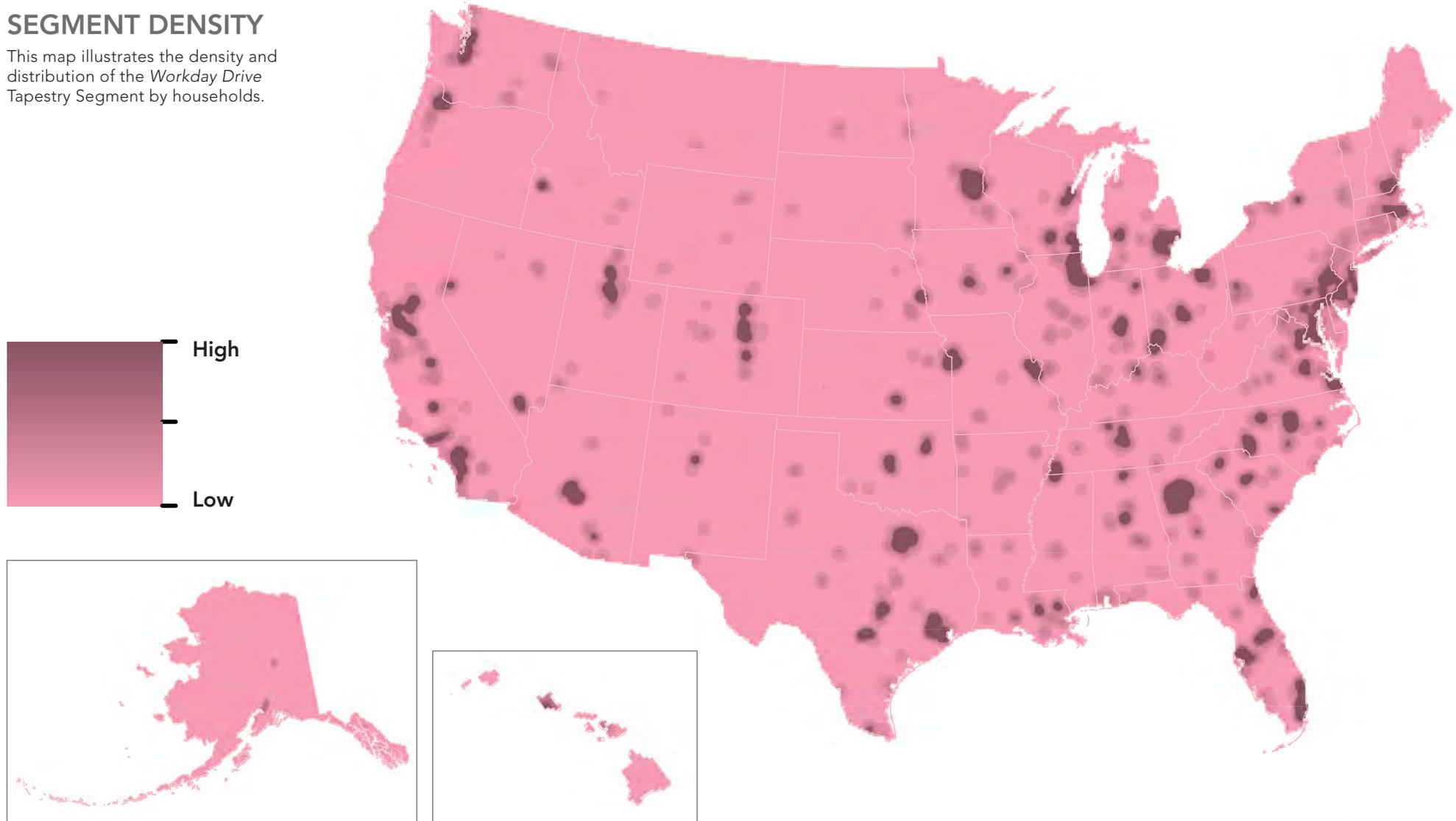
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Workday Drive* Tapestry Segment by households.



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LifeMode Group: GenXurban

In Style

5B

Households: 2,764,500

Average Household Size: 2.35

Median Age: 42.0

Median Household Income: \$73,000

WHO ARE WE?

In Style denizens embrace an urbane lifestyle that includes support of the arts, travel, and extensive reading. They are connected and make full use of the advantages of mobile devices. Professional couples or single households without children, they have the time to focus on their homes and their interests. The population is slightly older and already planning for their retirement.

OUR NEIGHBORHOOD

- City dwellers of large metropolitan areas.
- Married couples, primarily with no children (Index 112) or single households (Index 109); average household size at 2.35.
- Homeownership average at 68% (Index 108); nearly half, 47%, mortgaged (Index 114).
- Primarily single-family homes, in older neighborhoods (built before 1980), with a mix of townhomes (Index 132) and smaller (5–19 units) apartment buildings (Index 110).
- Median home value at \$243,900.
- Vacant housing units at 8.6%.

SOCIOECONOMIC TRAITS

- College educated: 48% are graduates (Index 155); 77% with some college education.
- Higher labor force participation rate is at 67% (Index 108) with proportionately more two-worker households (Index 110).
- Median household income of \$73,000 reveals an affluent market with income supplemented by investments (Index 142) and a substantial net worth (Index 178).
- Connected and knowledgeable, they carry smartphones and use many of the features.
- Attentive to price, they use coupons, especially mobile coupons.



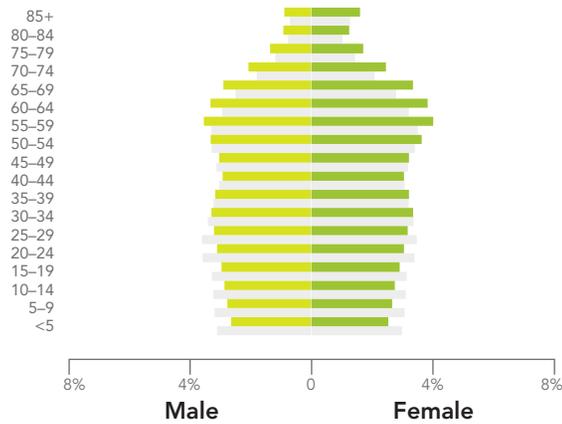
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **42.0** US: 38.2

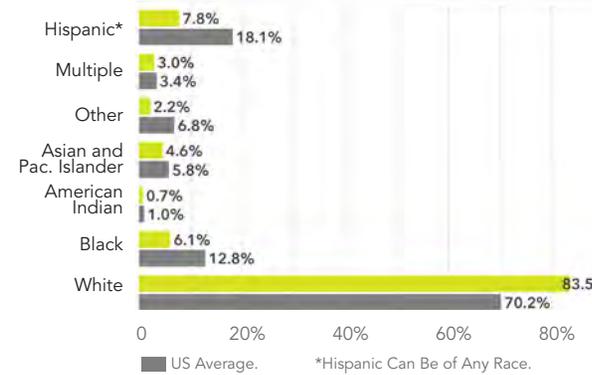
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **39.8** US: 64.0



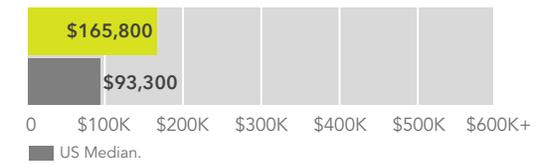
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

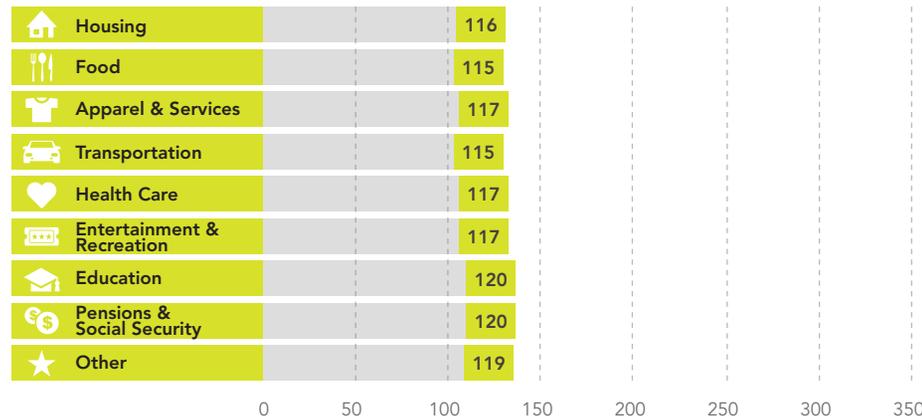


Median Net Worth



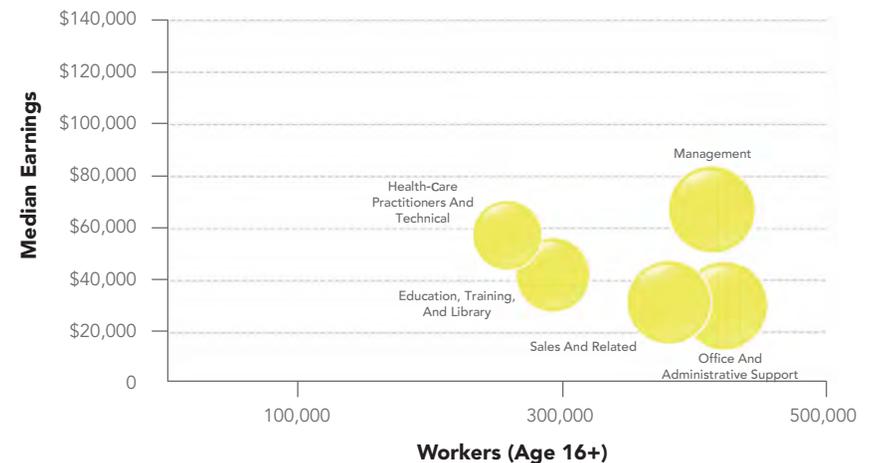
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Partial to late model SUVs: compact SUVs are gaining popularity.
- Homes integral part of their style; invest in home remodeling/maintenance, DIY or contractors; housekeeping hired.
- Prefer organic foods, including growing their own vegetables.
- Financially active, own a variety of investments often managed by a financial planner.
- Meticulous planners, both well insured and well invested in retirement savings.
- Generous with support of various charities and causes.
- Actively support the arts, theater, concerts, and museums.

HOUSING

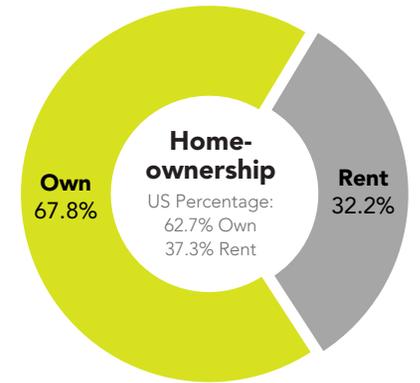
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family

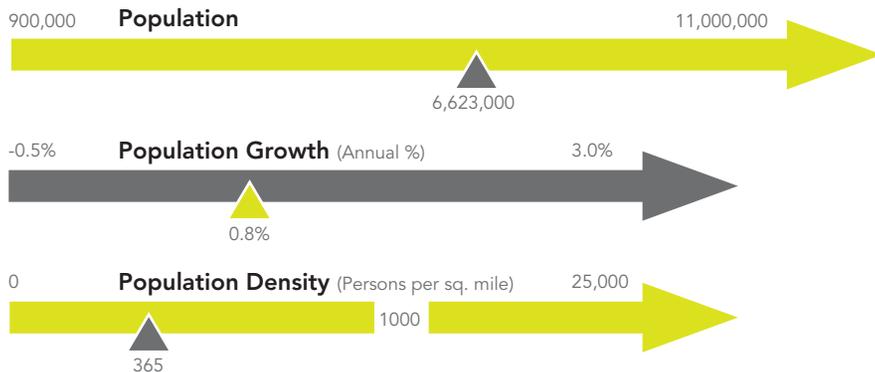
Median Value:
\$243,900

US Median: \$207,300



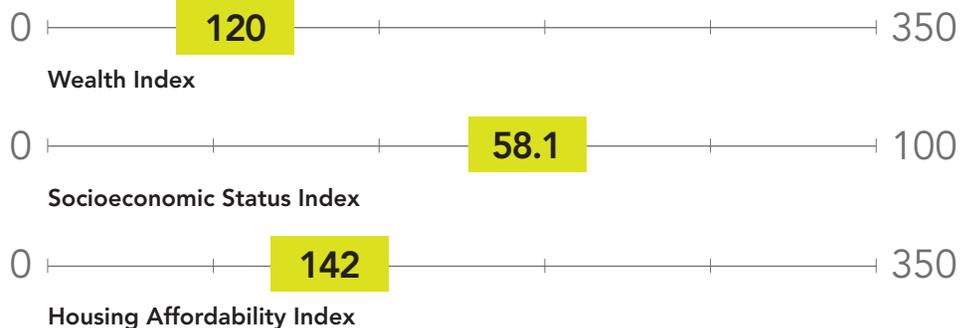
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

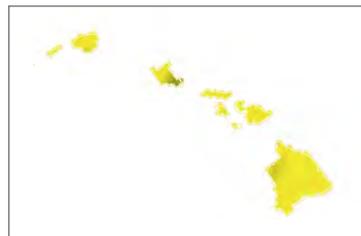
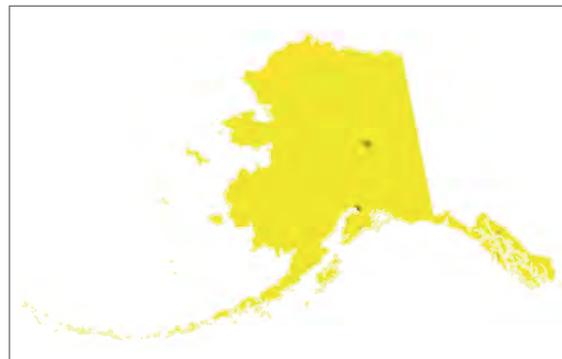
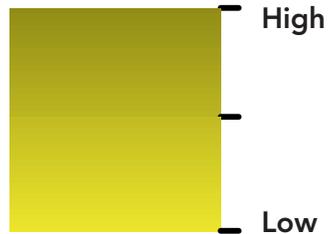
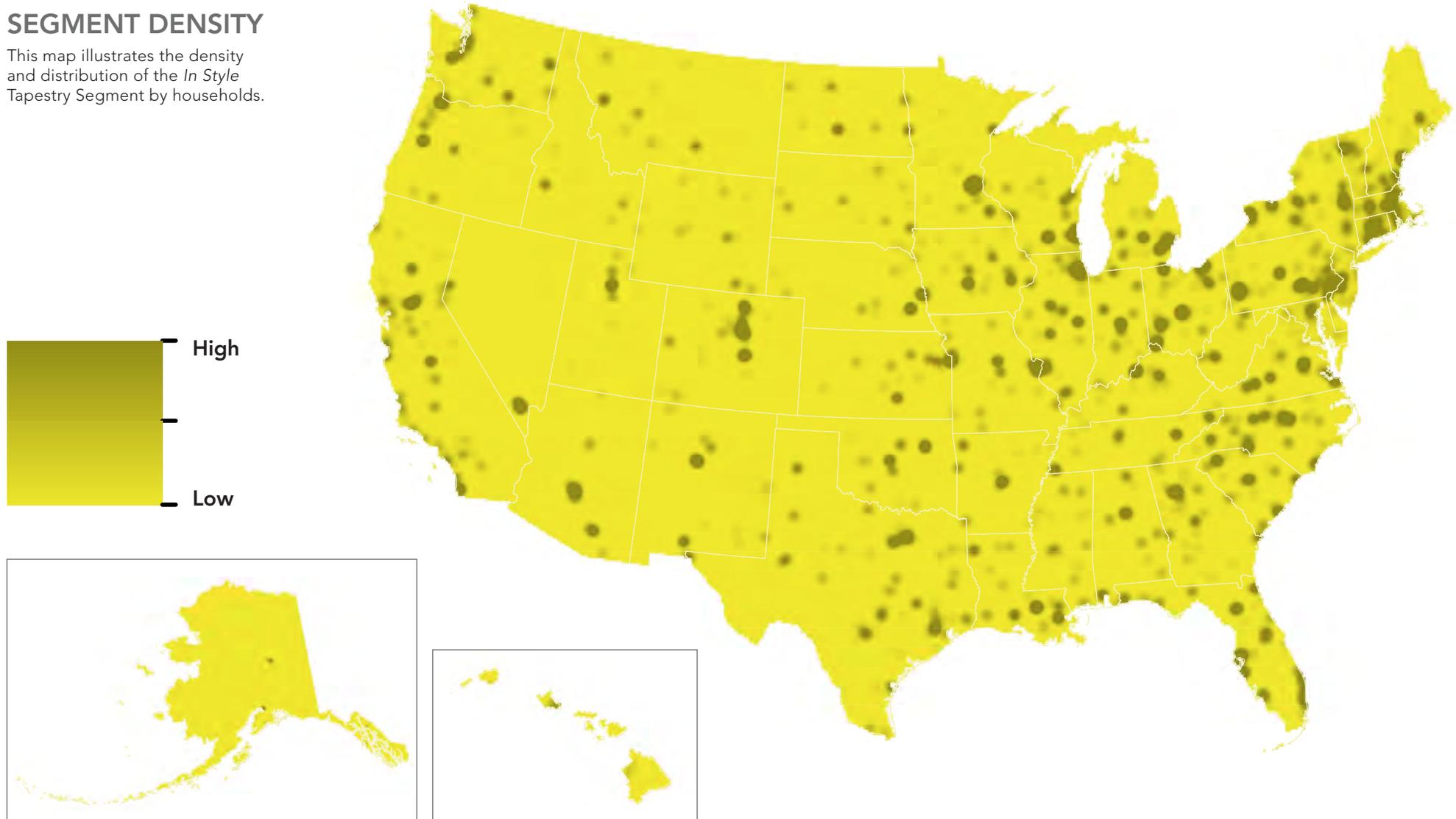
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *In Style* Tapestry Segment by households.





LifeMode Group: GenXurban

Comfortable Empty Nesters



Households: 3,024,200

Average Household Size: 2.52

Median Age: 48.0

Median Household Income: \$75,000

WHO ARE WE?

Residents in this large, growing segment are older, with nearly half of all householders aged 55 or older; many still live in the suburbs where they grew up. Most are professionals working in government, health care, or manufacturing. These Baby Boomers are earning a comfortable living and benefitting from years of prudent investing and saving. Their net worth is well above average (Index 314). Many are enjoying the transition from child rearing to retirement. They value their health and financial well-being.

OUR NEIGHBORHOOD

- Married couples, some with children, but most without (Index 149).
- Average household size slightly higher at 2.52.
- Found throughout the suburbs and small towns of metropolitan areas, where most residents own and live in single-family detached homes (Index 142).
- Most homes built between 1950 and 1990 (Index 131).
- Households generally have one or two vehicles.

SOCIOECONOMIC TRAITS

- Education: 36% college graduates; nearly 68% with some college education.
- Average labor force participation at 61%.
- Most households' income from wages or salaries, but a third also draw income from investments (Index 150) and retirement (Index 159).
- *Comfortable Empty Nesters* residents physically and financially active.
- Prefer eating at home instead of dining out.
- Home maintenance a priority among these homeowners.



Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.

Comfortable Empty Nesters



AGE BY SEX (Esri data)

Median Age: **48.0** US: 38.2

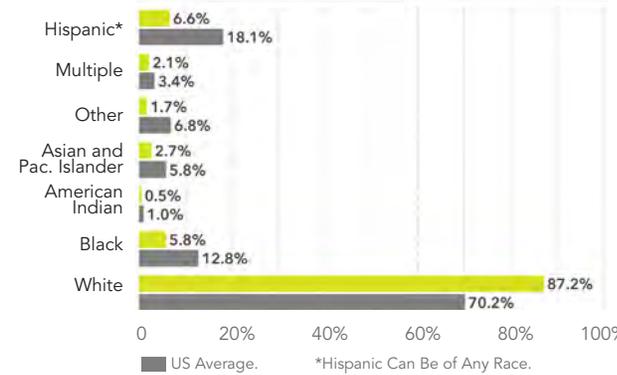
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **33.0** US: 64.0



INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

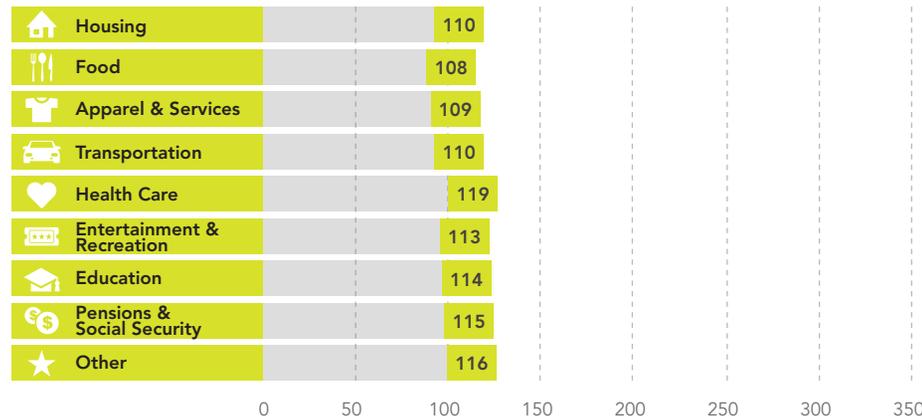


Median Net Worth



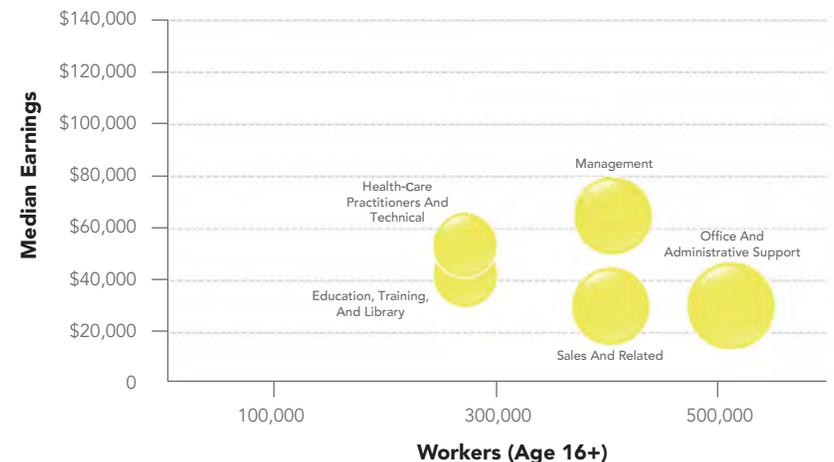
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Residents enjoy listening to sports radio or watching sports on television.
- Physically active, they play golf, ski, ride bicycles, and work out regularly.
- Spending a lot of time online isn't a priority, so most own older home computers.
- Financial portfolio includes stocks, certificates of deposit, mutual funds, and real estate.

HOUSING

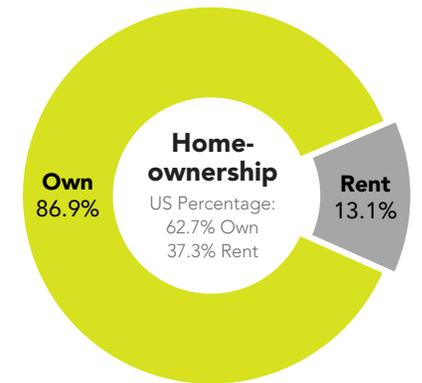
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family

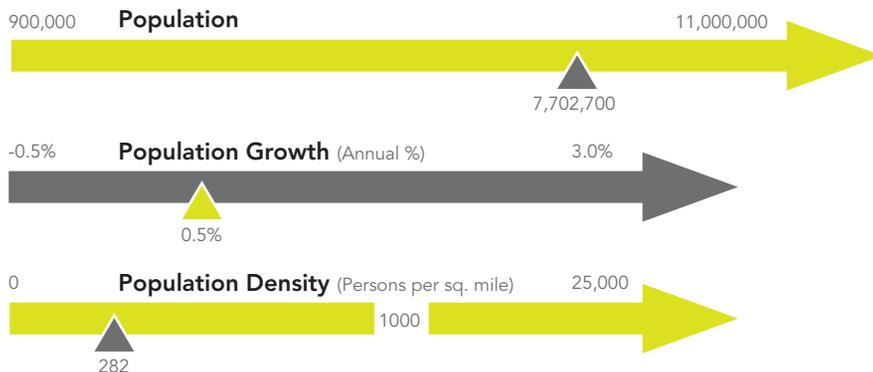
Median Value:
\$203,400

US Median: \$207,300



POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.

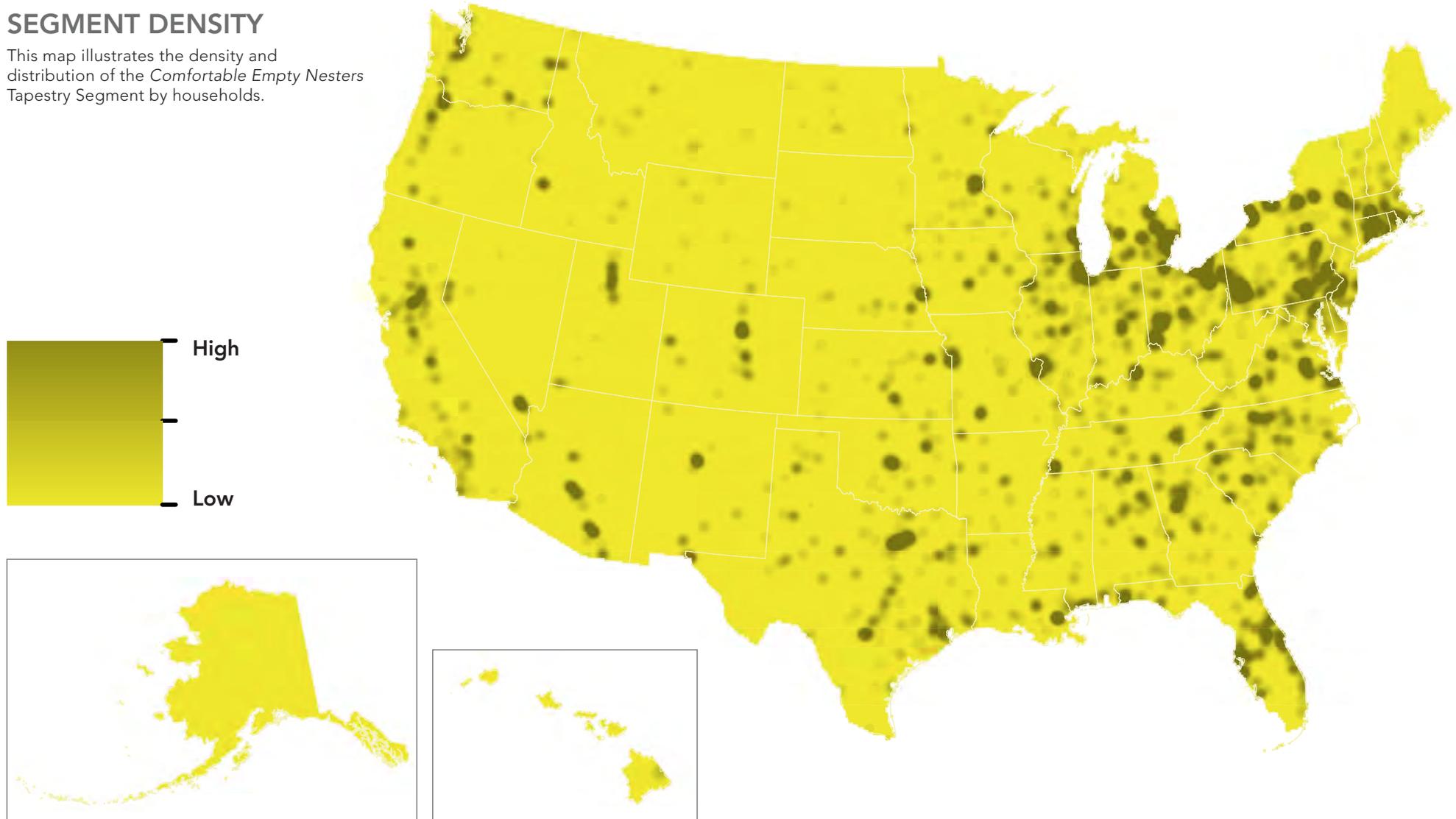


Comfortable Empty Nesters



SEGMENT DENSITY

This map illustrates the density and distribution of the *Comfortable Empty Nesters* Tapestry Segment by households.



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LifeMode Group: Middle Ground

Bright Young Professionals



Households: 2,750,200

Average Household Size: 2.41

Median Age: 33.0

Median Household Income: \$54,000

WHO ARE WE?

Bright Young Professionals is a large market, primarily located in urban outskirts of large metropolitan areas. These communities are home to young, educated, working professionals. More than one out of three householders are under the age of 35. Slightly more diverse couples dominate this market, with more renters than homeowners. More than two-fifths of the households live in single-family homes; over a third live in 5+ unit buildings. Labor force participation is high, generally white-collar work, with a mix of food service and part-time jobs (among the college students). Median household income, median home value, and average rent are close to the US values. Residents of this segment are physically active and up on the latest technology.

OUR NEIGHBORHOOD

- Approximately 57% of the households rent; 43% own their homes.
- Household type is primarily couples, married (or unmarried), with above-average concentrations of both single-parent (Index 125) and single-person (Index 115) households.
- Multiunit buildings or row housing make up 56% of the housing stock (row housing [Index 178], buildings with 5–19 units [Index 275]); 43% built 1980–99.
- Average rent mirrors the US (Index 100).
- Lower vacancy rate is at 8.2%.

SOCIOECONOMIC TRAITS

- Education completed: 35% with some college or an associate's degree, 33% with a bachelor's degree or higher.
- Labor force participation rate of 72% is higher than the US rate.
- These consumers are up on the latest technology.
- They get most of their information from the internet.
- Concern about the environment impacts their purchasing decisions.



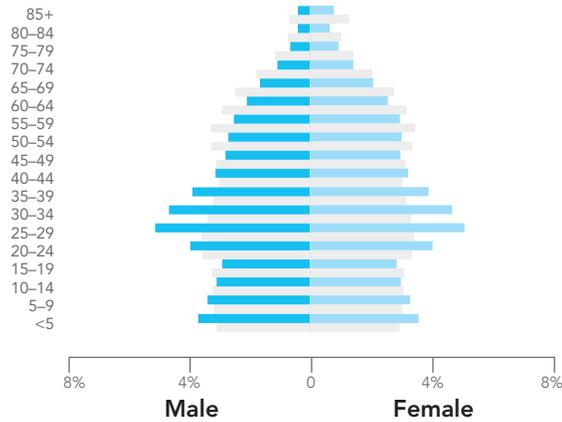
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **33.0** US: 38.2

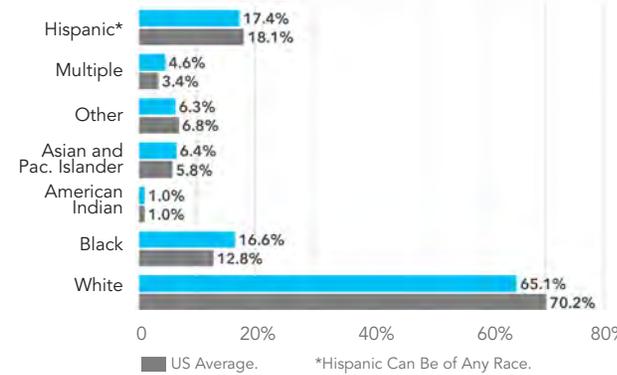
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **67.5** US: 64.0



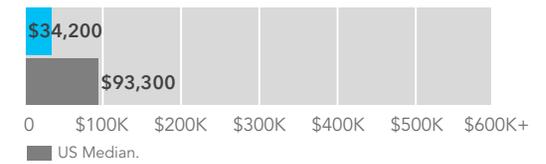
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

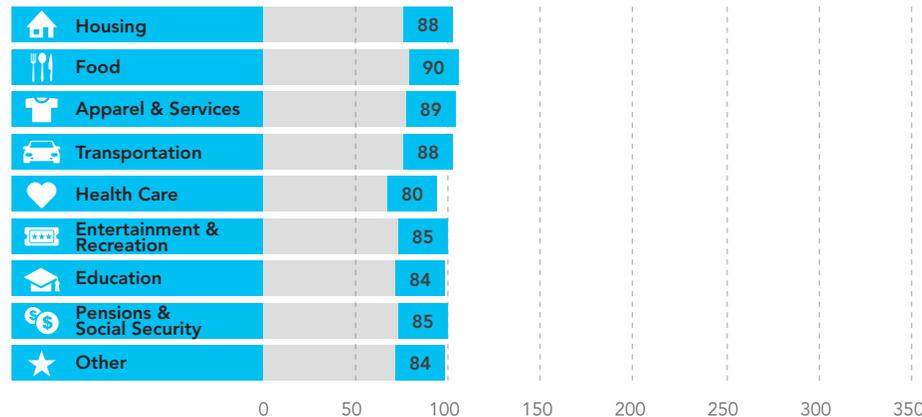


Median Net Worth



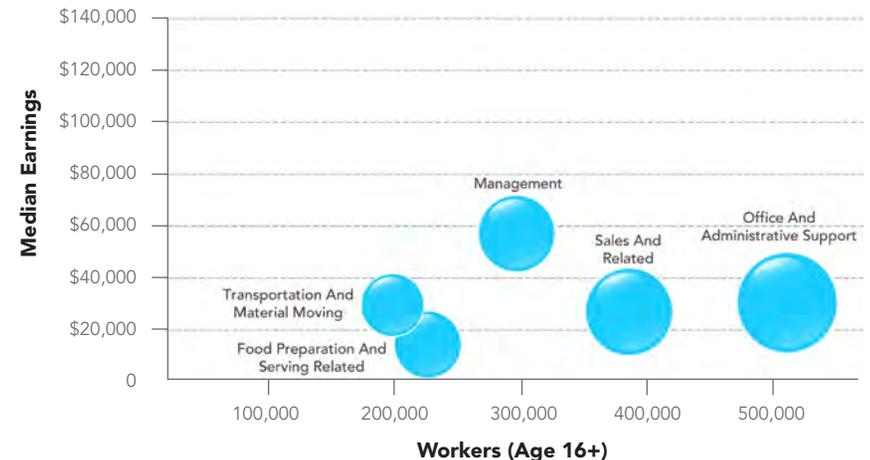
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Own retirement savings and student loans.
- Own newer computers (desktop, laptop, or both), iPods, and 2+ TVs.
- Go online and use mobile devices for banking, access YouTube or Facebook, visit blogs, download movies, and play games.
- Use cell phones to text, redeem mobile coupons, listen to music, and check for news and financial information.
- Find leisure going to bars/clubs, attending concerts, going to the beach.
- Enjoy a variety of sports, including backpacking, rock climbing, football, Pilates, running, and yoga.
- Eat out often at fast-food and family restaurants.

HOUSING

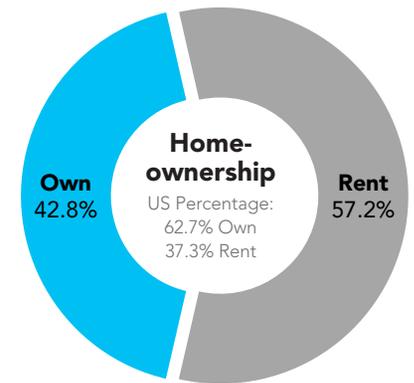
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family;
Multi-Units

Average Rent:
\$1,042

US Average: \$1,038



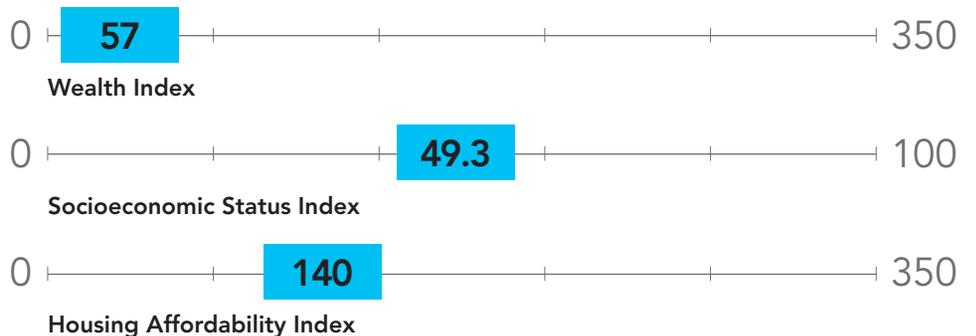
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.



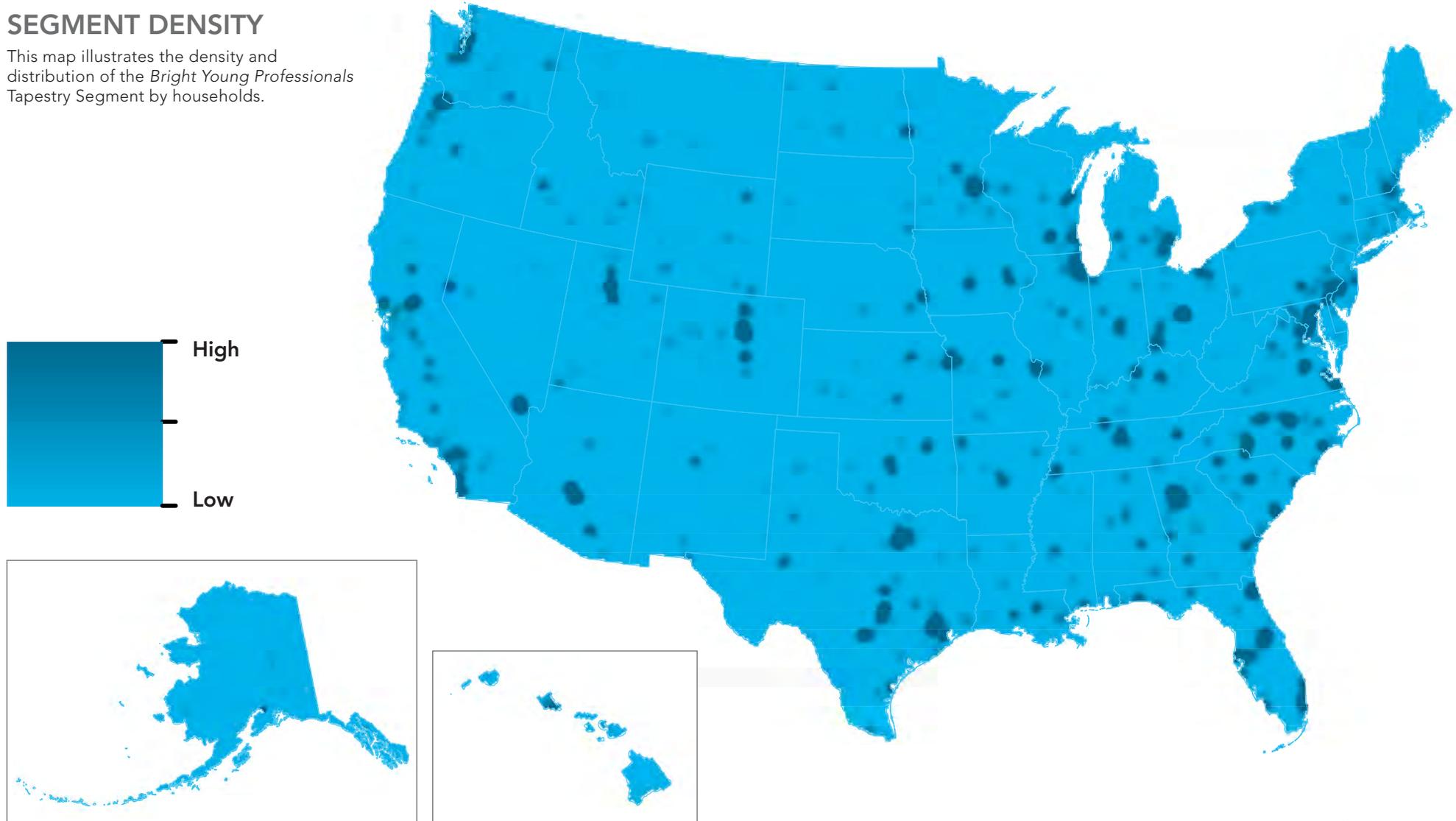


Bright Young Professionals



SEGMENT DENSITY

This map illustrates the density and distribution of the *Bright Young Professionals* Tapestry Segment by households.



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THE
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LifeMode Group: Cozy Country Living

Green Acres

6A

Households: 3,923,400

Average Household Size: 2.70

Median Age: 43.9

Median Household Income: \$76,800

WHO ARE WE?

The *Green Acres* lifestyle features country living and self-reliance. Avid do-it-yourselfers, they maintain and remodel their homes with all the necessary power tools to accomplish the jobs. Gardening, especially growing vegetables, is a priority, again with the right tools, tillers, tractors, and riding mowers. Outdoor living features a variety of sports: hunting and fishing, motorcycling, hiking and camping, and even golf.

OUR NEIGHBORHOOD

- This large segment is concentrated in rural enclaves in metropolitan areas.
- Primarily (not exclusively) older homes with acreage; new housing growth in the past 15 years.
- Single-family, owner-occupied housing, with a median value of \$235,500.
- An older market, primarily married couples, most with no children.

SOCIOECONOMIC TRAITS

- Education: More than 60% are college educated.
- Labor force participation rate is high at 66.8% (Index 107).
- Income is derived not only from wages and salaries but also from self-employment (more than 13% of households), investments (27% of households), and increasingly, from retirement.
- They are cautious consumers with a focus on quality and durability.
- Comfortable with technology, more as a tool than a trend: banking or paying bills online is convenient; but the internet is not viewed as entertainment.
- Economic outlook is professed as pessimistic, but consumers are comfortable with debt, primarily as home and auto loans, and investments.



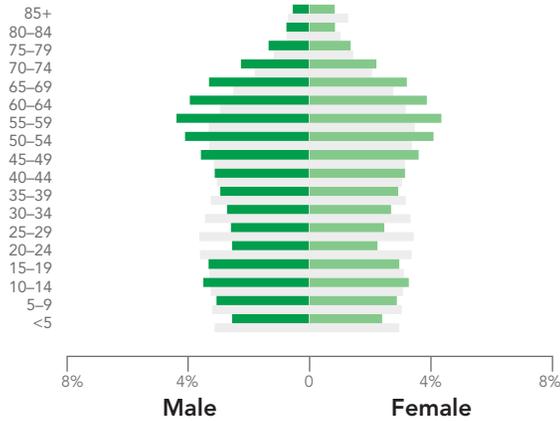
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **43.9** US: 38.2

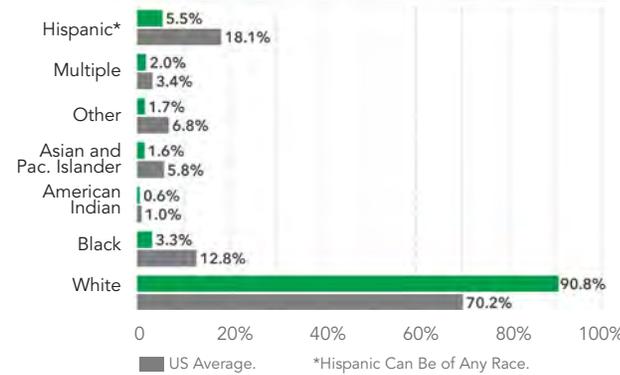
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **26.0** US: 64.0



INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

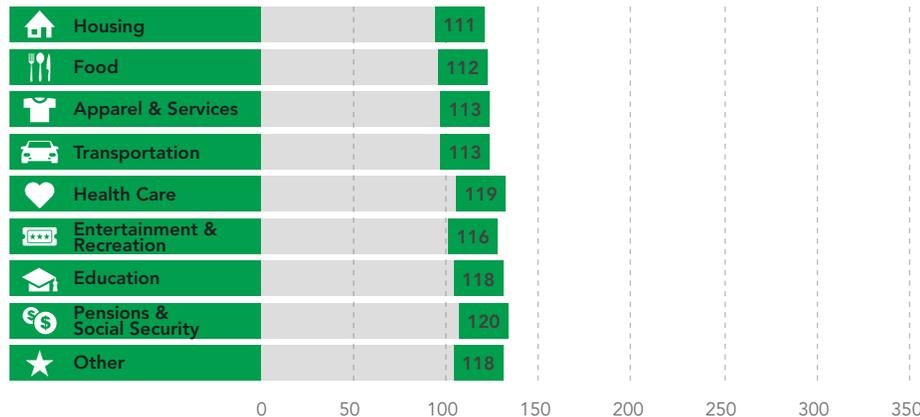


Median Net Worth



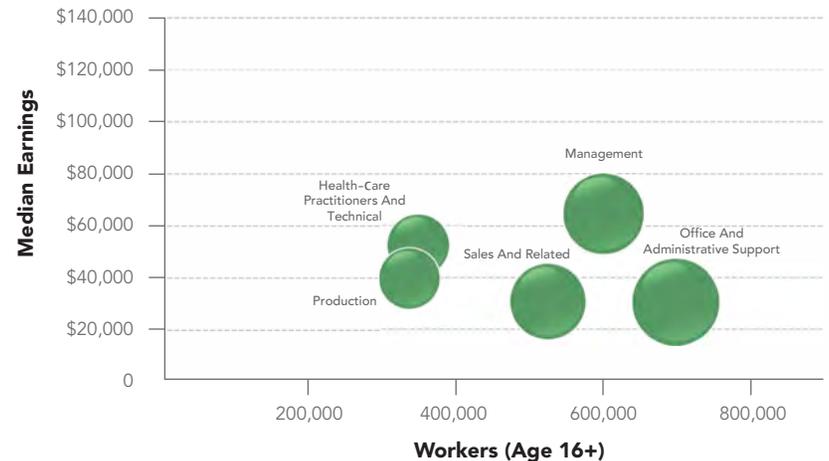
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Purchasing choices reflect *Green Acres* residents' country life, including a variety of vehicles, from trucks and SUVs to ATVs and motorcycles, preferably late model.
- Homeowners favor DIY home improvement projects and gardening.
- Media of choice are provided by satellite service, radio, and television, also with an emphasis on country and home and garden.
- *Green Acres* residents pursue physical fitness vigorously, from working out on home exercise equipment to playing a variety of sports.
- Residents are active in their communities and a variety of social organizations, from charitable to veterans' clubs.

HOUSING

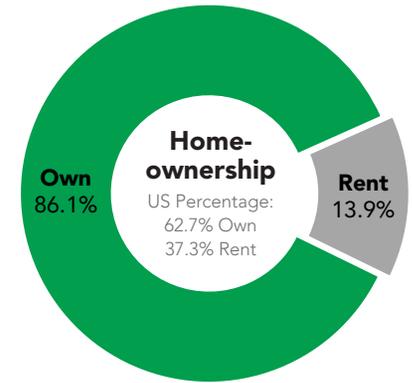
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family

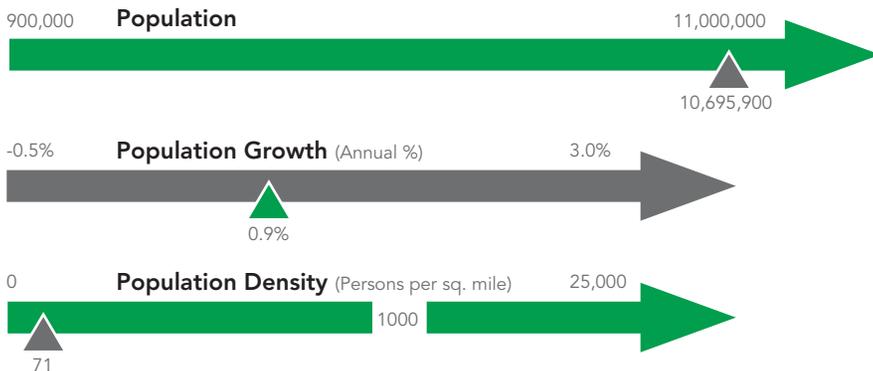
Median Value:
\$235,500

US Median: \$207,300



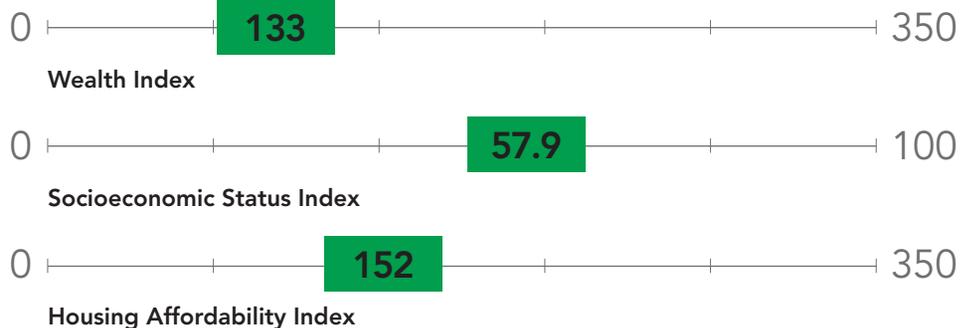
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

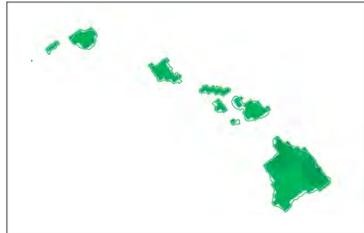
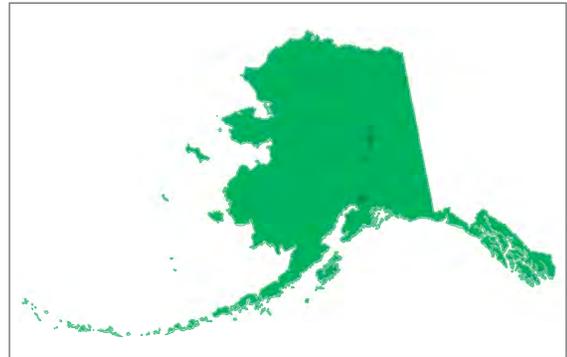
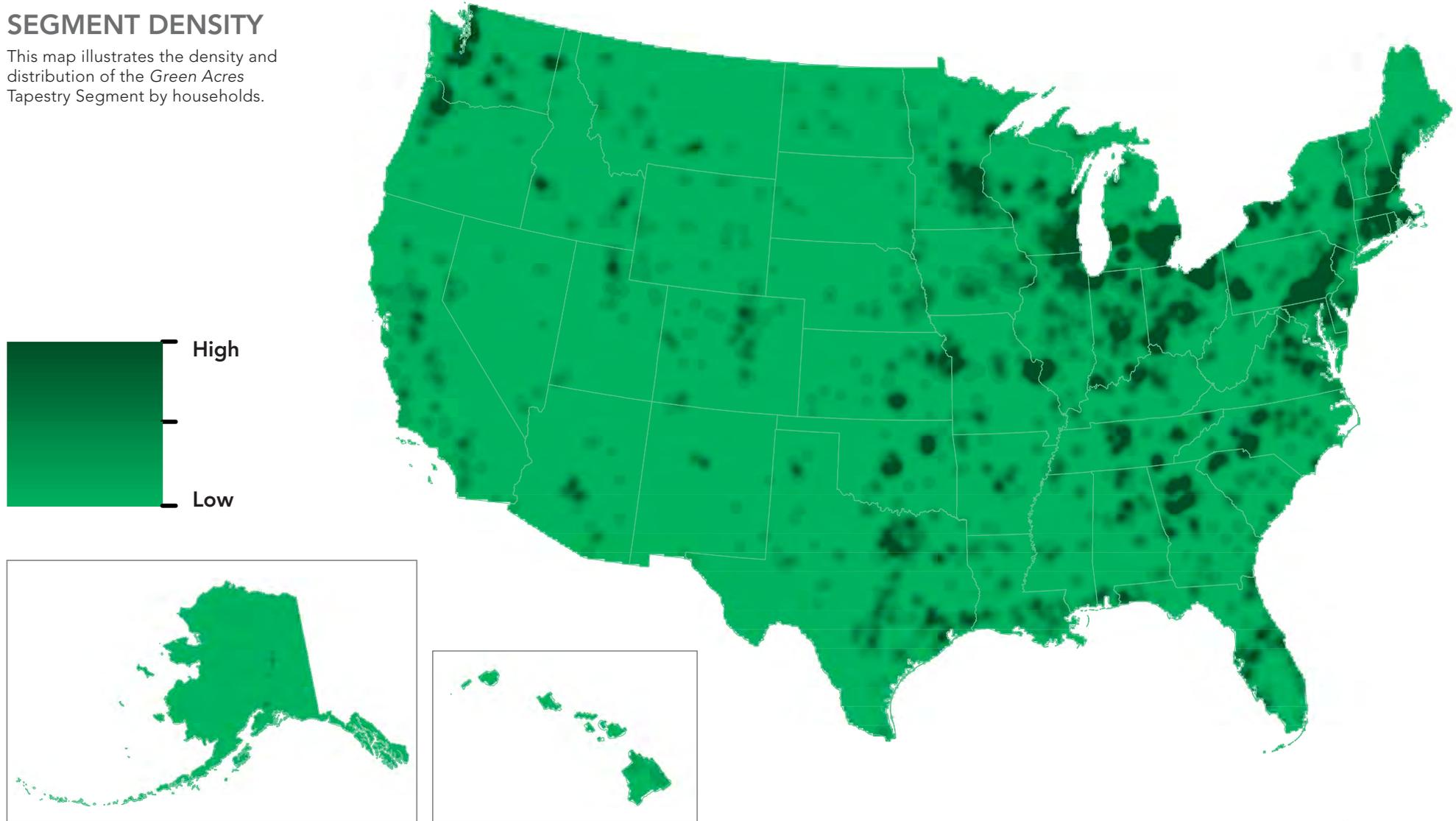
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Green Acres* Tapestry Segment by households.



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LifeMode Group: Rustic Outposts

Down the Road

10D

Households: 1,406,700

Average Household Size: 2.76

Median Age: 35.0

Median Household Income: \$38,700

WHO ARE WE?

Down the Road is a mix of low-density, semirural neighborhoods in large metropolitan areas; half are located in the South, with the rest primarily in the West and Midwest. Almost half of householders live in mobile homes; more than two-fifths live in single-family homes. These are young, family-oriented consumers who value their traditions. Workers are in service, retail trade, manufacturing, and construction industries, with higher proportions in agriculture and mining, compared to the US.

OUR NEIGHBORHOOD

- Nearly two-thirds of households are owned.
- Family market, primarily married couples or single-parent households (Index 145).
- Close to half of all households live in mobile homes (Index 780).
- Four-fifths of households were built in 1970 or later.
- About 32% of homes are valued under \$50,000.

SOCIOECONOMIC TRAITS

- Education completed: 36% with a high school diploma only, 41% with some college education or a degree.
- Labor force participation rate is 59.0%, slightly lower than the US.
- Family-oriented, outgoing consumers; they place importance on preserving time-honored customs.

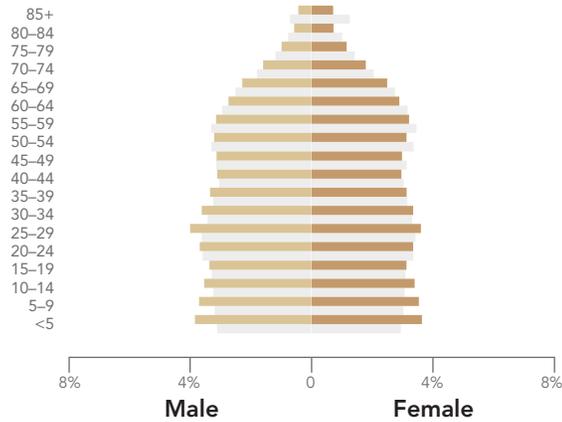


Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.

AGE BY SEX (Esri data)

Median Age: **35.0** US: 38.2

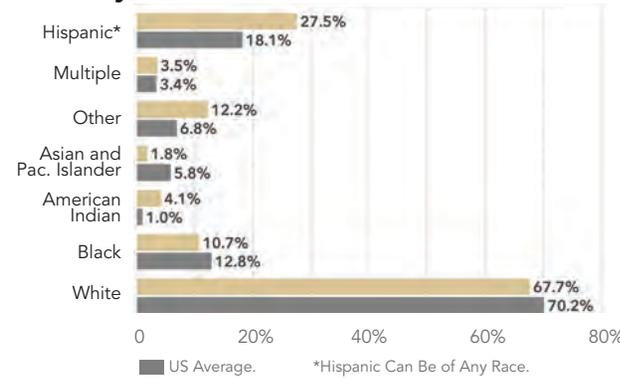
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **71.7** US: 64.0



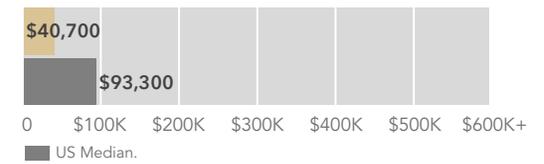
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

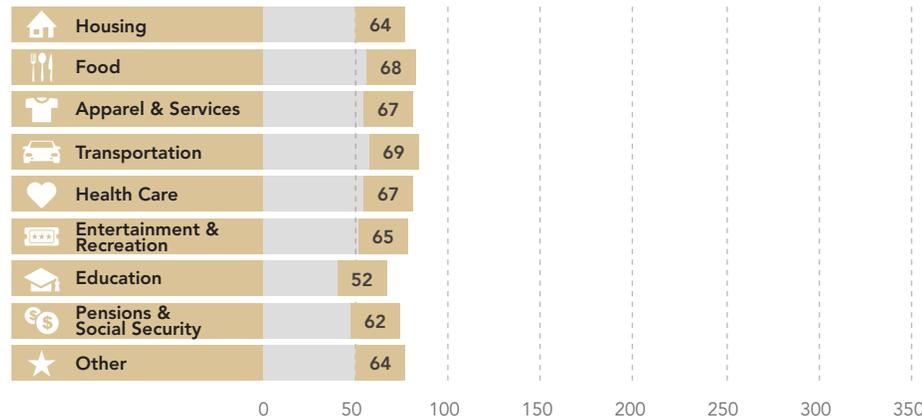


Median Net Worth



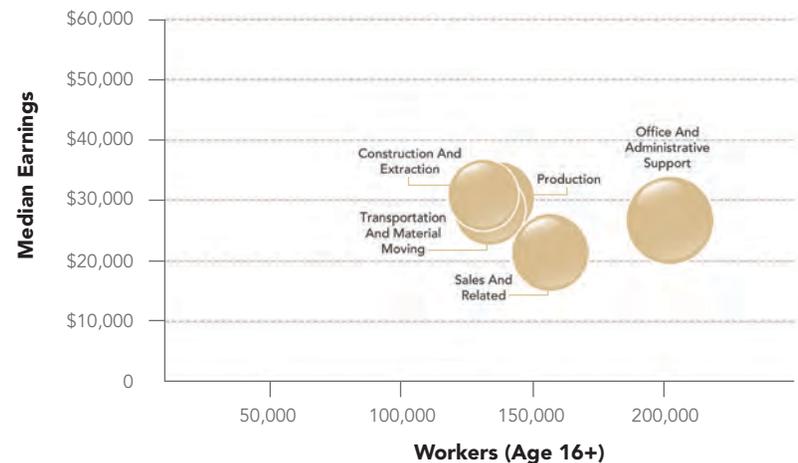
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- Purchased a used vehicle in the past year, likely maintaining the vehicle themselves.
- Routinely stop by the convenience store to purchase gas, groceries, and snacks.
- Participate in fishing and hunting.
- Use the internet to stay connected with friends and play online video games.
- Listen to the radio, especially at work, with a preference for rap, R&B, and country music.
- Enjoy programs on Investigation Discovery, CMT, and Hallmark, typically watching via satellite dish.
- Often prepare quick meals, using packaged or frozen dinner entrees.
- Favorite fast food: burgers and pizza.
- Frequent Walmart Supercenters, Walgreens, dollar stores, Kmart, and Big Lots for all their shopping needs (groceries, clothing, pharmacy, etc.).

HOUSING

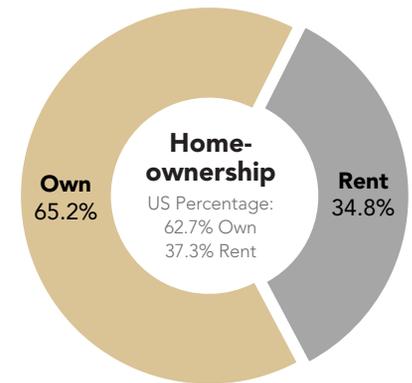
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
 Mobile Homes;
 Single Family

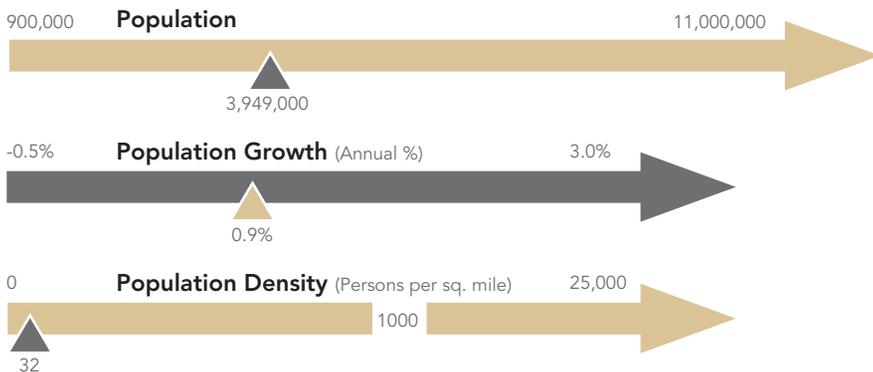
Median Value:
 \$89,800

US Median: \$207,300



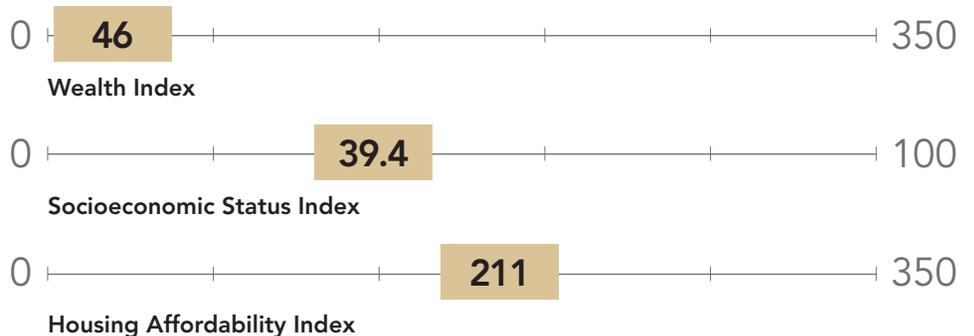
POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

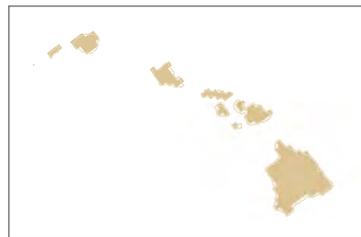
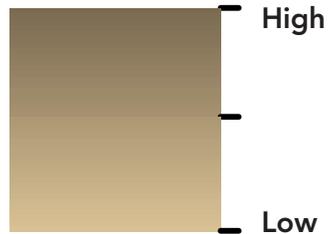
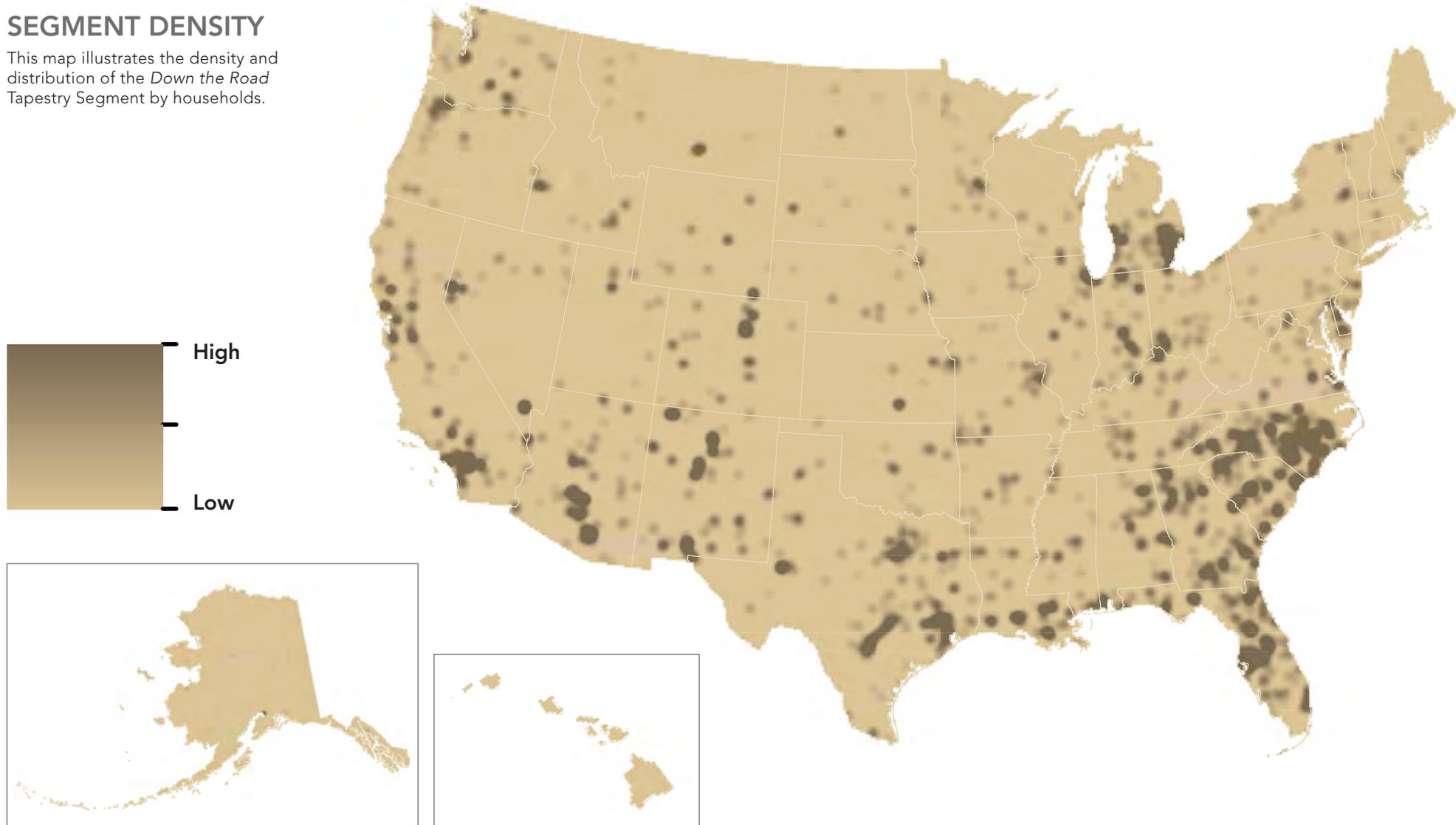
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Down the Road* Tapestry Segment by households.





LifeMode Group: Affluent Estates

Professional Pride

1B

Households: 1,982,300

Average Household Size: 3.13

Median Age: 40.8

Median Household Income: \$138,100

WHO ARE WE?

Professional Pride consumers are well-educated career professionals that have prospered through the Great Recession. To maintain their upscale suburban lifestyles, these goal-oriented couples work, often commuting far and working long hours. However, their schedules are fine-tuned to meet the needs of their school-age children. They are financially savvy; they invest wisely and benefit from interest and dividend income. So far, these established families have accumulated an average of US\$1.6 million dollars in net worth, and their annual household income runs at more than twice the US level. They take pride in their newer homes and spend valuable time and energy upgrading. Their homes are furnished with the latest in home trends, including finished basements equipped with home gyms and in-home theaters.

OUR NEIGHBORHOOD

- Typically owner-occupied (Index 146), single-family homes are in newer neighborhoods: 67% of units were built in the last 20 years.
- Neighborhoods are primarily located in the suburban periphery of large metropolitan areas.
- Most households own three or more vehicles; long commutes are the norm.
- Homes are valued at more than twice the US median home value, although three out of four homeowners have mortgages to pay off.
- Families are mostly married couples (almost 80% of households), and nearly half of these families have kids. Their average household size, 3.13, reflects the presence of children.

SOCIOECONOMIC TRAITS

- *Professional Pride* consumers are highly qualified in the science, technology, law, or finance fields; they've worked hard to build their professional reputation or their start-up businesses.
- These consumers are willing to risk their accumulated wealth in the stock market.
- Most have a preferred financial institution, regularly read financial news, and use the internet for banking transactions.
- Residents are goal oriented and strive for lifelong earning and learning.
- Life here is well organized; routine is a key ingredient to daily life.



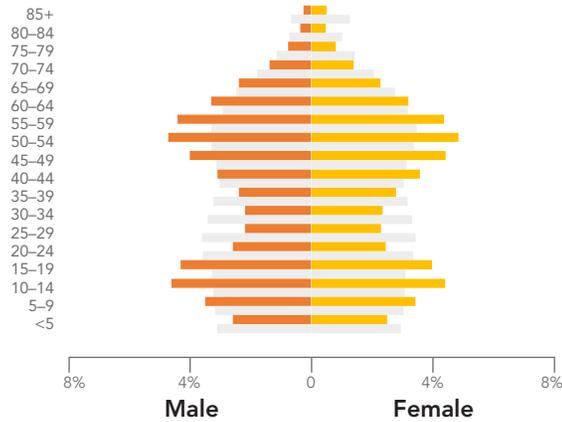
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **40.8** US: 38.2

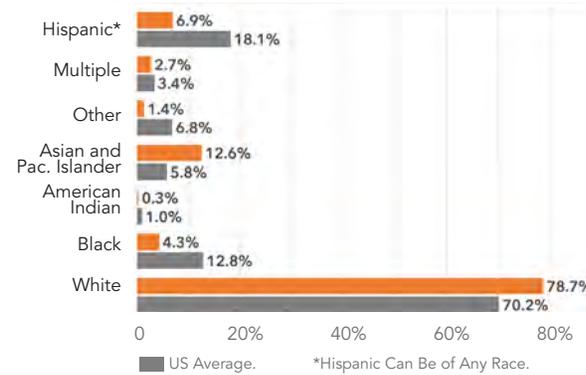
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **44.5** US: 64.0



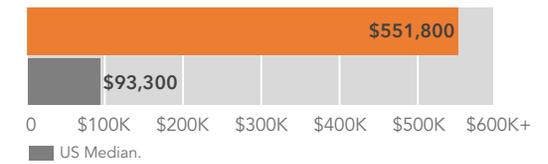
INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

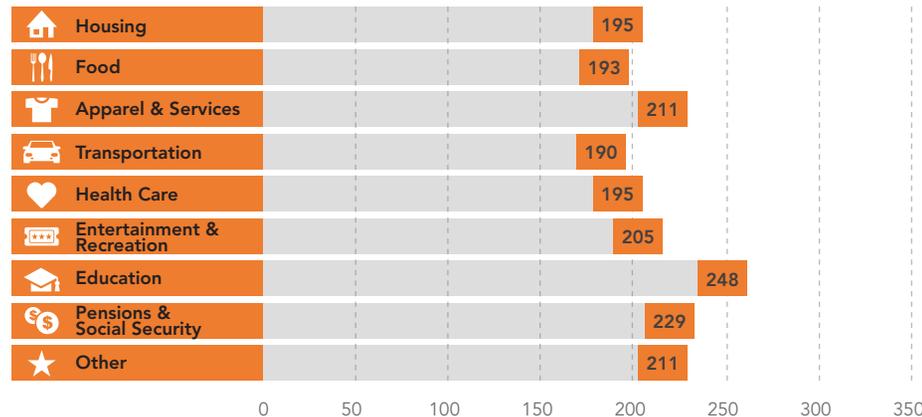


Median Net Worth



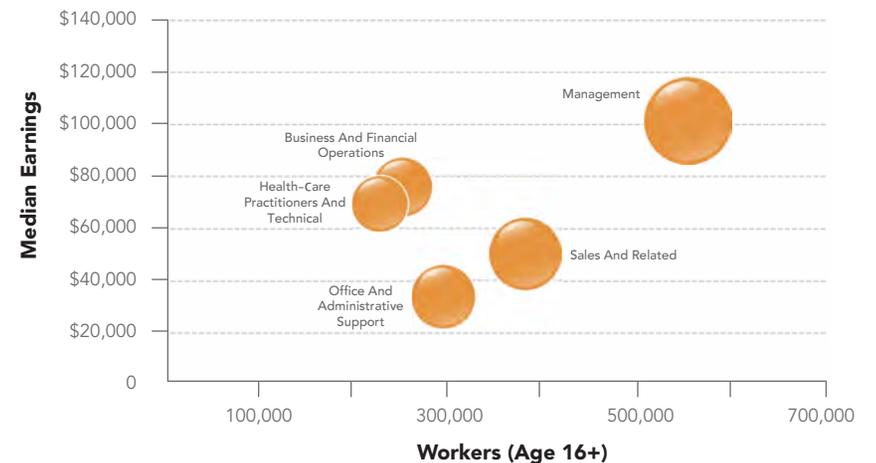
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.

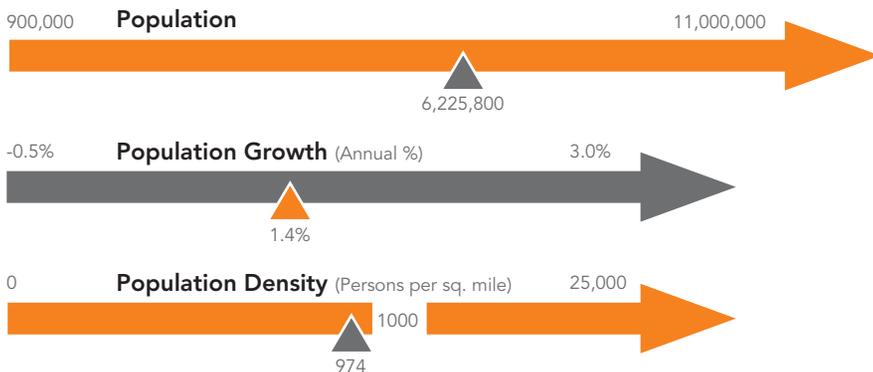


MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- These frequent travelers take several domestic trips a year, preferring to book their plane tickets, accommodations, and rental cars via the internet.
- Residents take pride in their picture-perfect homes, which they continually upgrade. They shop at Home Depot and Bed Bath & Beyond to tackle the smaller home improvement and remodeling tasks but contract out the larger projects.
- Hire housekeepers or professional cleaners.
- Residents are prepared for the ups and downs in life; they maintain life insurance; homeowners and auto insurance; as well as medical, vision, dental, and prescription insurance through work. They are actively investing for the future; they hold 401(k) and IRA retirement plans, plus securities.
- Consumers spend on credit but have the disposable income to avoid a balance on their credit cards. They spend heavily on internet shopping; Amazon.com is a favorite website.
- Consumers find time in their busy schedules for themselves. They work out in their home gyms, owning at least a treadmill, an elliptical, or weightlifting equipment. They also visit the salon and spa regularly.
- All family members are avid readers; they read on their smartphones, tablets, and e-readers but also read hard copies of epicurean, home service, and sports magazines.
- Residents, both young and old, are tech savvy; they not only own the latest and greatest in tablets, smartphones, and laptops but actually use the features each has to offer.

POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



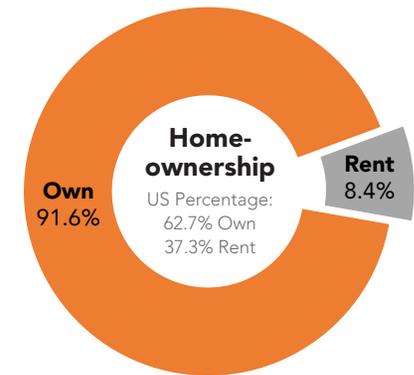
HOUSING

Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



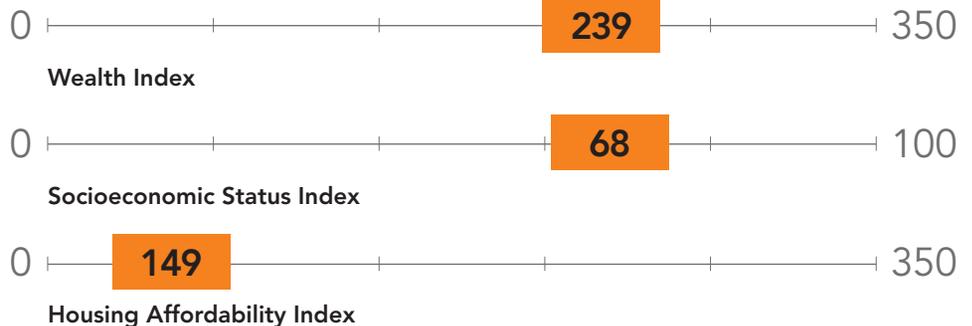
Typical Housing:
Single Family

Median Value:
\$433,400
US Median: \$207,300



ESRI INDEXES

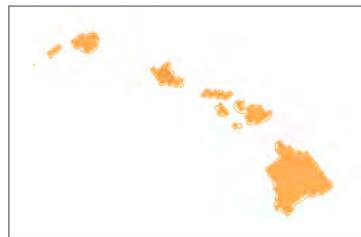
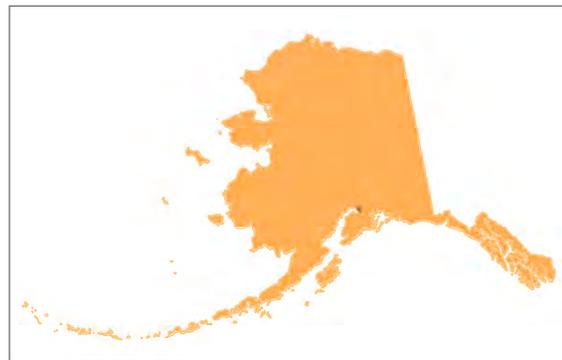
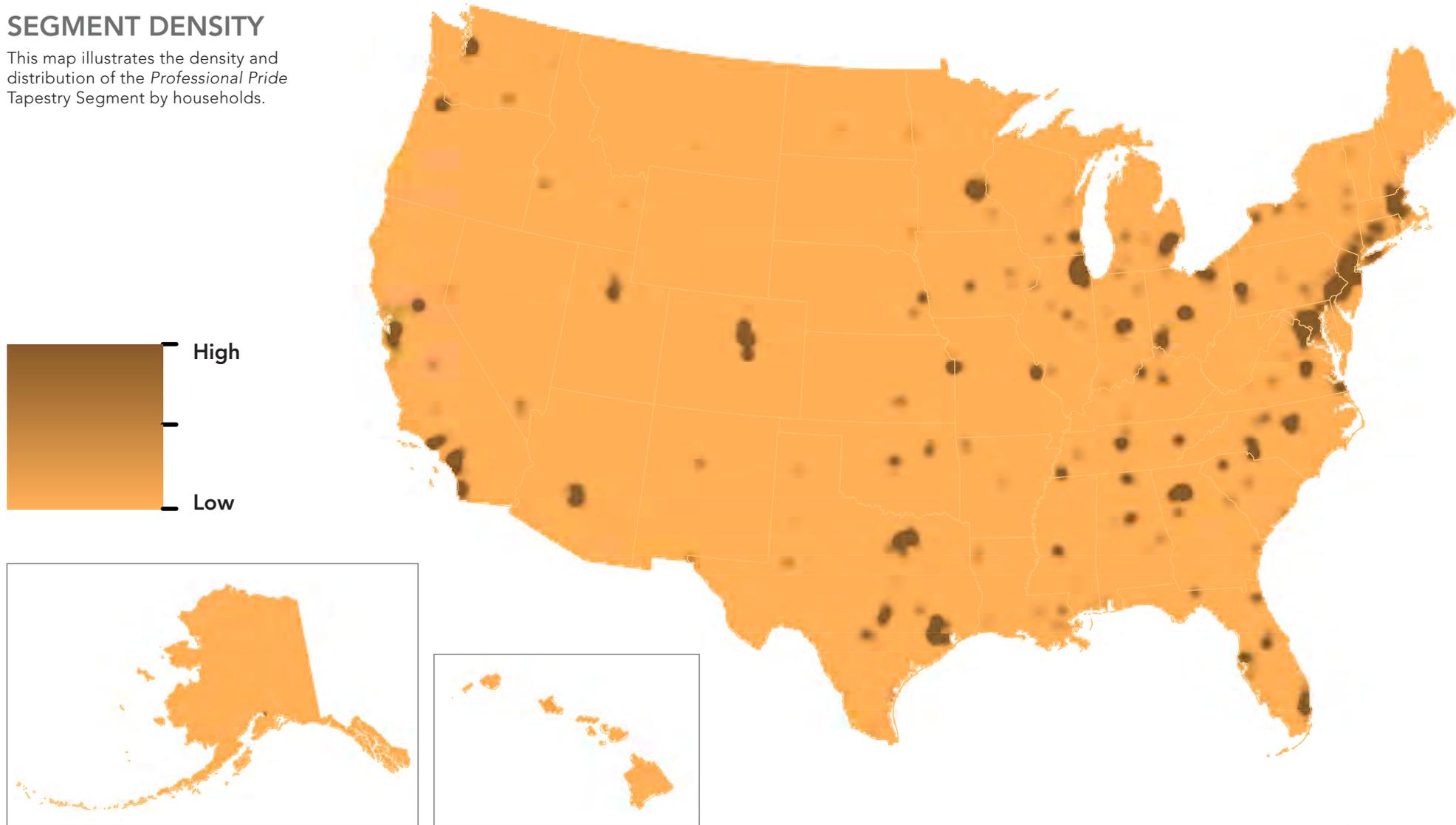
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Professional Pride* Tapestry Segment by households.



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LifeMode Group: Affluent Estates

Exurbanites



Households: 2,398,200

Average Household Size: 2.50

Median Age: 51.0

Median Household Income: \$103,400

WHO ARE WE?

Exurbanites residents are now approaching retirement but showing few signs of slowing down. They are active in their communities, generous in their donations, and seasoned travelers. They take advantage of their proximity to large metropolitan centers to support the arts but prefer a more expansive home style in less crowded neighborhoods. They have cultivated a lifestyle that is both affluent and urbane.

OUR NEIGHBORHOOD

- Established neighborhoods (most built between 1970 and 1990) found in the suburban periphery of large metropolitan markets.
- A larger market of empty nesters, married couples with no children; average household size is 2.50.
- Primarily single-family homes with a high median value of \$423,400 (Index 204), most still carrying mortgages.
- Higher vacancy rate at 9%.

SOCIOECONOMIC TRAITS

- Residents are college educated; more than half have a bachelor's degree or higher; nearly 81% have some college education.
- This labor force is beginning to retire. One in three households currently receive Social Security or retirement income. Labor force participation has declined to less than 60% (Index 95).
- More of the residents prefer self-employment (Index 178) or working from home (Index 177).
- Consumers are more interested in quality than cost. They take pride in their homes and foster a sense of personal style.
- *Exurbanites* residents are well connected, using the internet for everything from shopping to managing their finances.
- Sociable and hardworking, they still find time to stay physically fit.



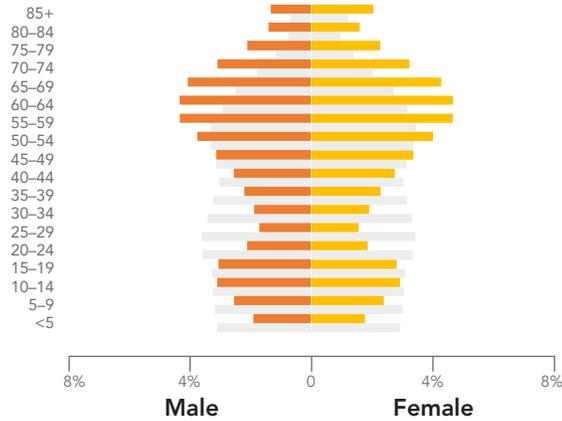
Note: The Index represents the ratio of the segment rate to the US rate multiplied by 100. Consumer preferences are estimated from data by MRI-Simmons.



AGE BY SEX (Esri data)

Median Age: **51.0** US: 38.2

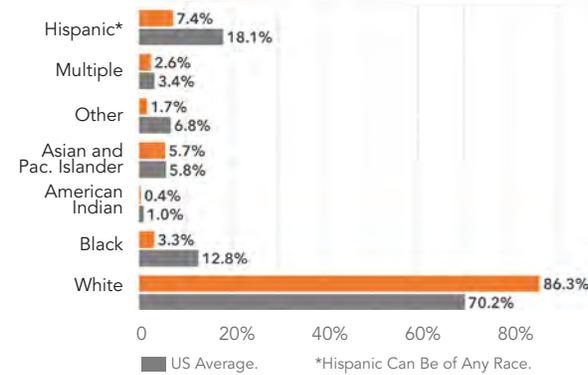
■ Indicates US



RACE AND ETHNICITY (Esri data)

The Diversity Index summarizes racial and ethnic diversity. The index shows the likelihood that two persons, chosen at random from the same area, belong to different race or ethnic groups. The index ranges from 0 (no diversity) to 100 (complete diversity).

Diversity Index: **35.2** US: 64.0



INCOME AND NET WORTH

Net worth measures total household assets (homes, vehicles, investments, etc.) less any debts, secured (e.g., mortgages) or unsecured (credit cards). Household income and net worth are estimated by Esri.

Median Household Income

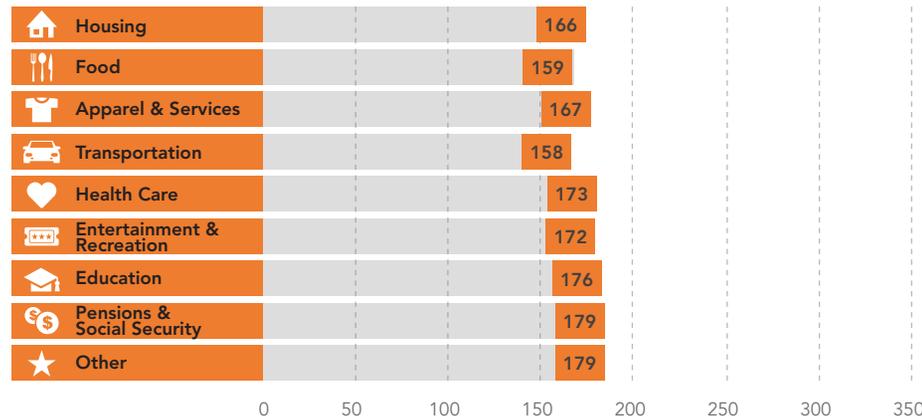


Median Net Worth



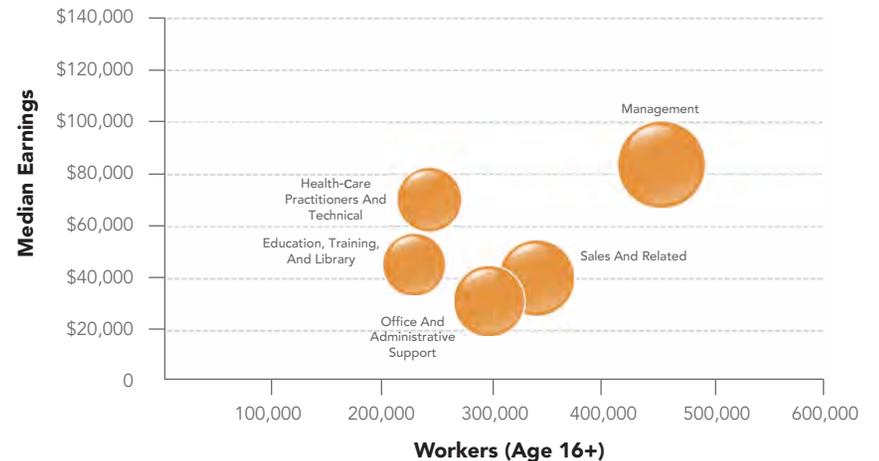
AVERAGE HOUSEHOLD BUDGET INDEX

The index compares the average amount spent in this market's household budgets for housing, food, apparel, etc., to the average amount spent by all US households. An index of 100 is average. An index of 120 shows that average spending by consumers in this market is 20 percent above the national average. Consumer expenditures are estimated by Esri.



OCCUPATION BY EARNINGS

The five occupations with the highest number of workers in the market are displayed by median earnings. Data from the Census Bureau's American Community Survey.





MARKET PROFILE (Consumer preferences are estimated from data by MRI-Simmons.)

- *Exurbanites* residents' preferred vehicles are late model luxury cars or SUVs.
- Active supporters of the arts and public television/radio.
- Attentive to ingredients, they prefer natural or organic products.
- Gardening and home improvement are priorities, but they also use a number of services, from home care and maintenance to personal care.
- Financially active with wide-ranging investments, these investors rely on financial planners, extensive reading, and the internet to handle their money.

HOUSING

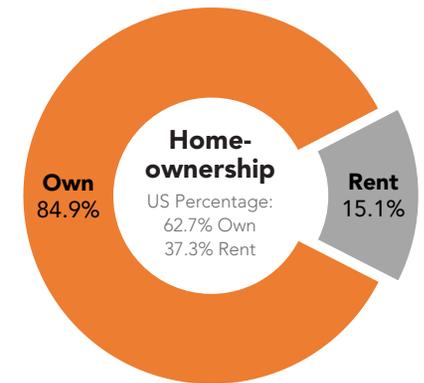
Median home value is displayed for markets that are primarily owner occupied; average rent is shown for renter-occupied markets. Tenure and home value are estimated by Esri. Housing type and average rent are from the Census Bureau's American Community Survey.



Typical Housing:
Single Family

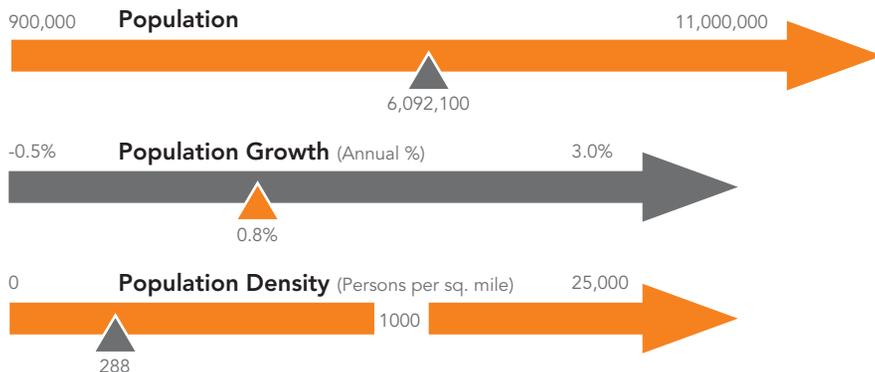
Median Value:
\$423,400

US Median: \$207,300



POPULATION CHARACTERISTICS

Total population, average annual population change since Census 2010, and average density (population per square mile) are displayed for the market relative to the size and change among all Tapestry markets. Data estimated by Esri.



ESRI INDEXES

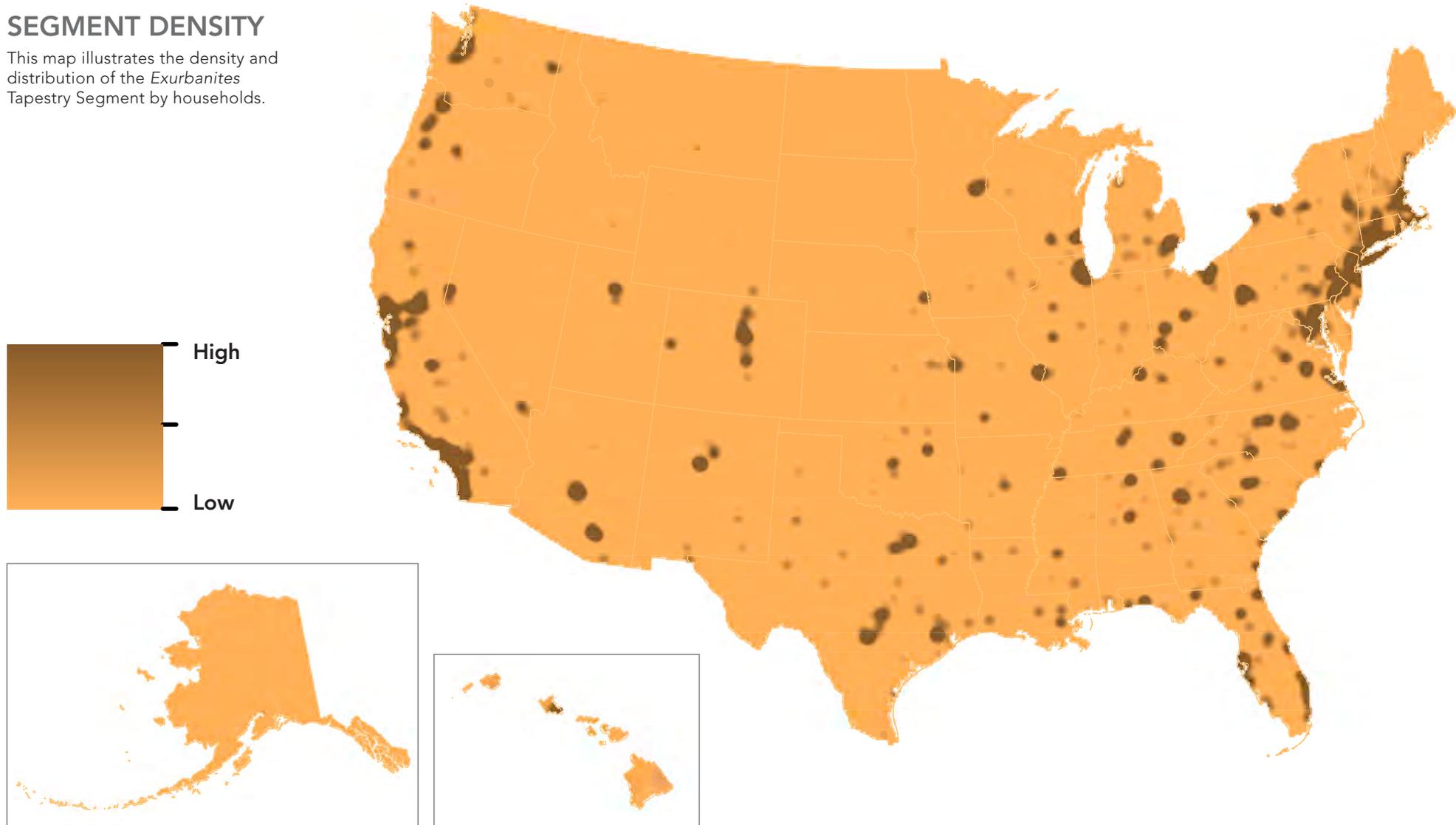
Esri developed three indexes to display average household wealth, socioeconomic status, and housing affordability for the market relative to US standards.





SEGMENT DENSITY

This map illustrates the density and distribution of the *Exurbanites* Tapestry Segment by households.



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City of Rockwall
The New Horizon

MEMORANDUM

TO: Mayor and Council
FROM: Kenneth Cullins, Fire Chief
DATE: April 15, 2025
SUBJECT: “Recognized Best Practices Fire Department”

The Rockwall Fire Department is proud to announce that it has been designated as a "Recognized Best Practices Fire Department" by the Texas Fire Chiefs Association Best Practices Recognition Program. This prestigious honor places Rockwall Fire among only 36 departments statewide to achieve this status since the program's inception in 2013.

The Best Practices Recognition Program rigorously evaluates fire departments against 117 best practice standards developed by fire chiefs across Texas. These standards cover every facet of fire department management—from administration and organization, emergency medical services, training, and special operations to records management, fire operations, prevention, risk reduction, community outreach, response analysis, communications, safety, resources management, and professional conduct.

The journey to recognition began several years ago, when the department voluntarily undertook an in-depth self-review of its policies, procedures, facilities, and operations. While many of the 117 best practices were already in place, the process prompted the team to capture and formalize areas that needed enhancement through documented policies and plans. An external audit was then conducted by trained Fire Chiefs and Assistant Fire Chiefs from across the state. Their comprehensive evaluation was subsequently reviewed by the Texas Fire Chiefs Association Best Practices Recognition Board, culminating in the award of Recognized status.

Chief Joel Baker of the Austin Fire Dept., who is current President of the TX Fire Chiefs Association, as well as Chief James Mallinger, immediate Past President and Fire Chief of Cedar Park, TX, will be present at the Council meeting Monday evening to present the award to our department.

ROCKWALL CITY COUNCIL MEETING

Monday, April 7, 2025 - 6:00 PM

City Hall Council Chambers - 385 S. Goliad St., Rockwall, TX 75087

I. Call Public Meeting to Order

Mayor Pro Tem Jorif called the meeting to order at 6:00 p.m. Present were Mayor Pro Tem Clarence Jorif and Councilmembers Sedric Thomas, Mark Moeller, Anna Campbell, Dennis Lewis and Tim McCallum. Also present were City Manager Mary Smith and Assistant City Manager Joey Boyd. Mayor Trace Johannesen was absent from the meeting.

II. Invocation and Pledge of Allegiance - Councilmember Lewis

Councilmember Lewis delivered the invocation and led the Pledge of Allegiance.

III. Proclamations / Awards / Recognitions

1. Child Abuse Awareness & Prevention Month Proclamation

Mayor Pro Tem Jorif called forth members of several local organizations, including the Children’s Advocacy Center for Rockwall County, EMPOWER, Lone Star CASA (Court Appointed Special Advocates), Rockwall County Department of Family and Protective Services (DFPS), and The Rainbow Room. He then read and presented this proclamation.

2. Public Safety Telecommunicators Week Proclamation

Mayor Pro Tem Jorif called forth staff members of the Rockwall Police Department, including several dispatch personnel. He then read and presented this proclamation. He and Chief Fowler thanked the dispatchers for all they do, day in and day out.

3. JustServe Recognition Day Proclamation

Mayor Pro Tem Jorif read and presented this proclamation. Two representatives from this organization expressed gratitude for this proclamation, explaining that their website helps link volunteers with volunteer organizations / opportunities.

- 4. Fire Department Lifesaving Award - Engine 2, "A Shift"**
 - Driver Andrew Burton
 - Firefighter Cody Baumann
 - Firefighter Joseph Wharmby

IV. Open Forum

Mayor Pro Tem Jorif explained how Open Forum is conducted, asking if anyone would like to come forth and speak at this time. No one indicated a desire to speak, so he closed Open Forum.

V. Consent Agenda

1. Consider approval of the minutes from the March 17, 2025 city council meeting, and take any action necessary.
2. **Z2025-004** - Consider a request by Corey Smith on behalf of Destiny Smith for the approval of an **ordinance** for a Specific Use Permit (SUP) allowing a Guest Quarters/Secondary Living Unit and an Agricultural Accessory Building/Barn on a 10.00-acre tract of land identified as Tract 1-5 of the J. W. Pitman Survey, Abstract No. 181, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, addressed as 2201 E. FM-550, and take any action necessary (**2nd Reading**).
3. **Z2025-005** - Consider a request by Adam Buczek of the Skorburg Company on behalf of Dr. Karl Erwin of the Estate of Karl W. Erwin for the approval of an **ordinance** for a Zoning Change from an Agricultural (AG) District to a Planned Development District for Single-Family 10 (SF-10) District land uses on a 98.40-acre tract of land identified as a portion of Tract 4 of the J. M. Gass Survey, Abstract No. 88, City of Rockwall, Rockwall County, Texas zoned Agricultural (AG) District, generally located on the southside of North Country Lane east of the intersection of North Country Lane and FM-1141, and take any action necessary (**2nd Reading**).
4. **Z2025-006** - Consider a request by Lisa Brooks and Rene'e Holland of Makeway, LLC for the approval of an **ordinance** for a Zoning Change from an Agricultural (AG) District to a General Retail (GR) District for a 2.751-acre tract of land identified as Tract 1-04 of the S. R. Barnes Survey, Abstract No. 13, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, situated within the SH-205 By-Pass Overlay (SH-205 BY-OV) District, located at the southeast corner of the intersection of John King Boulevard and Quail Run Road, and take any action necessary (**2nd Reading**).
5. **Z2025-007** - Consider a request by Tzemach Moshe Kalmenson for the approval of an **ordinance** for a Specific Use Permit (SUP) allowing a *Church/House of Worship* on a 0.5010-acre tract of land identified as Block 29 of the Gardner Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 50 (PD-50) [*Ordinance No. 25-07*], situated within the North Goliad Corridor Overlay (NGC OV) District, addressed as 917 N. Goliad Street, and take any action necessary (**2nd Reading**).
6. **Z2025-010** - Consider a request by Price Pointer of TCB Construction on behalf of Jay Odom for the approval of an **ordinance** for a Zoning Change amending Planned Development District 50 (PD-50) [*Ordinance No. 25-07*] to incorporate an additional 1.65-acre parcel of land within the district being a 22.19-acre tract of land situated within the S. S. McCurry Survey, Abstract No. 146 and B. F. Boydston Survey, Abstract No. 14, City of Rockwall, Rockwall County, Texas, zoned: Planned Development District 50 (PD-50) for Residential-Office (RO) District land uses, Single-Family 7 (SF-7) District, and Downtown (DT) District, situated within the North Goliad Corridor Overlay (NGC OV) District and the Old Town Rockwall (OTR) Historic District, generally located along N. Goliad Street [*SH-205*], north of Olive Street and south of Live Oak Street, and take any action necessary (**2nd Reading**).
7. **P2025-008** - Consider a request by Phil Wagner of the Rockwall Economic Development Corporation (REDC) for the approval of a Preliminary Plat for Lot 1, Block A, Ridge Road Addition being a 12.1462-acre tract of land identified as Tract 2 of the D. Atkins Survey, Abstract No. 1, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 4 (PD-4) [*Ordinance No.*

24-02] for General Retail (GR) District land uses, situated within the Scenic Overlay (SOV) District, generally located east of the intersection of Ridge Road [FM-740] and Becky Lane, and take any action necessary.

8. **P2025-010** - Consider a request by Phil Wagner of the Rockwall Economic Development Corporation (REDC) for the approval of a *Replat* for Lots 9, 10, & 11, Block A, Rockwall Technology Park Addition being a 23.1612-acre parcel of land identified as Lots 5, 7, & 8, Block A, Rockwall Technology Park Addition, City of Rockwall, Rockwall County, Texas, zoned Light Industrial (LI) District, situated within the FM-549 Overlay (FM-549 OV) District and the SH-276 Overlay (SH-276 OV) District, generally located at the northwest corner of the intersection of SH-276 and Corporate Crossing [FM-549], and take any action necessary.
9. Consider approving contract addendums for concrete and asphalt pavement repairs and maintenance and authorizing the City Manager to execute associated purchase orders to multiple vendors for a total of \$760,000 to be funded by the Streets & Drainage Operating Budget, and take any action necessary.

Councilmember Lewis moved to approve the Consent Agenda in its entirety (#s 1, 2, 3, 4, 5, 6, 7, 8, and 9). Councilmember Moeller seconded the ordinance. The ordinance captions were read into the record as follows:

**CITY OF ROCKWALL
ORDINANCE NO. 25-14
SPECIFIC USE PERMIT NO. S-355**

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE (UDC) [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) FOR GUEST QUARTERS/SECONDARY LIVING UNIT AND AN AGRICULTURAL ACCESSORY BUILDING ON A 10.00-ACRE TRACT OF LAND, IDENTIFIED AS TRACT 1-5 OF THE J. W. PITMAN SURVEY, ABSTRACT NO. 181, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; AND MORE SPECIFICALLY DESCRIBED AND DEPICTED IN EXHIBIT 'A' OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

CITY OF ROCKWALL

ORDINANCE NO. 25-15

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, AS HERETOFORE AMENDED, SO AS TO CHANGE THE ZONING FROM AN AGRICULTURAL (AG) DISTRICT TO PLANNED DEVELOPMENT DISTRICT 104 (PD-104) FOR SINGLE-FAMILY 10 (SF-10) DISTRICT LAND USES ON THE SUBJECT PROPERTY, BEING A 98.316-ACRE TRACT OF LAND IDENTIFIED AS TRACT 4 OF THE J. M. GASS SURVEY, ABSTRACT NO. 88, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS AND MORE FULLY DESCRIBED HEREIN BY EXHIBIT 'A' AND DEPICTED HEREIN BY EXHIBIT 'B'; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A

PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

CITY OF ROCKWALL
ORDINANCE NO. 25-16

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE [*ORDINANCE NO. 20-02*] OF THE CITY OF ROCKWALL, AS HERETOFORE AMENDED, SO AS TO APPROVE A CHANGE IN ZONING FROM AN AGRICULTURAL (AG) DISTRICT TO A NEIGHBORHOOD SERVICES (NS) DISTRICT FOR A 2.751-ACRE TRACT OF LAND IDENTIFIED AS TRACT 1-04 OF THE S. R. BARNES SURVEY, ABSTRACT NO. 13, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AND BEING MORE SPECIFICALLY DESCRIBED AND DEPICTED IN *EXHIBIT 'A'* AND *EXHIBIT 'B'* OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

CITY OF ROCKWALL
ORDINANCE NO. 25-17
SPECIFIC USE PERMIT NO. S-356

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING PLANNED DEVELOPMENT DISTRICT 50 (PD-50) [*ORDINANCE NO. 25-07*] AND THE UNIFIED DEVELOPMENT CODE [*ORDINANCE NO. 20-02*] OF THE CITY OF ROCKWALL, TEXAS, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) TO ALLOW AN *CHURCH/HOUSE OF WORSHIP* ON A 0.5010-ACRE TRACT OF LAND IDENTIFIED AS BLOCK 29 OF THE GARDNER ADDITION, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OR FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

CITY OF ROCKWALL
ORDINANCE NO. 25-19

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS AMENDING PLANNED DEVELOPMENT DISTRICT 50 (PD-50) [*ORDINANCE NO. 25-07*] AND THE UNIFIED DEVELOPMENT CODE [*ORDINANCE NO. 20-02*] OF THE CITY OF ROCKWALL, AS HERETOFORE AMENDED, FOR THE PURPOSE OF AMENDING PLANNED DEVELOPMENT DISTRICT 50 (PD-50) TO INCORPORATE AN ADDITIONAL 1.65-ACRE PARCEL OF LAND INTO THE DISTRICT BEING A 22.19-ACRE TRACT OF LAND SITUATED WITHIN THE S. S. McCURRY SURVEY, ABSTRACT NO. 146 AND B. F. BOYDSTON SURVEY, ABSTRACT NO. 14, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS AND MORE FULLY DESCRIBED HEREIN BY *EXHIBIT 'A'* AND DEPICTED IN *EXHIBIT 'B'* OF THIS ORDINANCE; PROVIDING FOR SPECIAL

CONDITIONS; PROVIDING FOR A PENALTY OF A FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

The motion to approve the Consent Agenda passed by a vote of 6 ayes with 1 absence (Johannesen).

VI. Action Items

- 1. Z2025-008** - Discuss and consider a request by Pat Atkins of Saddle Star Land Development on behalf of Betty Thompson for the approval of an **ordinance** for a Zoning Change from an Agricultural (AG) District to a General Retail (GR) District for a 6.511-acre identified as Tract 1-1 of the P. B. Harrison Survey, Abstract No. 97, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, situated within the SH-205 By-Pass Overlay (SH-205 BY-OV) District, located at the southeast corner of the intersection of John King Boulevard and FM-552, and take any action necessary **(2nd Reading)**.

Planning Director, Ryan Miller shared that this item was placed on Action Items since it did not receive unanimous approval upon 1st reading at the last city council meeting. The applicant briefly came forth and respectfully requested approval of this item. Councilmember Thomas asked the applicant if there have been any updates regarding this property since the council meeting a couple of weeks ago. Mr. Atkins, the applicant, indicated that no updates are known at this time. He commented that this rezoning will just allow them to better market the property. Councilmember Thomas expressed concerns about personally voting in favor of this zoning change.

Councilmember Lewis expressed his observation that most of this property is located within a floodplain. He then moved to approve Z2025-008. Councilmember Moeller seconded the motion. The ordinance caption was read as follows:

CITY OF ROCKWALL

ORDINANCE NO. 25-18

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, AS HERETOFORE AMENDED, SO AS TO APPROVE A CHANGE IN ZONING FROM AN AGRICULTURAL (AG) DISTRICT TO A NIEGHBORHOOD SERVICES (NS) DISTRICT FOR A 6.511-ACRE TRACT OF LAND IDENTIFIED AS TRACT 1-1 OF THE P. B. HARRISON SURVEY, ABSTRACT NO. 97, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AND BEING MORE SPECIFICALLY DESCRIBED AND DEPICTED IN *EXHIBIT 'A'* AND *EXHIBIT 'B'* OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

The motion passed by a vote of 4 ayes with 2 nays (Thomas and McCallum), and 1 absence (Johannesen).

2. Discuss and consider authorizing the City Manager to execute an agreement between The Boys and Girls Club and The City of Rockwall for the renovation of Gloria Williams Park basketball court as a Shaq / Icy Hot / Comebaq Court, and take any action necessary.

Parks Director, Travis Sales came forth and provided background information pertaining to this agenda item. He shared that this project was proposed to the City of Rockwall by Jenny Krueger, Executive Director of the Rockwall Boys & Girls Club. Mrs. Krueger then came forth and addressed the Council, indicating that Shaquille O'Neal recently bought a home in this area, and both he and his mother are alumnus of Boys & Girls Clubs. She went on to share that she researched and made a connection with Shaquille O'Neal to try and determine if any funding might be available to make an impact in the local community. She did discover that funding might be possible, and - through her research and work with the Parks Department and the Park Board - Gloria Williams Park was identified as the best location to implement renovations. They hope to have the renovations undertaken and finished by June 1; however, this will be weather-dependent. Once the opening details are known, Shaquille O'Neal will attend an unveiling or grand (re)opening type event. In the future, things like 'pop up' basketball camps will likely be held at this location. Mayor Pro Tem Jorif thanked Mrs. Krueger and all others involved for their efforts to make this renovation and its funding possible. Councilmember Thomas thanked Mrs. Krueger for this project and her efforts, expressing kind words of gratitude.

Councilmember Lewis echoed the 'thanks' given by Councilmember Thomas this evening. He then moved to authorize the City Manager to execute this agreement, as described in the item's caption above. Councilmember McCallum seconded the motion. McCallum indicated that he thinks it is a fantastic vision to extend the Boys & Girls Club into the community, beyond the Club's four walls. He thanked Mrs. Krueger for her efforts and for her commitment to the kids. Councilmember Campbell provided words of thanks to Mrs. Krueger and to Mr. Sales for their efforts.

Following the comments, the motion passed by a vote of 6 ayes to 0 nays (with one absence (Johannesen)).

3. Discuss and consider approval of an **ordinance** temporarily altering (reducing) the speed limit on the IH-30 frontage roads during (re)construction within the corporate city limits, and take any action necessary. (1st Reading)

City Manager, Mary Smith provided background information on this agenda item. She shared that TXDOT helped the city prepare the ordinance that is being put forth to Council this evening. The service roads from the area of Lakepointe Church down to Freebirds is just about to get torn up, and it will be severely impacted by construction, including drivers being on a temporary surface. She explained that it will be that way for the remainder of the year. There will be a hill that drivers will have to traverse to get over into the shopping center stores. So the city discussed with TXDOT reducing the speed limits, especially to allow for drivers to come out of the shopping centers back onto the temporary surface of the service roads. The impact that the reconstruction may have on sales tax is of concern. She went on to explain that staff member, David Sweet has gone and met personally with the businesses along IH-30. For example, the city staff has worked closely with Lakepointe Church as well as Home Depot. Home Depot's main driveway will be closed for the rest of the year, but staff has worked with them, and they have an understanding of how things will alternatively work.

Mayor Pro Tem Jorif suggested that drivers may want to divert to Ralph Hall Parkway and try to avoid the service roads. He went on to move to approve the ordinance to temporarily reduce the speed limit along the IH-30 frontage roads. Councilmember Lewis seconded the motion. Lewis commented that, being in the

concrete business, he knows that these types of projects are vitally important in that the main priority is that every single person working on this project goes home safely, without injury, each and every night. The ordinance caption was read as follows:

**CITY OF ROCKWALL
ORDINANCE NO. 25-**

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, TEMPORARILY ALTERING THE PRIMA FACIE SPEED LIMITS ESTABLISHED FOR VEHICLES UNDER THE PROVISIONS OF TRANSPORTATION CODE, SECTION 545.356 UPON INTERSTATE HIGHWAY 30 (IH-30) FRONTAGE ROADS OR PARTS THEREOF, WITHIN THE INCORPORATE LIMITS OF THE CITY OF ROCKWALL, TEXAS, AS SET OUT IN THIS ORDINANCE; AND PROVIDING A PENALTY OF A FINE NOT TO EXCEED THE SUM OF \$200.00 FOR EACH OFFENSE; AND PROVIDING FOR AN EFFECTIVE DATE.

The motion to approve passed by a vote of 6 ayes to 0 nays (with one absence (Johannesen)).

VII. City Manager's Report, Departmental Reports and Related Discussions Pertaining to Current City Activities, Upcoming Meetings, Future Legislative Activities, and Other Related Matters.

1. Building Inspections Department Monthly Report
2. Fire Department Monthly Report
3. Parks & Recreation Department Monthly Report
4. Police Department Monthly Report
5. Sales Tax Historical Comparison

City Manager, Mary Smith shared that there are some upcoming events scheduled (i.e. Special Needs Easter Egg Hunt this weekend and then the Easter Eggstravaganza at The Harbor, and Founder's Day is May 17). Also, a city council candidate forum is being held at The Center on Wednesday, April 16 at 6:00 p.m. Mrs. Smith explained that it will be live streamed and posted on social media so that it may be watched back afterwards.

VIII. Adjournment

Mayor Pro Tem Jorif adjourned the meeting at 6:57 p.m.

PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS ON THIS 21st DAY OF APRIL, 2025.

TRACE JOHANNESSEN, MAYOR

ATTEST:

KRISTY TEAGUE, CITY SECRETARY

CITY OF ROCKWALL

ORDINANCE NO. 25-20

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, TEMPORARILY ALTERING THE PRIMA FACIE SPEED LIMITS ESTABLISHED FOR VEHICLES UNDER THE PROVISIONS OF TRANSPORTATION CODE, SECTION 545.356 UPON INTERSTATE HIGHWAY 30 (IH-30) FRONTAGE ROADS OR PARTS THEREOF, WITHIN THE INCORPORATE LIMITS OF THE CITY OF ROCKWALL, TEXAS, AS SET OUT IN THIS ORDINANCE; AND PROVIDING A PENALTY OF A FINE NOT TO EXCEED THE SUM OF \$200.00 FOR EACH OFFENSE; AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, Section 545.356 of the Texas Transportation Code, provides that whenever the governing body of the City shall determine upon the basis of an engineering and traffic investigation that any prima facie speed therein set forth is greater or less than is reasonable or safe under the conditions found to exist at any intersection or other place or upon any part of a street or highway within the City, taking into consideration that width and condition of the pavement and other circumstances such portion of said street or highway, as well as the traffic thereon, said governing body may determine and declare a reasonable and safe prima facie speed limit thereat or thereon by the passage of an ordinance, which shall be effective when appropriate signs giving notice thereof are erected at such intersection or other place or part of the street or highway;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS THAT:

SECTION 1. Upon the basis of an engineering and traffic investigation heretofore made as authorized by the provisions of Transportation Code, Section 545.356, the following prima facie speed limits hereafter indicated for vehicles are hereby determined and declared to be reasonable and safe; and

SECTION 2. Such speed limits are hereby fixed at the rate of speed indicated for vehicles traveling upon the named streets and highways, or parts thereof, described within this ordinance; and

SECTION 3. That from and after the date of the passage of this speed zone ordinance, no motor vehicle shall be operated along and upon Interstate Highway No. 30 Frontage Roads within the corporate limits of the City of Rockwall in excess of speeds now set forth in “**EXHIBIT A**” of this ordinance until the completion and/or acceptance of the project shall make this ordinance null and void. At such time, the speed limit shall revert to the originally posted prima facie speed limit.

SECTION 4. The Mayor of Rockwall or his designee is hereby authorized to cause to be erected, appropriated signs indicating such speed zones; and

SECTION 5. Any person violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction thereof shall be fined in any sum not to exceed Two Hundred Dollars (\$200.00) for each offense.

PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS THIS 21st DAY OF APRIL, 2025.

Trace Johannesen, Mayor

ATTEST:

Kristy Teague, City Secretary

APPROVED AS TO LEGAL FORM:

Frank Garza, City Attorney

1st Reading: April 7, 2025

2nd Reading: April 21, 2025

Rockwall Exhibit A

Temporary Construction Speed Limit

130 Eastbound Frontage Road
130 Westbound Frontage Road



IH 30
Reference Marker 66.63

IH 30
Reference Marker 71.23



City of Rockwall
The New Horizon

MEMORANDUM

TO: Mary Smith, City Manager

FROM: Amy Williams, P.E., Director of Public Works/City Engineer

DATE: April 21, 2025

**SUBJECT: Professional Engineering Services Contract
Forest Trace Reconstruction Project**

The Forest Trace was constructed in 1972 with the High Ridge Estates Addition. There is approximately 1,200 linear feet of Forest Trace (from W. Boydston Ave. to 210 Forest Trace approximately) that needs to be fully reconstructed. This street was evaluated during the latest street assessment (2021), and the Pavement Condition Index (PCI) rated sections from 36 to 50. The PCI ratings of 36 equates to “very poor” and 50 to “poor” condition. The project will consist of roadway and sanitary sewer improvements.

Staff requested a proposal from Kimley-Horn and Associates, Inc. to provide the engineering design and specifications for this project. The engineering fee to reconstruct a portion of Forest Trace is \$234,100.00. The funding of this project will be provided by the 2018 Street Bond and water/wastewater funds. Staff requests City Council consider approval of the professional engineering services contract for Kimley-Horn and Associates, Inc. to perform the engineering design services and specifications for the Forest Trace Reconstruction Project in an amount not to exceed \$234,100.00, to be paid for out of the 2018 Street Bond funds and water/wastewater funds, and take any action necessary.

If you have any questions, please advise.

AJW:jmw

Attachments

Cc: File



Future
Proposed
Project

Project Limits



STATE OF TEXAS



COUNTY OF ROCKWALL



PROFESSIONAL ENGINEERING SERVICES CONTRACT

This Agreement is made and entered into in Rockwall County, Texas, between City of Rockwall, Texas (“CITY”), a municipal corporation and political subdivision of the State of Texas, acting by and through its City Manager and Kimley-Horn and Associates, Inc., (“ENGINEER”), located at 225 E John W. Carpenter Fwy, Ste. 1100, Irving, TX 75062, Engineers duly licensed and practicing under the laws of the State of Texas.

WHEREAS, CITY desires to engage ENGINEER as an independent contractor to render certain technical and professional services necessary for performing:

PROFESSIONAL ENGINEERING SERVICES for Forest Trace (W. Boydston Avenue to the east approximately 1,200 linear feet) Reconstruction Project.

NOW, THEREFORE, for and in consideration of the mutual covenants and agreements contained herein, the Parties hereby agree as follows:

1. Scope of Work

ENGINEER agrees to perform professional engineering services as specifically defined in this Contract as Attachment “A” and as authorized by CITY. Specifically, ENGINEER shall perform Professional services as requested by CITY and detailed in Attachment “A”.

The Parties by mutual agreement through contract amendments may provide for additional technical and professional services to be performed under the basic general terms and conditions of this Contract. CITY reserves the right to enter into another agreement with other engineering firms to provide the same or similar professional services during the term of this Contract for different projects.

2. Compensation & Term of Agreement

Cost for such services will be an amount not to exceed two hundred thirty-four thousand one hundred dollars (\$234,100.00) and billed as a lump sum and hourly basis per rates provided in Attachment “B”. ENGINEER is not authorized to perform any work beyond the limited not to exceed amount without authorized written approval by CITY.

The term of this Agreement shall commence upon execution of this agreement and follow the schedule described in Attachment “C”. In the event of termination, ENGINEER will assist the CITY in arranging a smooth transition process. However, ENGINEER’s obligation to provide services to the CITY will cease upon the effective date of termination, unless otherwise agreed in

writing.

3. Method of Payment

CITY shall pay ENGINEER its fees based on the presentation by ENGINEER to CITY of a correct monthly statement for all the amounts earned under the Contract together with reasonable supporting documentation verifying the accuracy of the fees and expenses. CITY shall then pay ENGINEER its fee within thirty (30) days after presentation of the accurate monthly statement by ENGINEER to CITY. CITY is a State sales and use tax exempt political subdivision of the State of Texas. All records supporting payment shall be kept in the offices of ENGINEER for a period of not less than three (3) years and shall be made available to CITY for inspection, audit or copying upon reasonable request.

4. Engineer's Standard of Care

ENGINEER shall provide its services under this Contract with the same degree of care, skill and diligence as is ordinarily provided by a professional engineer under similar circumstances for a similar project. ENGINEER represents that it has the capability, experience, available personnel, and means required to perform the services contemplated by this Contract. Services will be performed using personnel and equipment qualified and/or suitable to perform the work requested by the CITY. CITY retains the right to report to ENGINEER any unsatisfactory performance of ENGINEER personnel for appropriate corrective action. ENGINEER shall comply with applicable federal, state, and local laws in connection with any work performed hereunder.

ENGINEER will seek written CITY approval to accept any contract or perform any services for any person, entity, or business working on this project. CITY may waive this potential conflict, but such waiver is at CITY's sole discretion and its decision shall be final.

5. Ownership of Documents

As part of the total compensation which CITY has agreed to pay ENGINEER for the professional services to be rendered under this Contract, ENGINEER agrees that hard copies of all finished and unfinished documents, data, studies, surveys, drawings, specifications, field notes, maps, models, photographs, preliminary reports, reports, bid packet/construction contract documents/advertisement for bids incorporating any CITY standard provisions provided by ENGINEER, will remain the property of the CITY. ENGINEER will furnish CITY with paper and electronic copies, to the extent they are available, of all of the foregoing to facilitate coordination, however, ownership of the underlying work product shall remain the intellectual property of the ENGINEER. ENGINEER shall have the right to use such work products for ENGINEER's purposes. However, such documents are not intended to be suitable for reuse by CITY or others on extension of the Project or on any other project. Any reuse without the express written consent of the ENGINEER will be at reuser's sole risk and without liability or legal exposure to the ENGINEER, and CITY to the extent allowed by law, shall hold harmless the ENGINEER from all claims, damages, losses, expenses, and costs, including attorneys' fees

arising out of or resulting from the reuse of said documents without the ENGINEER's consent. The granting of such consent will entitle the ENGINEER to further compensation at rates to be agreed upon by CITY and the ENGINEER. The above notwithstanding, ENGINEER shall retain all rights in its standard drawing details, designs, specifications, databases, computer software and any other proprietary and intellectual property information provided pursuant to this Contract, whether or not such proprietary information was modified during the course of providing the services.

6. Insurance

A. ENGINEER agrees to maintain Worker's Compensation and Employer's Liability Insurance to cover all of its own personnel engaged in performing services for CITY under this Contract in at least the following amounts:

Workmen's Compensation – Statutory
Employer's Liability – \$100,000
Bodily Injury by Disease - \$500,000 (policy limits)
Bodily Injury by Disease - \$100,000 (each employee)

B. ENGINEER also agrees to maintain Commercial General Liability, Business Automobile Liability, and Umbrella Liability Insurance covering claims against ENGINEER for damages resulting from bodily injury, death or property damages from accidents arising in the course of work performed under this Contract in not less than the following amounts:

\$2,000,000 General aggregate limit

\$1,000,000 each occurrence sub-limit for all bodily injury or property damage incurred all in one occurrence

\$1,000,000 each occurrence sub-limit for Personal Injury and Advertising

C. ENGINEER shall add CITY, its City Council members and employees, as an additional insureds on all required insurance policies, except worker's compensation, employer's liability and errors and omissions insurance. The Commercial General Liability Policy and Umbrella Liability Policy shall be of an "occurrence" type policy.

D. ENGINEER shall furnish CITY with an Insurance Certificate on the date this Contract is executed and accepted by CITY, which confirms that all above required insurance policies are in full force and effect.

E. ENGINEER agrees to maintain errors and omissions professional liability insurance in the amount of not less than one million dollars (\$1,000,000) annual aggregate, on a claims made basis, as long as reasonably available under standard policies.

7. **INDEMNIFICATION**

ENGINEER SHALL INDEMNIFY AND SAVE HARMLESS THE CITY AND ITS CITY COUNCIL MEMBERS AND EMPLOYEES FROM SUITS, ACTIONS, LOSSES, DAMAGES, CLAIMS, OR LIABILITY, INCLUDING WITHOUT LIMITING THE GENERALITY OF THE FOREGOING, ALL EXPENSES OF LITIGATION, COURT COSTS, AND REASONABLE ATTORNEY'S FEES FOR INJURY OR DEATH TO ANY PERSON, OR INJURY TO ANY PROPERTY, RECEIVED OR SUSTAINED BY ANY PERSON OR PERSONS OR PROPERTY, TO THE EXTENT CAUSED BY THE NEGLIGENT ACTS OF ENGINEER OR ITS AGENTS OR EMPLOYEES, IN THE EXECUTION OF PERFORMANCE OF THIS CONTRACT.

ENGINEER'S TOTAL LIABILITY TO CITY FOR ANY LOSS OR DAMAGES FROM CLAIMS ARISING OUT OF, OR IN CONNECTION WITH, THIS CONTRACT FROM ANY CAUSE INCLUDING ENGINEER'S STRICT LIABILITY, BREACH OF CONTRACT, OR PROFESSIONAL NEGLIGENCE, ERRORS AND OMISSIONS SHALL NOT EXCEED ONE MILLION DOLLARS (\$1,000,000). NEITHER PARTY TO THIS AGREEMENT SHALL BE LIABLE TO THE OTHER PARTY OR ANY THIRD PARTY CLAIMING THROUGH THE OTHER RESPECTIVE PARTY, FOR ANY SPECIAL, INCIDENTAL, INDIRECT, PUNITIVE, LIQUIDATED, DELAY OR CONSEQUENTIAL DAMAGES OF ANY KIND INCLUDING BUT NOT LIMITED TO LOST PROFITS OR USE OF PROPERTY, FACILITIES OR RESOURCES, THAT MAY RESULT FROM THIS AGREEMENT, OR OUT OF ANY GOODS OR SERVICES FURNISHED HEREUNDER.

8. **Addresses for Notices and Communications**

CITY

Amy Williams, P.E.
Director of Public Works/City Engineer
City of Rockwall
385 S. Goliad Street
Rockwall, Texas 75087

ENGINEER

Jacob Reinig, P.E.
Project Manager
225 E W. John Carpenter Fwy, Ste. 1100
Irving, TX 75062

All notices and communications under this Contract shall be mailed or delivered to **CITY** and **ENGINEER** at the above addresses.

9. Successors and Assigns

CITY and ENGINEER each binds itself and its successors, executors, administrators and assigns to the other parties of this Contract and to the successors, executors, administrators and assigns of such other parties, in respect to all covenants of this Contract. Except as noted in the first part of this Paragraph, neither CITY nor ENGINEER shall assign, sublet or transfer its interest in this Contract without the written consent of the other. Nothing herein shall be construed as creating any personal liability on the part of any officer, council member, employee or agent of any public body which is a party hereto.

10. Termination for Convenience of the Parties

ENGINEER and CITY may terminate this Contract for their convenience at any time by giving at least thirty (30) days notice in writing to each other. If the Contract is terminated by CITY and/or ENGINEER as provided herein, ENGINEER will be paid for the Work provided and expenses incurred up to the termination date, if such final compensation is approved by CITY, in its sole discretion. If this Contract is terminated due to the fault of ENGINEER, Paragraph 10 hereof, relative to Termination for Cause, shall apply.

11. Changes

CITY may, from time to time, request changes in the Scope of Work of ENGINEER to be performed hereunder. Such changes, including any increase or decrease in the amount of ENGINEER's compensation, or time for performance, which are mutually agreed upon by and between CITY and ENGINEER, shall be incorporated in written amendments to this Contract. Any subsequent contract amendments shall be executed by the City Manager or other authorized representative as designated by the City Manager or City Council.

Any alterations, additions or deletions to the terms of this Contract, including the scope of work, shall be by amendment **in writing** executed by both CITY and ENGINEER.

13. Reports and Information

ENGINEER, at such times and in such forms as CITY may reasonably require, and as specified in the Scope of Work or in additional Contract Amendments shall furnish CITY periodic reports pertaining to the Work or services undertaken pursuant to this Contract, the cost and obligations incurred, or to be incurred in connection therewith, and any other matter covered by this Contract.

14. Entire Agreement

This Contract and its Attachments and any future Contract Amendments constitute the entire agreement, and supersede all prior agreements and understandings between the parties concerning the subject matter of this Contract.

15. Waiver

The failure on the part of either party herein at any time to require the performance by the other party, of any portion of this Contract, shall not be deemed a waiver of, or in any way affect that party's rights to enforce such provision, or any other provision. Any waiver by any party herein of any provision hereof, shall not be taken or held to be a waiver of any other provision hereof, or any other breach hereof.

16. Severability

The invalidity or unenforceability of any provision of this Contract shall not affect the validity or enforceability of any other provision of this Contract.

17. Survival

Any and all representations, conditions and warranties made by ENGINEER under this Contract are of the essence of this Contract and shall survive the execution, delivery and termination of it.

18. Governing Powers and Law

Both Parties agree and understand that the City does not waive or surrender any of its governmental powers by execution of this Agreement. To that end, the parties further understand that this agreement shall not be considered a contract for goods or services under Texas Local Government Code, Section 271.151 and Contractor waives any right or entitlement granted said provisions. This Contract is governed by the laws of the State of Texas and all obligations of the parties under this Contract are performable in Rockwall County, Texas.

19. Attorney's Fees

If it is necessary for either Party herein to file a cause of action at law or in equity against the other Party due to: (a) a breach of this Contract by the other Party and/or (b) any intentional and/or negligent act or omission by the other Party arising out of this Contract, the non-breaching or non-negligent Party shall be entitled to reasonable attorney's fees and costs, and any necessary disbursements, in addition to any other relief to which it is legally entitled.

20. State or Federal Laws

This Contract is subject to all applicable federal and state laws, statutes, codes, and any applicable permits, ordinances, rules, orders and regulations of any local, state or federal government authority having or asserting jurisdiction, but nothing contained herein shall be construed as a waiver of any right to question or contest any such law, ordinance, order, rule or regulation in any forum having jurisdiction.

EXECUTED in triplicate originals on this 9 day of April 2025 .

Kimley-Horn and Associates, Inc.

By: *L. Nathan Ante*

Name: L. Nathan Ante

Title: Senior Vice President

EXECUTED in triplicate originals on this ____ day of _____ 202__.

ATTEST:

City of Rockwall, Texas

Mary Smith
City Manager

Attachment "A"
Forest Trace Reconstruction Project
For
City of Rockwall
Scope of Services

Project Understanding

The City of Rockwall (City) wishes to reconstruct Forest Trace from W. Boydston Avenue to the east for approximately 1,200 feet in length. The roadway lies within a residential area of the City of Rockwall. Reconstruction will be two lane undivided: two vehicle lanes (28' F-F) with concrete curb and gutter and parkways graded for future sidewalks on each side. These improvements will be within the existing 50' Right of Way (ROW); however, the contract will include a provision for parcel take or easements in the event that they are required.

Scope of work also includes storm sewer and sanitary sewer improvements. The existing storm sewer will be analyzed using current City drainage criteria and improvements will be identified. Sanitary sewer will be reconstructed for approximately 1,200 linear feet. Water lines are not planned for improvements as part of this scope of work.

Other design components include coordination with franchise utilities, traffic control, and construction phasing.

Consultant will prepare drawings, specifications, estimates, and permits for the project and they will provide support to the City of Rockwall's advertising/bidding and construction phase services.

BASIC SERVICES

1. Schematic Conceptual Design (30%)

1.1. Project Management

- 1.1.1 Conduct Consultant internal project kickoff meeting with entire project team and City Staff.
- 1.1.2 Prepare and send Project Status Update (PSU) to City's Project Manager on monthly basis. PSU will address work completed in previous period, work in progress and/or anticipated to be completed in next period, actions or information needed from City, and a schedule update.
- 1.1.3 Develop design criteria and design decisions tables.
- 1.1.4 Prepare and submit project schedule to City staff and provide general project management activities.
- 1.1.5 Project Administration
 - Prepare project correspondence and invoicing documents.
 - Industry standard quality control and quality assurance.

1.2. Project stakeholder coordination

- 1.2.1 Develop project stakeholder contact list including but not limited to franchise utilities, City's sewer department, traffic department, and other governmental entity stakeholder.
- 1.2.2 Conduct progress meetings to monitor the development of the project. During this phase of the project, conduct up to two (2) meetings with the City staff regarding project status and coordination issues. The first meeting will be the project kick-off meeting, and the second will be the conceptual design presentation and review. Additional meetings requested by the City may be provided as an *Additional Service*.

1.3. Schematic Roll Plot and Graphics (30%)

The following services will be provided as part of the Conceptual Design Phase (30%):

- 1.3.1 Review and research previously prepared construction plans record documents, and other pertinent information related to proposed project.
- 1.3.2 Review City master plans, design standards, specifications, construction details, and other pertinent information that may impact the design. Review of documents will include but not limited to:
 - City of Rockwall Master Thoroughfare Plan
 - City of Rockwall Water/Wastewater Master Plan
 - City of Rockwall Standards of Design and Construction
- 1.3.3 Develop base map showing existing Right-of-Way (ROW), easements, and utilities.
- 1.3.4 Develop and evaluate typical section to use for Forest Trace and other intersecting streets within the project limits.
- 1.3.5 Develop horizontal alignments for roadway.
- 1.3.6 Develop and evaluate alignment for proposed sanitary sewer line. No water line improvements are included in this scope of work.
- 1.3.7 Develop vertical alignment for roadway and cut cross sections at critical points, at center of driveways, alley approaches, cross streets, lead walks, at alignment geometric points and at 50' intervals to evaluate the grading.
- 1.3.8 Prepare 30% schematic paving plan and profile roll plots, produced at 1" =40' horizontal scale and 1" =5' vertical scale. The horizontal alignment of proposed sanitary sewer lines will be included in the roll plots.
- 1.3.9 Prepare cross sections sheets.
- 1.3.10 Meet with City staff and present concept paving plan and profile roll plots.
- 1.4. Drainage Analysis (30%)

The following services will be provided as part of the Conceptual Design Phase (30%):

 - 1.4.1 Review and research existing construction plans and record documents, historical flooding events and other pertinent information related to the existing drainage infrastructure along, upstream and downstream of the proposed project.
 - 1.4.2 Existing drainage infrastructure analysis
 - Drainage Area Map of existing conditions/infrastructure.
 - Analyze the effect on existing system of proposed off-site development or infrastructure improvements currently being planned.
 - Drainage calculations – Drainage Area, Inlet, Storm Sewer, Culvert, etc. (to be in City Standard's tabular format).
 - Synopsis of hydraulic deficiencies in written and plan format, includes downstream off-site drainage infrastructure.
 - 1.4.3 Recommendation of improvements to existing drainage system to meet the City's current 100-year design storm criteria.
 - Drainage Area Map of proposed improvements
 - Proposed drainage calculations – Drainage Area, Inlet, Storm Sewer, Culvert, etc. (to be in City Standard's tabular format)
 - Summary of proposed improvement of drainage infrastructure in written and plan format to fix hydraulic and structural deficiencies in order to meet current City drainage criteria standards.

2. Construction Documents

2.1. Project Management

- 2.1.1 Conduct progress meetings at each stage of the construction documents phase to monitor the development of the project regarding project status, coordination issues and go over City review comments of the plans.
- 2.1.2 Prepare and send Project Status Update (PSU) to City's Project Manager on monthly basis. PSU will address work completed in previous period, work in progress and/or anticipated to be completed in next period, actions or information needed from City, and a schedule update.
- 2.1.3 Update project schedule and interim milestones (coordinate with City staff as necessary)
- 2.1.4 Project site visits (up to two (2) trips)

2.2. Construction Plans, Specifications and Estimates (60%, 90%, 100%)

2.2.1 Submittals

Consultant to provide 60%, 90% and 100% submittals for the City for review and comment. The following will be required for each submittal:

2.2.1.1. Construction Plans

- One (1) PDF copy
- One (1) PDF copy with City comments from previous submittal

2.2.1.2. Engineer's Opinion of Probable Construction Cost

2.2.1.3. Construction Contract Template/Specification Manual

- This space left intentionally blank -

2.2.2 Construction Plans

Each plan sheet in the construction plans set will conform to the Engineering Drawing Requirements set forth in the City of Rockwall’s Standards of Design and Construction. Plan sheets will also include any other additional documentation, notation, and clarification required by the City’s Project Manager to depict design and constructability of the project. Each stage of the construction plan review process will incorporate any changes associated with City comments on previous submittals.

Below is a table of the required construction plan sheets and which submittals they are required.

| Plan Sheets | 60% | 90% | 100% |
|--|--------------|------------|-------------|
| Cover Sheet | X | X | X |
| Legend Sheet | X | X | X |
| General Notes Sheet | X | X | X |
| Existing Condition Plan (1"=20' scale) | X | X | X |
| Horizontal/Vertical Control Sheet (1"=40' scale) | X | X | X |
| Typical Sections (Existing and Proposed) | X | X | X |
| Erosion Control Plan (1"=40' scale) | | X | X |
| Demolition/Removals Plan (1"=20' scale) | X | X | X |
| Roadway Plan/Profiles (1"=20'H, 1"=5'V scale) | X | X | X |
| Alley and Driveway Plan/Profile (1"=20'H, 1"=5'V scale) | | X | X |
| Intersection Grading Plan (major and critical intersections) | Not Included | | |
| Pavement Signage and Striping Plan (1"=20' scale) | | X | X |
| Drainage Area Map (Existing and Proposed) | X | X | X |
| Drainage Calculations (Area, Inlet, Storm, Culvert) | | X | X |
| Drainage Plan/Profile (Trunks and Laterals) (1"=20'H, 1"=5'V scale) | X | X | X |
| Water Plan/Profile (1"=20'H, 1"=5'V scale) | Not Included | | |
| Sanitary Sewer Plan/Profile (1"=20'H, 1"=5'V scale) | X | X | X |
| Retaining Wall Plan/Profile (1"=20'H, 1"=5'V scale) | Not Included | | |
| Lighting/Illumination Plan | | X | X |
| Traffic Signal Plans (Existing and Proposed) | Not Included | | |
| Construction Sequencing Phasing Narrative | X | X | X |
| Detour Plan (for each Phase) | | X | X |
| Traffic Control Plan (for each phase & typ. sections) (1"=40' scale) | | X | X |
| Rdwy Cross-Sections (50' int. & crit. pts.) (1"=20'H, 1"=5'V scale) | X | X | X |
| Structural Layout Sheets | Not Included | | |
| Details (for each section of plans) | | X | X |

Each subsection below details the services to be performed for each section of the construction plan set.

2.2.2.1. Roadway Design

- Incorporate the preliminary design submittal review comments.
- Prepare plan / profile sheets.
- Analyze all driveways within the project and develop driveway adjustment and crosswalk profiles as needed. Typically driveways will be defined vertically by profiles.
- Develop design cross-sections.

- Develop on 50' station intervals and other critical points.
- Show pavement and subgrade, right-of-way limits, sides slopes, pavement cross slopes, curbs, and retaining walls (if applicable)
- Cross-sections will be provided in the plan set.
- Prepare roadway details to clarify intent of design.
- Modify standard details as needed and include.

2.2.2.2. Drainage

- Create Drainage Area Maps (existing and proposed)
- Create Drainage Calculation Sheets (Tables to be formatted to match City Standard's tabular format)
- Identify the need for additional drainage easements to accommodate the proposed offsite drainage improvements (if required)
- Perform final storm sewer sizing and alignment design. Prepare final inlet design sheets and hydraulic design sheets for inclusion in the plans.
- Prepare storm sewer plan/profile sheets depicting storm sewer lines, inlets, junction boxes, connections, manholes, utility crossings and the hydraulic grade lines.
- Prepare profiles for all trunks and laterals (to be profiled along centerline of pipe)
- If drainage flow patterns/areas are changing due to proposed improvements then existing downstream and upstream systems will be analysed and included in plan set to prove that there are no adverse impacts as a result. Full Hydrologic and Hydraulic Calcs Sheets and storm sewer plan and profiles sheets for these existing upstream and downstream systems are to be included in the plan set.
- Prepare drainage details to clarify intent of design.
- Compile applicable City standard details. Modify standard details as needed.
- Prepare erosion control plan

2.2.2.3. Utilities

- Establish the location of existing public utilities based upon information provided by the City, SUE and field survey information.
- Franchise Utility Coordination
 - Identify franchise utility contacts.
 - Submit drawings to each franchise utility concurrent with each milestone submittal. Request franchise utility to markup drawings to show the size, type, and location of utilities.
 - Notify the City if relocations are required.
 - Consultant shall perform coordination on behalf of the City for any relocations.
 - Attend City Franchise Utility Coordination Meetings. Consultant to provide project background and updates on project status.
- Water
 - No water line improvements are included in this scope of work.
 - Minor adjustments to existing water valve boxes, etc. will be specified in the plans.
 - Check coverage of Fire Hydrants to meet City Standard.
- Sanitary Sewer
 - Prepare the plan and profile plan for approximately 1,200 linear feet of 8" sanitary sewer main.
 - Minor adjustments to existing manholes, etc. will be specified in the plans.
 - Prepare sanitary sewer details to clarify intent of design. Compile applicable City Standard details, modify standard details as needed.

2.2.2.4. Lighting/Illumination

- No lighting design is included in this scope of work.

- Lighting improvements will specify locations of standard light pole foundations. It is assumed that existing electrical service locations will be adequate.
- Drawings will be submitted to electrical provider for review. Provider will perform installation of conductors and light poles and fixtures

2.2.2.5. Traffic

- Pavement Marking and Signing Plans
 - Determine potential conflicts with existing signs and propose sign relocations.
 - Prepare crosswalk pavement marking and signing layouts in accordance with City design standards and the Texas Manual on Uniform Traffic Control Devices (TMUTCD).
 - Prepare details to clarify intent design.
 - Compile applicable City standard details. Modify standard details as needed.
- Traffic Control Plans, Detour Plans, Construction Sequencing/Phasing
 - Develop construction sequence plan showing:
 - Construction area for each phase of construction and closure of existing travel lanes (if needed).
 - Temporary signing and striping, barricades, and other channelization device
 - Narrative of the sequence of work.
 - Detour Plan for each construction phase
 - Storm sewer construction phasing.
 - Public utilities relocation phasing.
 - Pedestrian routing during construction.
 - Develop typical cross section showing lane widths, edge conditions, channelization and proposed construction area.
 - Develop typical driveway staging plans for similar driveways. Develop custom driveway staging layouts for special conditions.
 - Prepare traffic control details to clarify intent of design.

2.3. Specifications/Construction Contracts

Prepare general notes, specification data, and final Project Manual in accordance with City of Rockwall requirements. Identify and prepare special specifications and/or special provisions applicable to the project. Prepare Bid-Tab sheets of applicable items for Contractor to fill/present bid prices.

2.3.1 Contact documents, including the following:

- City of Rockwall standard construction contract forms
- Notice to bidders.
- Special instructions to bidders
- Bid form (Bid-Tab) – Electronic for Bidding
- Standard construction contract
- Performance bond
- Payment bond
- Maintenance bond
- Certificate of insurance
- General conditions
- Special conditions
- Technical specifications
- Permits
- Geotechnical Report

- 2.4. Opinion of Probable Construction Cost
 - 2.4.1 Compile an overall opinion of probable construction cost (OPCC) based on current market values. Cost will be broken out in to separate schedules for each section of the job.
 - 2.4.2 OPCC to be provided at each milestone submittal.

3. Bidding Phase

- 3.1. Furnishing the City a construction bid set one (1) hard copy and Project Manual.
- 3.2. City will be responsible for advertisement of the project in the Local Paper.
- 3.3. Consultant will make the plans and specifications available on the Bid document distribution site CivCast.
- 3.4. During this phase service to be provided are:
 - 3.4.3 Respond to contractor questions and provide design clarifications.
 - 3.4.4 Attendance at the pre-bid meeting.
 - 3.4.5 Production of all Addenda items and designs changes, submittal to City for review and correction before issuance.
 - 3.4.6 Attendance of Bid Opening
 - 3.4.7 Tabulating Bids and checking for errors.
 - 3.4.8 Checking references and other qualification information for the low bidder and writing a letter of recommendation concerning award of the contract.
 - 3.4.9 Returning Bid Bonds to all non-low bidders.

SPECIAL SERVICES

4. Data Collection & Property Research / Topographic and Design Survey

- 4.1. Data Collection and Property Research
 - 4.1.1 Gather existing plat information.
 - 4.1.2 Collect property owner and record information.
 - 4.1.3 Gather existing ROW and easement information, identify recorded easements through typical research methodologies (i.e. plats, court house filings, etc.).
- 4.2. Topographic and Design Survey
 - 4.2.1 The limits of the survey shall be from the intersection of Forest Trace and W. Boydston Avenue to the east, along Forest Trace, for approximately 1,200 linear feet. The survey shall be from ROW line to ROW line (50 foot ROW) plus 10 feet beyond ROW (70 feet total width). Survey will include intersecting roadways extending 200 feet along each leg of the minor intersections, alleys for 100 feet beyond ROW, and driveways to residential properties for 10 feet beyond ROW.
 - 4.2.2 Establish horizontal control points at 500' maximum spacing.
 - 4.2.3 Establish a vertical control benchmark circuit as needed throughout the project. Use existing City established monument information for the City and tie survey into.
 - 4.2.4 Set control points, which shall be based on NAD-83, on both sides of the roadway, as required to maintain horizontal control throughout the project limits.
 - 4.2.5 Perform a field survey to identify and locate existing topographic elements within the roadway corridor. Identified topographic elements shall be tied down horizontally and vertically. The field survey shall not limited to the following:
 - Property corner monumentation
 - Existing pavement, curbs, sidewalks, barrier free ramps, etc.
 - Roadway and lane striping
 - Driveways
 - Existing storm sewer inlets, manholes, junction boxes Headwalls (including culvert sizes and invert elevations)

- Outfalls, bridges and erosion control
 - Existing driveway culverts and swales (flowlines, sizes, types, etc.)
 - Guardrail
 - Utility manholes, vaults, water valves, water meters, wastewater cleanouts, sprinkler heads, telephone poles, power poles, utility markers, other public utilities and franchise utilities
 - Traffic Signal poles, cabinets, pull boxes and other signal equipment.
 - Signs (excluding temporary signs)
 - Trees (all sizes)
 - Landscape planters, Shrubs, rocks and other features.
 - Buildings and permanent structures
 - Retaining walls
 - Fencing walls
 - Fence limits and material types (excluding temporary fences)
 - Mailboxes (types – wood, brick, stone)
 - Other applicable physical features that could impact design.
- 4.2.6 Perform cross-sections throughout project limits at 50-foot intervals and at grade breaks and prepare cross-section field notes.
- 4.2.7 Prepare a final topographic drawing in digital format (including contours and breaklines) showing the features located in the field, an ASCII coordinate file of the points located in the field, and a hard copy of the coordinates and feature descriptions.
- 4.2.8 Consultant will perform site visits for the purposes of reviewing the provided survey in the field and making notes for additional survey needs and design considerations

5. Subsurface Utility Engineering (S.U.E.) Services

- 5.1. Provide S.U.E. Services for the purpose of locating horizontal and vertical locations of underground city and franchise utilities, in conjunction with and /or prior to the field survey.
- 5.1.1 S.U.E. Level ‘B’ Services will be provided to determine horizontal location of utilities for the project limits identified in Task 4.
- 5.1.2 S.U.E. Level ‘A’ Services will be provided as follows:
- Up to three (3) test holes of 0-10 ft depth are budgeted for this task.
 - Test holes in addition to those listed above will be provided at the rate per test hole agreed to by the City of Rockwall.
- 5.2. The Consultant shall arrange and make provisions for access to perform the services specified within this scope. The surveyor shall provide the City with the name and address of the property owners.

6. Geotechnical Investigation

- 6.1. Subsurface Exploration
- 6.1.1 Three (3) borings are planned for this project. The borings and report will be through a subconsultant.
- 6.1.2 Boring Services – Pavement borings will be sampled to 10 feet. A total of 30 feet for drilling is planned.
- Check samples for consistency with a hand penetrometer.
 - Stake the boring locations using normal taping procedures.
 - Backfill bore holes and plug at the surface.

6.2. Laboratory Services

6.2.1 Considering the planned facilities, anticipated soil conditions, and geology, the following laboratory test will be required for classification purpose and to determine strength characteristics:

- Visual Classification
- Moisture content and soil identification
- Liquid and plastic limit determinations
- Unconfined compression
- Calibrated hand penetrometer tests
- Lime/ph series tests
- Soluble sulfate tests (to determine lime-induced heaving potential)

6.3. Engineering Services

6.3.1 Prepare an engineering report presenting the following:

- Sample boring location map
- Boring logs and laboratory results
- General soil and ground-water conditions
- Recommendations will be based on a 30 year design life.
- Subgrade design recommendations for lime treated and cement treated.
- Pavement design recommendation for reinforced concrete, asphalt or combination of concrete and asphalt system (including pavement thickness, strength, reinforcement size and spacing, and joint spacing)
- Earthwork recommendations

6.3.2 The geotechnical engineer's design recommendations shall be relied upon by the civil and structural engineers for their design.

6.3.3 Submit one (1) paper copy and one pdf file copy of the report to the City

7. Construction Phase Services

The scope of services listed below may or may not be performed as part of our construction phase services. Kimley-Horn's role during construction is limited and services are only provided upon request of the Client and billed on a reimbursable basis as labor and direct expenses are incurred. The budgeted fee for this task is based upon approximately 60 hours of labor. We will not proceed with performance of services beyond the hours budgeted without written authorization by the Client.

7.1. Consultant to provide five (5) full size set of construction plans and contract documents.

7.2. Bid Document Preparation and Contractor Notification. Consultant will issue a bid package and conduct a pre-bid meeting with potential bidders. Consultant will tabulate the bids received and evaluate general compliance of bids with the bidding documents. Consultant will provide a summary of this tabulation and evaluation. If requested, Consultant will notify the selected Contractor.

7.3. Pre-Construction Conference. Consultant will attend a Pre-Construction Conference before the start of construction.

7.4. Site Visits and Construction Observation. Consultant will make visits to observe the progress of the work. Observations will not be exhaustive or extend to every aspect of Contractor's work, but will be limited to spot checking, and similar methods of general observation. Based on the site visits, Consultant will evaluate whether Contractor's work is generally proceeding in accordance with the Contract Document and keep Client informed of the general progress of the work.

Consultant will not supervise, direct, or control Contractor's work, and will not have authority to stop the Work or responsibility for the means, methods, techniques, equipment choice and use, schedules, or procedures of construction selected by Contractor, for safety programs incident to

Contractor's work, or for failure of Contractor to comply with laws. Consultant does not guarantee Contractor's performance and has no responsibility for Contractor's failure to perform in accordance with the Contract Documents.

Consultant is not responsible for any duties assigned to it in the construction contract that are not expressly provided for in this Agreement.

- 7.5. Construction Meetings. Consultant will attend construction meetings on site.
- 7.6. Clarifications and Interpretations. Consultant will respond to reasonable and appropriate Contractor requests for information made in accordance with the Contract Documents and issue necessary clarifications and interpretations. Any orders authorizing variations from the Contract Documents will be made only by Client.
- 7.7. Shop Drawings and Samples. Consultant will review Shop Drawings and Samples and other data which Contractor is required to submit, but only for general conformance with the Contract Documents. Such review and any action taken in response will not extend to means, methods, techniques, equipment choice and usage, schedules, or procedures of construction or to related safety programs. Any action in response to a shop drawing will not constitute a change in the Contract Documents, which can be changed only through the Change Orders.
- 7.8. Substitutes and "or-equal/equivalent." Consultant will evaluate the acceptability of substitute or "or-equal/equivalent" materials and equipment proposed by Contractor in accordance with the Contract Documents.
- 7.9. Substantial Completion. When requested by Contractor and Client, Consultant will conduct a site visit to determine if the Work is substantially complete. Work will be considered substantially complete following satisfactory completion of all items with the exception of those identified on a final punch list.
- 7.10. Record Drawings, prepare project "Record Drawing" based on information provided by the Contractor, and/or City as to the actual field placement of the work including any changes or deletions. Consultant will provide the following deliverables:
 - 7.10.1 Electronic copy of the record drawings shall be submitted to City in *.pdf format
 - 7.10.2 Design files will be provided in *.dwg format
 - 7.10.3 Electronic copy of geotechnical reports, survey data, photographs, and exhibits used for the project, in .pdf or jpg format.
 - 7.10.4 Electronic copy of project specifications in Microsoft Word (.doc) and PDF format.
 - 7.10.5 Record drawings shall be prepared in accordance with the Texas Board of Professional Engineers Policy Advisory Opinion Regarding Record Drawings.
 - 7.10.6 All field changes and revisions shall be shown and noted in the revision block.
 - 7.10.7 Revisions shall be drawn using industry drafting standards and shall be clear and legible.
 - 7.10.8 Line work and notes related to work deleted or changed shall be omitted from the Record Drawing.

8. Additional Services

Services not specifically identified in the Scope of Services above shall be considered additional and shall be performed on an individual basis upon authorization by the City of Rockwall. Such services shall include, but are not limited to the following:

- 8.1. Construction staking
- 8.2. Title research
- 8.3. Design of any offsite drainage improvements beyond the improvements identified in the scope.
- 8.4. Design of water line improvements
- 8.5. Traffic signal timing
- 8.6. Design of lighting improvements

- 8.7. Design of retaining walls, specialized inlets, or gabion mattress erosion control systems
- 8.8. Preparation for and attendance at additional public meetings
- 8.9. Furnish additional copies of review documents and/or bid documents in excess of the number of the same identified above.
- 8.10. Assist the City as an expert witness in litigation in connection with the project or in hearings before approving and regulatory agencies.
- 8.11. Redesign to reflect project scope changes requested by the City, required to address changed conditions or change in direction previously approved by the City, mandated by changing governmental laws, or necessitated by the City's acceptance of substitutions proposed by the contractor.

- End of Scope of Services -

ATTACHMENT “B”

Payment Schedule

Compensation for Basic Services in Tasks 1-3 shall be on a lump sum basis. The tabulation below establishes the not to exceed amount for each category of contract service:

| | <u>Task</u> | <u>Fee</u> |
|--|---|-------------------|
| | <u>BASIC SERVICES</u> | |
| 1. | Schematic Conceptual Design (30%) | \$ 20,600 |
| 2. | Construction Documents | |
| 2.1 | 60% Design | \$ 32,100 |
| 2.2 | 90% Design | \$ 38,700 |
| 2.3 | 100% Design | \$ 37,300 |
| 3. | Bidding Phase Services | \$ 10,900 |
| | Basic Services Subtotal: | \$ 139,600 |
| Compensation for special services under Tasks 4-7 shall be a labor fee plus expense (reimbursable) basis with the max fee shown below. | | |
| | <u>SPECIAL SERVICES</u> | |
| 4. | Data Collection & Property Research / Topographic and Design Survey | \$ 16,900 |
| 5. | Subsurface Utility Engineering (S.U.E.) Services | |
| 5.1 | Quality Level ‘C/D’ S.U.E. Services | \$ 0 |
| 5.2 | Quality Level ‘B’ S.U.E. Services | \$ 16,200 |
| 5.3 | Quality Level ‘A’ S.U.E. Services | \$ 9,600 |
| 6. | Geotechnical Investigation | \$ 10,200 |
| 7. | Construction Phase Services | \$ 20,300 |
| | Special Services Subtotal* | \$ 73,200 |
| | <u>ENGINEERING SERVICES CONTINGENCY** (To be 10% of Services)</u> | \$ 21,300 |
| | <i>** (This service is a miscellaneous amount to be used at the discretion of the City for Additional Services outside of the scope of the contract. This item will be controlled by the City and will only be used if the City chooses. The ENGINEER has no right or guarantee to the use of this Contingency)</i> | |
| | Project Total* | \$ 234,100 |

Labor fees for tasks 1 - 3 will be invoiced monthly based upon the overall percentage of services performed.

As to these tasks, direct reimbursable expenses such as express delivery services, fees, air travel, and other direct expenses will be billed at 1.15 times cost. A percentage of labor fee will be added to each invoice to cover certain other expenses as to these tasks such as telecommunications, in-house reproduction, postage, supplies, project related computer time, and local mileage. Administrative time related to the project may be billed hourly. All permitting, application, and similar project fees will be paid directly by the City unless noted otherwise in Attachment “A”.

ATTACHMENT "C"



Forest Trace: Project Schedule

Project No. XXXXXX

Updated: 4/9/2025



ATTACHMENT “D”

Sub-Consultants

1. Sub-Consultant:

Company Name: Yellow Rose Mapping, LLC
Services of the Scope Being Provided: S.U.E.
Contact Person: Tim Habenicht Title: Director
Email: tim.habenicht@yellowrosemapping.com
Phone: 214-493-8531

2. Sub-Consultant:

Company Name: Survey Consultants, Inc.
Services of the Scope Being Provided: Topographic/Boundary Survey
Contact Person: Jason Lashlee Title: Survey Manager
Email: jason@surveyconsultantsinc.com
Phone: 972-424-7002

3. Sub-Consultant:

Company Name: CMJ Engineering, Inc.
Services of the Scope Being Provided: Geotechnical Investigation
Contact Person: Matt Kammerdiener Title: Senior Engineer
Email: makammerdiener@cmjeng.com
Phone: 817-284-9400



City of Rockwall
The New Horizon

MEMORANDUM

TO: Mary Smith, City Manager

FROM: Amy Williams, P.E., Director of Public Works/City Engineer

DATE: April 21, 2025

**SUBJECT: Professional Engineering Services Contract
Amendment for North Lakeshore Rehab Project**

City Council approved a profession engineering services contract with Kimley-Horn and Associates, on June 17, 2024. The design engineers have determined that the existing storm system in North Lakeshore and downstream could be deficient in drainage capacity. Additional engineering design and study needs to be performed to determine the possible construction improvement.

Staff requested an amendment to the existing Kimley-Horn and Associates, Inc. contract for the professional engineering services for this project. The amendment will increase the approved contract to \$1,811,800 (an additional \$258,100). The funding of this project will be provided by the 2018 Street Bond and water/wastewater funds. Staff requests City Council consider approval of the amendment to the professional engineering services contract for Kimley-Horn and Associates, Inc. in the additional amount of \$258,100.00, to be paid for out of the 2018 Street Bond funds and water/wastewater funds, and take any action necessary.

If you have any questions, please advise.

AJW:jmw

Attachments

Cc:
File

**AMENDMENT NUMBER 2 TO THE AGREEMENT BETWEEN
THE CLIENT AND KIMLEY-HORN AND ASSOCIATES, INC.**

AMENDMENT NUMBER 2 DATED April 9, 2025 to the agreement between City of Rockwall, ("CITY") and Kimley-Horn and Associates, Inc., ("ENGINEER") dated June 10, 2024 ("the Agreement") concerning N. Lakeshore Drive (State Highway 66 to Masters Boulevard) and Reconstruction Project (the "Project").

The ENGINEER has entered into the Agreement with CITY for the furnishing of professional services, and the parties now desire to amend the Agreement.

Therefore, it is mutually agreed that the Agreement is amended to include Additional Services to be performed by ENGINEER and provisions for additional compensation by the CITY to the ENGINEER, all as set forth in Exhibit A hereto. The parties ratify the terms and conditions of the Agreement not inconsistent with this Amendment, all of which are incorporated by reference.

CITY OF ROCKWALL

KIMLEY-HORN AND ASSOCIATES, INC.

By: _____

By: *L. Nathan Antle*

Title: _____

Title: Senior Vice President

Date: _____

Date: 4 - 9 - 2025

Project Understanding

The City has directed the Engineer to proceed with the design of upgrades of the storm sewer to satisfy the City's Design Criteria in accordance with the results of the preliminary drainage study *Hydrologic and Hydraulic Analysis – N. Lakeshore Drive (SH 66 to Masters Boulevard)* dated January 24, 2025.

This consists of the following:

1. Corridor Drainage Upgrades:
 - o Upsize and locate proposed storm sewer main line in the southbound travel lanes to accommodate a proposed superelevated roadway section
 - o Update inlet locations to accommodate a proposed superelevated roadway section
2. Off-Site Drainage Upgrades:
 - o Design of approximately 2,900 LF of storm sewer:
 - Crestwood Drive (1,400 LF)
 - Montclair Street (1,500 LF)

ENGINEER shall perform the following Additional Services:

Basic Services

2.2.2.7. Corridor Drainage Upgrade

- Perform services in accordance with Task 2.2.2.2. for the components listed above in Item 1 – Corridor Drainage Upgrades.

2.2.2.8. Off-Site Drainage Upgrade

- Perform services in accordance with Task 2.2.2.2. for the components listed above in Item 2 – Off-Site Drainage Upgrades.

Special Services

4.3. Topographic and Design Survey (Off-Site Drainage Upgrades)

- Perform services in accordance with Task 4.2. for the following additional limits:
 - Crestwood Drive from N. Lakeshore Drive to Bayshore Drive, approximately 700 linear feet
 - Bayshore Drive from the alley East of Crestwood Drive to Highcrest Lane, approximately 500 linear feet
 - Montclair Street from N. Lakeshore Drive to Sunset Hill Drive, approximately 750 linear feet
 - Sunset Hill Drive from Montclair Street to the alley North of N. Hills Drive, approximately 500 linear feet.

5.3 Subsurface Utility Engineering (S.U.E.) Services (Off-Site Drainage Upgrades)

- Perform services in accordance with Task 5.1. for the additional project limits identified in Task 4.3.

Additional Services

Services not specifically identified in the Scope of Services above shall be considered additional and shall be performed on an individual basis upon authorization by the City of Rockwall.

Schedule:

Kimley-Horn will provide our services as expeditiously as practicable with the goal of meeting a mutually agreed upon schedule.

Method of Compensation:

It is the understanding of the ENGINEER at the direction of the CITY that the Contract Engineering Services Contingency will be utilized to compensate for the additional services outlined herein.

For the Additional Services set forth above, CITY shall pay ENGINEER the following additional compensation:

Basic Services:

| | |
|------------------------------------|------------------|
| 2.2.2.7 Corridor Drainage Upgrades | \$112,700 |
| 2.2.2.8 Off-Site Drainage Upgrades | \$82,700 |
| Total Lump Sum Fee | \$195,400 |

Special Services:

| | |
|--|-----------------|
| 4.3 Topographic and Design Survey (Off-Site Drainage Upgrades) | \$38,900 |
| 5.3 Subsurface Utility Engineering (S.U.E.) Services (Off-Site Drainage Upgrades) | \$23,800 |
| Total Reimbursable Fee | \$62,700 |

ENGINEER will not exceed the total maximum Reimbursable fee shown without authorization from the CITY. Individual task amounts are provided for budgeting purposes only. ENGINEER reserves the right to reallocate amounts among tasks as necessary. We will not proceed with performance of services beyond the hours budgeted without written authorization by the CITY.



City of Rockwall
The New Horizon

MEMORANDUM

TO: Mayor & City Council Members

FROM: Mary Smith, City Manager

DATE: April 17, 2025

SUBJECT: Updated Information Regarding Solid Waste Collection

We've spent the past few weeks gathering information from surrounding and wide-area cities regarding their solid waste service collection and associated rates.

The survey is included for the Council's review. The first page of the survey shows cities on this side of the area and the second page are more scattered. Grouping them in this way helps to compare similar traffic issues and landfill proximities.

We also asked Republic to determine the volumes of waste picked up each collection day. The information is shown below for the last several weeks. The volumes on Thursday and Friday include lawn/leaf bags and other small bulky items so those volumes include more than just household waste.

| | Monday | Tuesday | Thursday | Friday |
|------------------|---------------|----------------|-----------------|---------------|
| 2/10/2025 | 103 | 81 | 38 | 51 |
| 2/17/2025 | 101 | 94 | 43 | 42 |
| 2/24/2025 | 118 | 88 | 44 | 57 |
| 3/3/2025 | 121 | 105 | 57 | 59 |
| 3/10/2025 | 115 | 99 | 63 | 65 |
| 3/17/2025 | 116 | 105 | 59 | 51 |
| 3/24/2025 | 147 | 121 | 65 | 65 |
| 3/31/2025 | 133 | 113 | 61 | 61 |
| Total | 954 | 806 | 430 | 451 |
| | 69% | 64% | 31% | 36% |

We need to continue the discussion of the collection model and rates with this data. Rick Bernas with Republic will be here to continue to answer questions.

| Plan | Provider | Trash | Recycling | Bulk Freq. | Bulk Yds. | Brush Bundling | Carts | Rate | Extra Cart | Notes |
|-------------------|-------------------|------------|------------|--------------|-----------------------------------|----------------|----------|----------|------------|--|
| Rockwall Current | Republic | 2 x Weekly | 1 x Weekly | 2 x Monthly | Unlimited | No | Any | \$ 21.38 | \$ 4.40 | Maintained 3% CPI increase for past 5 per contract. |
| Rockwall Proposed | Republic | 1 x Weekly | 1 x Weekly | 2 x Monthly | 6 | No | Provided | \$ 25.00 | \$ 5.00 | |
| Forney | Community | 1 x Weekly | Bi-weekly | 2 x Monthly | 4 | Yes | Provided | \$ 14.06 | | 4 cubic yards weekly; brush must be bundled ≤4ft lengths 175lb weight limit; \$9.02+/month extra carts Free monthly landfill access (10 cubic yards); \$1.50 senior discount |
| Wylie | Community | 1 x Weekly | 1 x Weekly | Must Request | 1 | Yes | | \$ 16.76 | | Bulk bundled items must be less than 50 lbs. Not all costs passed to customer |
| Rowlett | Republic | 1 x Weekly | 1 x Weekly | 1 x Monthly | 8 | No | Provided | \$ 20.36 | \$ 10.18 | Council considering twice per month 6 yards at \$26.52 |
| Royse City | Waste Connections | 1 x Weekly | 1 x Weekly | 2 x Monthly | 1 | Yes | Provided | \$ 17.56 | | One brush bundle will be picked up per collection day but must be cut into lengths no larger than 4' long, 6" around, tied in a bundle and not to exceed 50lbs. Place near the curb/alley on your collection day |
| Heath | Waste Connections | 1 x Weekly | 1 x Weekly | Weekly | 3 | Yes | Provided | \$ 24.45 | \$ 12.05 | excess bulk at 12.26 per yard |
| Mclendon Chisholm | Community | 1 x Weekly | 1 x Weekly | Weekly | 1 | Yes | | \$ 31.06 | | 1 yard loose or multi yards tied and bundled |
| Fate | Waste Connections | 1 x Weekly | 1 x Weekly | Weekly | 1 | Yes | Provided | \$ 20.56 | \$ 11.20 | one bulk item per trash day but must call to schedule |
| Lavon | Community | 1 x Weekly | 1 x Weekly | Weekly | 2 | Yes | Provided | \$ 24.25 | \$ 9.00 | |
| Sachse | Community | 1 x Weekly | 1 x Weekly | 2 x Monthly | 6 | Yes | Provided | \$ 20.16 | \$ 10.20 | Bulk must be bundled, must be less than 40 lbs, \$10.20 add waste cart and \$4.55 recycle |
| Balch Springs | Frontier | 2 x Weekly | 1 x Weekly | Weekly | Frontier site states not eligible | Yes | Provided | \$ 24.41 | \$ 14.40 | disaster clean up fee \$288.88 per yard. Cut to 4' length and bundled less than 50 lbs/bundle on collection days |

| Plan | Provider | Trash | Recycling | Bulk Freq. | Bulk Yds. | Brush Bundling | Carts | Rate | Extra Cart | Notes |
|--------------|-------------------|------------|------------|------------------------------------|-----------|----------------|----------|----------|------------|---|
| McKinney | Frontier | 1 x Weekly | 1 x Weekly | 12x per year - schedule collection | 4 | Yes | Provided | \$ 19.75 | \$ 7.58 | Limbs must be cut in 4-foot lengths and individual branch diameter cannot exceed 6 inches. Limbs must be tied with string or twine in bundles of 50 pounds or less. Limit of 10 bags or 10 bundles, or a combination not to exceed 10 items total. Access to landfill for residents |
| Garland | City Crew | 1 x Weekly | 1 x Weekly | Weekly | | | | \$ 24.08 | \$ 10.10 | |
| Little Elm | Community | 1 x Weekly | 1 x Weekly | 2 x Monthly | 1 | Yes | | \$ 18.93 | | |
| Plano | Env. Waste Svc. | 1 x Weekly | 1 x Weekly | Must Request | 1 | Yes | Provided | \$ 24.60 | \$ 20.50 | All grass clippings and leaves have to go into brown paper biodegradable bags. |
| Granbury | Waste Connections | 1 x Weekly | 1 x Weekly | 1 x Monthly | | Yes | | | | Customer must provide cart |
| Grapevine | Republic | 1 x Weekly | 1 x Weekly | Must Request | | | Provided | \$ 18.29 | | Bulk pickup available by request; yard waste removal service Standard carts provided (size unspecified) Electronics/battery recycling offered |
| Trophy Club | Community | 2 x Weekly | 1 x Weekly | Weekly | 4 | Yes | Any | \$ 29.12 | | may get a smaller recycle cart for 25.71 per month. annual CPI adjustment. Switching 1 July. |
| Lewisville | Republic | 1 x Weekly | Bi-weekly | 2 x Monthly | 3 | | | \$ 15.15 | | 3 cubic yards max (bulky + yard waste combined) 96-gal trash/recycling carts provided 20% senior discount; monthly HHW collection |
| DeSoto | Republic | 2 x Weekly | 1 x Weekly | 2 x Monthly | 3 | Yes | Provided | \$ 31.47 | | 3 cubic yards small items or 1 large item weekly; brush requires scheduling New bins issued June 2024; \$11.21/roll 50 for approved bags East/West pickup divisions based on Hampton Road |
| Mansfield | Frontier | 2 x Weekly | 1 x Weekly | 2 x Monthly | 1 | Yes | Any | \$ 18.49 | \$ 13.92 | |
| Flower Mound | Republic | 1 x Weekly | 1 x Weekly | 2 x Monthly | 4 | Yes | Provided | \$ 15.64 | \$ 9.02 | |



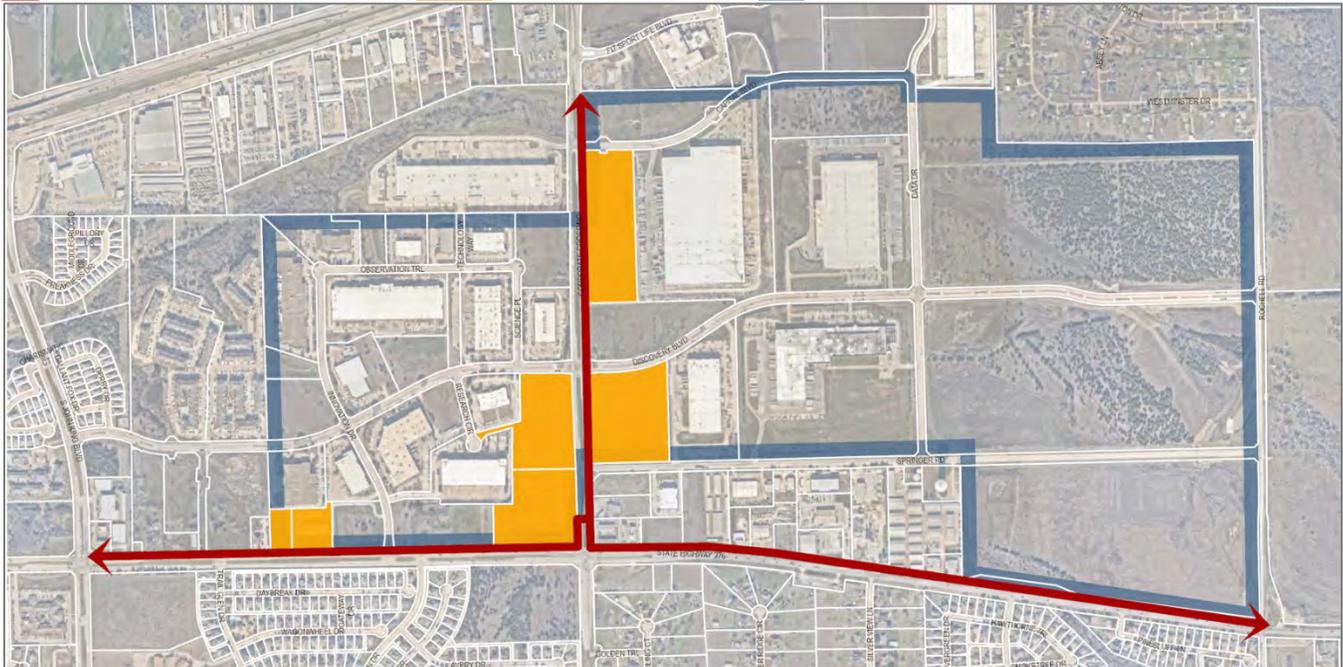
CITY OF ROCKWALL
CITY COUNCIL MEMORANDUM

PLANNING AND ZONING DEPARTMENT
385 S. GOLIAD STREET • ROCKWALL, TX 75087
PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
CC: Mary Smith, *City Manager*
 Joey Boyd, *Assistant City Manager*
FROM: Ryan Miller, *Director of Planning and Zoning*
DATE: April 21, 2025
SUBJECT: MIS2025-004; *Variance to the Utility Standards along Corporate Crossing and SH-276*

On July 5, 2022, the City Council approved a request (*i.e. Case No. MIS2022-011*) by the Rockwall Economic Development Corporation (REDC) to allow the overhead powerlines along Corporate Crossing and SH-276 to remain in place until July 5, 2027, at which time the REDC planned to underground the lines (*see Figure 1*). According to a letter submitted by the REDC, this extension was necessary due to "... four (4) projects and two (2) regional detention ponds in the development pipeline that ... [would] be impacted by these existing OH [overhead] utilities [*i.e. the powerlines located adjacent to Corporate Crossing and SH-276*]." Since this approval, the REDC has completed one (1) of the two (2) regional detention ponds, with the second currently in the engineering phase. Additionally, one (1) of the four (4) development projects -- *Integrated Defense Products (IDP)* -- has been completed, while another project -- *Chewters Chocolates* -- is currently under construction.

FIGURE 1. AREA APPROVED FOR TEMPORARY OVERHEAD POWERLINES
RED = EXISTING ABOVE GROUND POWERLINES; **ORANGE** = CURRENT REDC PROJECTS; **BLUE** = ROCKWALL TECHNOLOGY PARK BOUNDARIES



On February 14, 2025, the applicant -- *Phil Wagner of the REDC* -- submitted a development application and letter requesting a variance to utility placement requirements to allow the overhead powerlines -- *depicted in Figure 1* -- to be allowed to remain in place without the requirement to bury these lines in the future. The requirements stipulating that overhead powerlines be relocated at the time of development are outlined in both the Unified Development Code (UDC) and Municipal Code of Ordinances. These requirements are summarized as follows:

- Subsection 06.02, General Overlay District Standards, of Article 05, District Development Standards, of the UDC:

H. Utility Placement. All overhead utilities within any overlay district shall be placed underground.

NOTE: The subject property is located within the SH-205 Overlay (SH-205 OV) District.

- Section 03.03, Utility Distribution Lines, of Article 04, Permissible Uses, of the UDC:

All utility distribution lines shall be placed underground. Utility distribution lines placed above-ground shall require special approval of the City Council based upon a recommendation of the Planning and Zoning Commission.

- Section 38-15, Miscellaneous Requirements, of Chapter 38, Subdivisions, of the Municipal Code of Ordinances:

(e) Underground Utilities. All power and telephone service shall be underground. No overhead service will be allowed without special permission being given by the city council.

According to the applicant's letter "... it is anticipated that the project will cost the REDC approximately \$9M, not counting added design costs, nor the \$500,000.00 already spent by the REDC [for the utility costs associated with IDP]." The applicant's letter goes on to explain that the REDC was established to provide a dedicated funding source to attract businesses that could help broaden the City's tax base for the purpose of offsetting the residential tax base. In the applicant's estimation, the costs of this project could be better utilized for other projects that would have a greater benefit to the community (see applicant's letter in the attached packet).

According to Section 09.02, *Variances to the General Overlay District Standards*, of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC), "...an applicant may request the Planning and Zoning Commission grant a variance to any provision contained in Section 06.02, *General Overlay District Standards*, of Article 05, *District Development Standards*, where unique or extraordinary conditions exist or where strict adherence to the technical requirements of this section would create an undue hardship." In addition, the Unified Development Code (UDC) also tasks the City Council with approving a variance to *Undergrounding Utility Distribution Lines*. When considering this request, staff should point that [1] the request does appear to have benefits to all property owners along these roadways and [2] the cost of the proposed burial project does not appear to be reasonable considering *public funds* would be required to be used to complete the project. Based on this, it does appear that the REDC's request could be considered an undue hardship; however, variances to the underground utility requirements are discretionary decisions that are considered on a *case-by-case* basis by the City Council pending a recommendation from the Planning and Zoning Commission. On February 25, 2025, the Planning and Zoning Commission approved a motion to recommend denial of the applicant's request by a vote of 5-0, with Commissioners Deckard and Thompson absent. Should the City Council have any questions, staff and a representative of the REDC will be available at the April 21, 2025 City Council meeting.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY

PLANNING & ZONING CASE NO. _____

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING: _____

CITY ENGINEER: _____

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE)¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE)¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE)¹
- REPLAT (\$300.00 + \$20.00 ACRE)¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE)¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE)¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE)^{1 & 2}
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE)¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00)²

NOTES:

¹: IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.
²: A **\$1,000.00** FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS **State Highway 276 and Corporate Crossing**

SUBDIVISION **Rockwall Technology Park**

LOT _____

BLOCK _____

GENERAL LOCATION **North side of SH-276, East side of Corporate Crossing**

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING **Light Industrial**

CURRENT USE **Unimproved**

PROPOSED ZONING _____

PROPOSED USE **Light Industrial**

ACREAGE _____

LOTS [CURRENT] _____

LOTS [PROPOSED] _____

SITE PLANS AND PLATS: BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

OWNER **Rockwall Economic Development Corporation**

APPLICANT **same**

CONTACT PERSON **Phil Wagner**

CONTACT PERSON _____

ADDRESS **2610 Observation Trail, Suite 104**

ADDRESS _____

CITY, STATE & ZIP **Rockwall, TX 75032**

CITY, STATE & ZIP _____

PHONE **972-772-0025**

PHONE _____

E-MAIL **pwagner@rockwalledc.com**

E-MAIL _____

NOTARY VERIFICATION [REQUIRED]

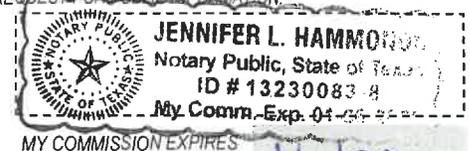
BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Phil Wagner - REDC [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

"I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 100.00 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE 14th DAY OF February 2025. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 14th DAY OF February 2025

OWNER'S SIGNATURE Phil Wagner

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS Jennifer L. Hammond



MY COMMISSION EXPIRES 11/6/28

MIS025-004: Variance Request for Overhead Utilities for REDC



Case Location Map = 

Esri Community Maps Contributors, City of Rockwall, GIS, Texas Parks & Wildlife, Esri, TomTom, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS, 1



City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.



Powerline Variance

The current estimated cost to bury the power lines on the Rockwall EDC property is \$9-10 million.

- *The REDC Board of Directors voted unanimously to not spend REDC (taxpayer funds) monies to bury the power lines. We believe the ROI is very low, if any, as it does not support our mission of developing non-residential tax base in the City of Rockwall.*

Value of \$9M to future REDC projects.

- Roughly equivalent to 97% of REDC annual revenue (\$9.3M budgeted)
- 5 times greater than last year's REDC operating expenditures (\$1.67M)
- Represents almost half of FY24 year end fund balance (\$18.7M)
- Equivalent to 17.4% of annual City operating expenditure budget (\$51.7M)

Significance of REDC funding to tax base / local economy

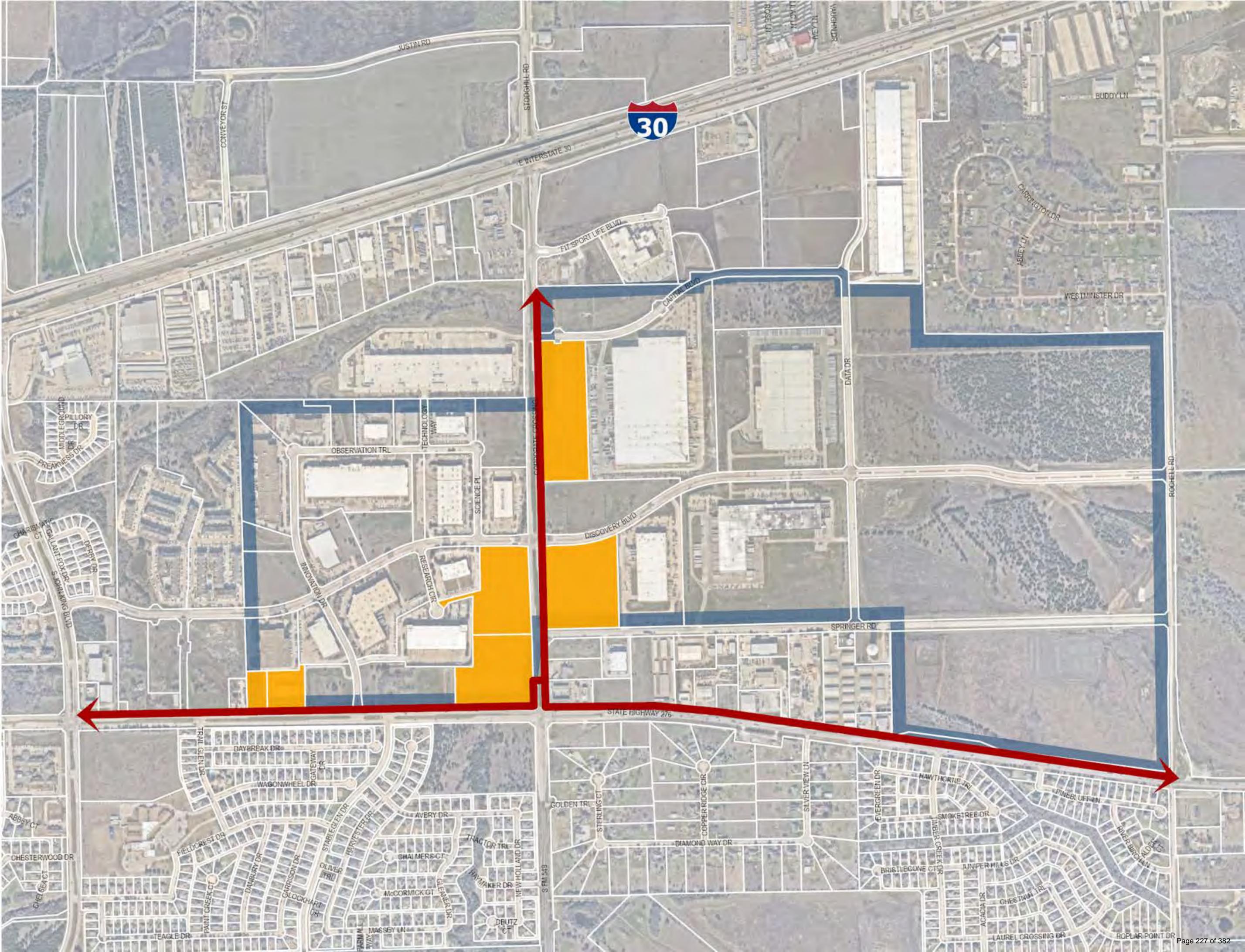
- Over the past 10 years, \$1M of REDC incentive funding on average generates \$15.4M in taxable activity and 17 high quality jobs
- \$9M of REDC funding would, on average, bring \$138M in taxable activity and 153 high quality jobs
- \$138M in taxable activity, expressed as taxable property, would generate just over \$2.1M in annual local tax revenue (\$340,000 directly to the City of Rockwall annually based upon the current tax rate)

Impact on Land Value adjacent to power lines to be buried.

- 40.5 acres of developable acreage impacted by the variance
- Value of land is \$8.35M according to RCAD
- With the encumbrance, the land has a negative value, and is undevelopable with standard market forces

Prevalence of Power Lines

- Unburied Power lines (of various types and sizes) can be seen throughout Rockwall, across every type of land use, on major thoroughfares and different ages of the the city's development.
 - Projects under construction currently
 - Newly built projects
 - City facilities, Other governmental facilities
 - Residential developments
 - Schools
 - Medical
 - Retail
 - Downtown
 - I-30
 - Ridge Road
 - Goliad / 205





CITY OF ROCKWALL
CITY COUNCIL MEMORANDUM

PLANNING AND ZONING DEPARTMENT
385 S. GOLIAD STREET • ROCKWALL, TX 75087
PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
CC: Mary Smith, *City Manager*
Joey Boyd, *Assistant City Manager*
FROM: Ryan Miller, *Director of Planning and Zoning*
DATE: July 5, 2022
SUBJECT: MIS2022-011; *Variance Request to the Utility Standards Along Corporate Crossing and SH-276*

On June 17, 2022, the applicant -- *Matt Wavering of the Rockwall Economic Development Corporation* -- submitted a development application requesting a variance to allow overhead powerlines to remain along Corporate Crossing and SH-276. According to the applicant's letter "(t)he REDC has four [4] projects and two [2] regional detention ponds in the development pipeline that will be impacted by these existing OH [*overhead*] utilities [*i.e. the powerlines located adjacent to Corporate Crossing and SH-276*]." Under the City's requirements each of these projects would be required to underground the powerlines at the time of development. This requirement is stipulated in several sections of the Unified Development Code (UDC) and Municipal Code of Ordinances. Specifically, the sections relevant to these projects are as follows:

- ☑ Subsection 06.02, *General Overlay District Standards*, of Article 05, *District Development Standards*, of the UDC:

H. Utility Placement. All overhead utilities within any overlay district shall be placed underground.

- ☑ Section 03.03, *Utility Distribution Lines*, of Article 04, *Permissible Uses*, of the UDC:

All utility distribution lines shall be placed underground. Utility distribution lines placed above-ground shall require special approval of the City Council based upon a recommendation of the Planning and Zoning Commission.

- ☑ Section 38-15, *Miscellaneous Requirements*, of Chapter 38, *Subdivisions*, of the Municipal Code of Ordinances:

(e) Underground utilities. All power and telephone service shall be underground. No overhead service will be allowed without special permission being given by the city council.

The applicant's letter goes on to state, that "(m)any utility companies are experiencing material and labor shortages which are driving up costs and extending project timelines ... [and] (r)ather than burdening each individual project with the costs and delays associated with the utility undergrounding, the REDC plans to coordinate a more efficient regional effort to take on the costs of undergrounding these utilities at one [1] time." In addition, the REDC plans to coordinate with third-party property owners along Corporate Crossing and SH-276 to allow these properties to participate in the project. This will significantly reduce the costs associated with undergrounding utilities on these properties. In order to achieve this goal, the REDC is requesting that a variance be granted allowing the overhead powerlines to remain in place for a period of five (5) years from the approval of the variance (*i.e. July 5, 2027*). This will allow the REDC time to coordinate this project with ONCOR and third-party property owners, and to allow supply chains and labor shortages to resolve.

According to Section 09.02, *Variances to the General Overlay District Standards*, of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC), "...an applicant may request the Planning and Zoning Commission grant a variance to any provision contained in Section 06.02, *General Overlay District Standards*, of Article 05, *District Development Standards*, where unique or extraordinary conditions exist or where strict adherence to the technical requirements of this section would create an undue hardship." In addition, the Unified Development Code (UDC) also tasks the City Council with approving a variance to *Undergrounding Utility Distribution Lines* pending a recommendation from the Planning and Zoning Commission. On June 28, 2022, the Planning and Zoning Commission approved a motion to recommend approval of the applicant's request by a vote of 7-0. Staff should point out that this appears to have benefits to all

property owners along these roadways; however, variances to the unground utility requirements are discretionary decisions that are considered on a *case-by-case basis* by the City Council.

In the attached packet staff has included a map showing the project scope and the applicant's letter. Should the City Council have any questions, staff and a representative for the applicant will be available at the July 5, 2022 City Council meeting.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY

PLANNING & ZONING CASE NO. _____

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING: _____

CITY ENGINEER: _____

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE) ¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE) ¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE) ¹
- REPLAT (\$300.00 + \$20.00 ACRE) ¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE) ¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE) ¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE) ^{1 & 2}
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE) ¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00) ²

NOTES:

¹ IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.
² A \$1,000.00 FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS State Highway 276 and Corporate Crossing

SUBDIVISION Rockwall Technology Park

LOT _____

BLOCK _____

GENERAL LOCATION North side of SH-276, East side of Corporate Crossing

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING Light Industrial

CURRENT USE Unimproved

PROPOSED ZONING _____

PROPOSED USE Light Industrial

ACREAGE _____

LOTS [CURRENT] _____

LOTS [PROPOSED] _____

- SITE PLANS AND PLATS:** BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

OWNER Rockwall Economic Development Corporation APPLICANT same

CONTACT PERSON Matt Wavering

CONTACT PERSON _____

ADDRESS 2610 Observation Trl, Suite 104

ADDRESS _____

CITY, STATE & ZIP Rockwall, TX 75032

CITY, STATE & ZIP _____

PHONE 972-772-0025

PHONE _____

E-MAIL mwavering@rockwalledc.com

E-MAIL _____

NOTARY VERIFICATION [REQUIRED]

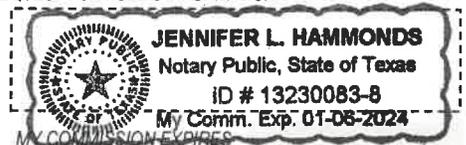
BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Matt Wavering [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

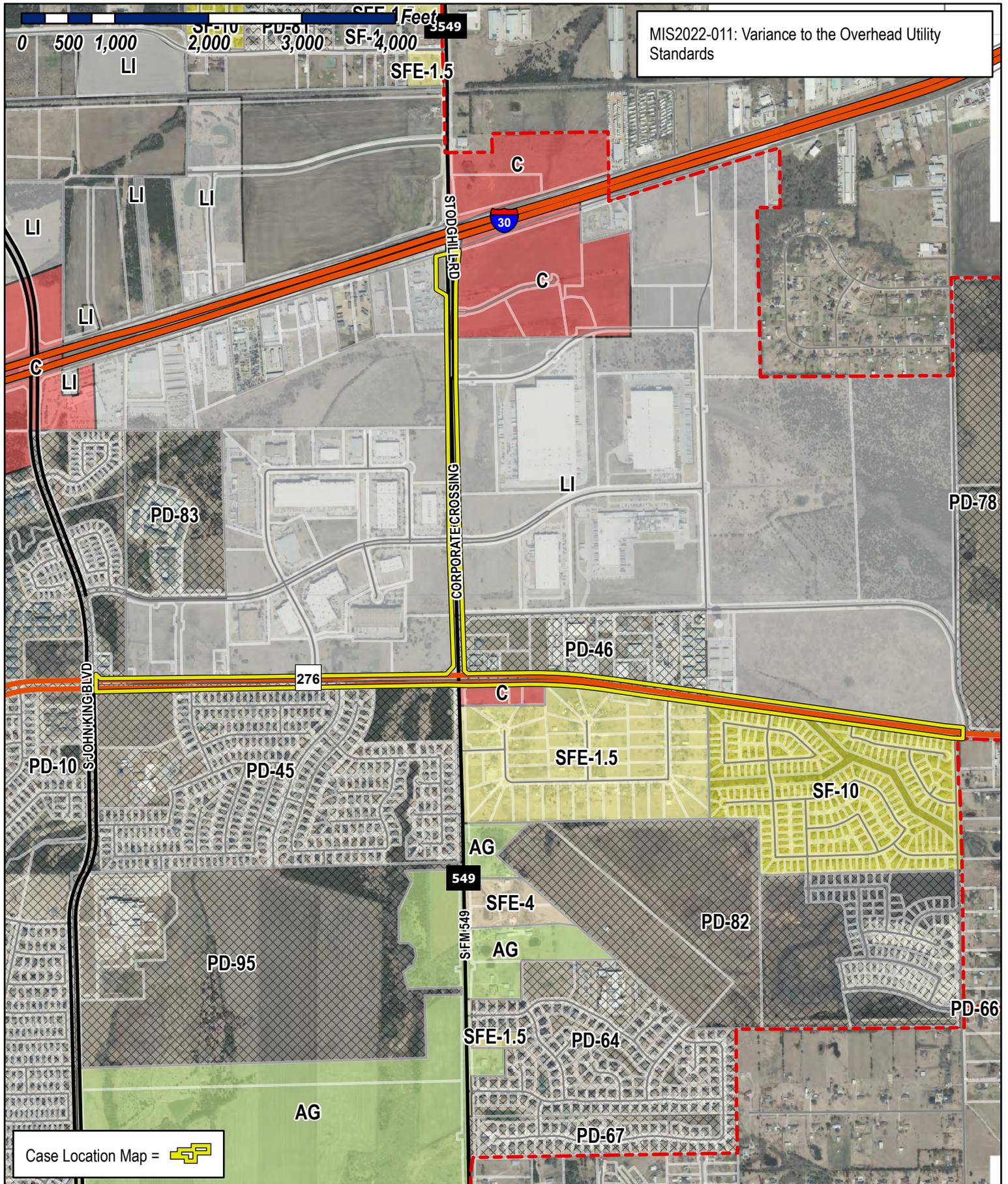
"I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ _____ TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE _____ DAY OF _____, 20____. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 14th DAY OF June, 2022

OWNER'S SIGNATURE [Signature]

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS [Signature]





MIS2022-011: Variance to the Overhead Utility Standards

Case Location Map = 



City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75032
 (P): (972) 771-7745
 (W): www.rockwall.com

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June 17, 2022

Mr. Ryan Miller
Planning Director
City of Rockwall
385 S. Goliad
Rockwall, TX 75087

Re: Overhead Utility Variance – Rockwall Technology Park

Mr. Miller:

The Rockwall Economic Development Corporation (REDC) is the developer of the Rockwall Technology Park, and owns eight light industrial lots impacted by existing overhead (OH) utility lines on Corporate Crossing and State Highway 276. The attached aerial map shows the OH utilities in red and the impacted lots owned by the REDC in gold. The utility poles are owned by Oncor Electric Delivery and support major feeders for the company. Additional franchise utility providers also lease space on Oncor's utility poles.

The REDC has four projects and two regional detention ponds in the development pipeline that will be impacted by these existing OH utilities. Each project is required to contract with the franchise utility companies and pay for the cost to underground those utility lines at the time of lot development, per City of Rockwall ordinance. Many utility companies are experiencing material and labor shortages which are driving up costs and extending project timelines. Rather than burdening each individual project with the costs and delays associated with utility undergrounding, the REDC plans to coordinate a more efficient regional effort to take on the costs of undergrounding these utilities at one time.

The REDC requests a variance to the City of Rockwall's ordinance to allow the REDC the time necessary to coordinate with the utility companies. Specifically, the variance request is for a period of five years. The REDC plans to underground all OH utilities impacting lots in the Rockwall Technology Park, including the undeveloped lots and the lots currently scheduled for development. Additionally, the REDC will cooperate with third party property owners with adjacent land who wish to benefit from the cost-savings associated with a single utility relocation project. The extended time frame will allow the REDC time to coordinate the regional project and complete the undergrounding in the most cost-effective manner.

We respectfully ask that the Planning & Zoning Commission and the Rockwall City Council consider approval of our variance request.

Sincerely,

A handwritten signature in black ink, appearing to read 'Matt Wavering', with a stylized, flowing script.

Matt Wavering
Director of Project Development

attachment



Corporate Crossing

Project Diamond

Project Cocoa Charlie

Project Bullseye

Regional Detention
Regional Cowboy Camp

Regional Detention

State Highway 276



CITY OF ROCKWALL

CITY COUNCIL CASE MEMO

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
DATE: April 21, 2025
APPLICANT: Tyler Adams; *Greenlight Studio*
CASE NUMBER: Z2025-011; *PD Development Plan for Rockwall Townhomes*

SUMMARY

Hold a public hearing to discuss and consider a request by Tyler Adams of Greenlight Studio on behalf of Matt Zahm of ZAPA Investments, LLC for the approval of a *PD Development Plan* for eight (8) *Townhomes* on a 0.87-acre parcel of land identified as Lot 3, Block A, Harbor Hills Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 32 (PD-32), situated within the *Interior Subdistrict* and the *Residential Subdistrict*, generally located on the northeast side of Glen Hill Way, northwest of the intersection of Glen Hill Way and Ridge Road [FM-740], and take any action necessary.

BACKGROUND

The City Council annexed the subject property into the City of Rockwall on January 9, 1984 through the adoption of *Ordinance No. 84-05*. Upon annexation, the subject property was zoned Agricultural (AG) District. On June 19, 1989, the subject property was rezoned to Planned Development District 32 (PD-32) [*Ordinance No. 02-55*] for Commercial (C) District land uses. On December 2, 2002, the City Council approved an amendment to Planned Development District 32 (PD-32) [*Ordinance No. 02-55*] that changed the underlying zoning to the General Retail (GR) District. This Planned Development District was again amended on February 4, 2008 by *Ordinance No. 08-11*. This ordinance increased the boundaries of the district, and established a limited set of land uses for the district.

On September 20, 2010, the City Council passed *Ordinance No. 10-21*, which superseded all previous ordinances associated with Planned Development District 32 (PD-32) and established a concept plan and development standards for an approximate 78.89-acre tract of land that included the subject property. Today, this land is now commonly referred to as *PD-32* or the *Harbor District*. Included within this ordinance was a concept plan that divided the district into ten (10) subdistrict, each of which contained its own set of development and land use standards. In addition, a pool of 1,161 *urban residential units* (i.e. *condominiums and/or townhomes*) and 49 *single-family residential units* (i.e. *zero-lot-line or patio homes*) was created. These units could then be allocated to properties within the district by the City Council -- in accordance with the land use charts in the ordinance -- on a *first-come-first-serve* basis through an interim zoning step called a *PD Development Plan*. According to Article 10, *Planned Development District Regulations, of the Unified Development Code (UDC)*, "(a) *PD Development Plan* constitutes an amendment to the approved *PD Concept Plan* and *PD Ordinance* ... The purposes of a *PD Development Plan* are to allow flexibility in the development process by deferring specification of all development standards at the time of *PD District* creation and to enable developers to satisfy conditions imposed on creation of the *District* prior to the submittal of a *PD Site Plan*." In addition, *Ordinance No. 17-22* [i.e. *the regulating ordinance for Planned Development District 32 (PD-32)*] states that the purpose of a *PD Development Plan* in Planned Development District 32 (PD-32) is to ensure that a proposed development meets the intent of the subdistrict and/or to address any waivers required by the development.

Currently, the City Council has approved *PD Development Plans* allocating all of the 1,161 *urban residential units* and all but seven (7) of the 49 *single-family residential units*. A summary of the unit allocations is as follows:

TABLE 1: URBAN RESIDENTIAL UNITS

| <i>Project Name</i> | <i>Units</i> | <i>Unit Type</i> | <i>Case No.</i> | <i>Ordinance No.</i> |
|---|--------------|------------------|-------------------------|----------------------|
| Summer Lee Condominiums (i.e. Harbor Hill) ¹ | 265 | Condominiums | SP2015-004 ² | N/A |
| Harbor Urban Condominiums (i.e. the Florence) | 228 | Condominiums | Z2018-001 | 18-16 ⁴ |
| Harbor Village Building 1 (i.e. Adjacent to Lakefront Trail) | 375 | Condominiums | Z2019-008 | 19-21 ⁵ |
| Harbor Village Building 2 (i.e. Adjacent to Harbor Fountain) ³ | 75 | Condominiums | Z2019-008 | 19-21 ⁵ |
| Harbor Residences (i.e. WC of Summer Lee and Horizon Road) | 176 | Condominiums | Z2022-058 | 23-05 ⁶ |
| Sunset Ridge Residences (i.e. Adjacent to Harbor Residences) | 42 | Condominiums | Z2022-002 | 22-11 |
| <i>Total Allocated:</i> | <u>1,161</u> | | | |
| <i>Permitted in District:</i> | <u>1,161</u> | | | |
| <i>Units Remaining:</i> | <u>0</u> | | | |

NOTES:

- ¹: This case was determined to be conforming to the *Concept Plan* originally approved with Ordinance No. 10-21, and was determined to not require a PD Development Plan at that time (i.e. the approval of the site plan allocated the units to this project).
- ²: Case No. SP2015-004 was amended by Case No. SP2018-037 & SP2020-018.
- ³: Ordinance No. 19-21 contains conditions that allow the City Council to claw back 25 of the 75 allocated units pending a building permit has not been issued by June 1, 2020. This project currently has not been issued a building permit.
- ⁴: This ordinance superseded Ordinance No. 16-54 (Case No. Z2016-025), which originally allocated the units for this project.
- ⁵: This ordinance superseded Ordinance No. 13-16 (Case No. Z2013-011); however, the units for this project were originally allocated with Ordinance No. 11-43 (i.e. Case No. Z2011-020).
- ⁶: This ordinance superseded Ordinance No. 22-10 (Case No. Z2022-001) and Ordinance No. 22-36 (Case No. Z2022-024).

TABLE 2: SINGLE-FAMILY RESIDENTIAL UNITS

| <i>Project Name</i> | <i>Units</i> | <i>Unit Type</i> | <i>Case No.</i> | <i>Ordinance No.</i> |
|-------------------------------------|--------------|------------------|-----------------|----------------------|
| Harbor Urban Townhomes ¹ | 42 | Townhomes | Z2018-001 | 18-16 ² |
| <i>Total Allocated:</i> | <u>42</u> | | | |
| <i>Permitted in District:</i> | <u>49</u> | | | |
| <i>Units Remaining:</i> | <u>7</u> | | | |

NOTES:

- ¹: This PD Development Plan only affected the Residential Subdistrict, which was the only district that allowed the single-family residential units. The approval of Ordinance No. 18-16 amended the district to allow the 42 townhomes in lieu of the 49 zero-lot-line or patio homes.
- ²: This ordinance superseded Ordinance No. 16-48 (Case No. Z2016-026), which originally allocated the units for this project.

On October 19, 2020, the City Council approved a final plat [Case No. P2020-036] establishing the subject property as Lot 3, Block A, Harbor Hills Addition. On January 17, 2025, the applicant submitted a PD Development Plan request; however, due to the applicant's failure to attend the Planning and Zoning Commission work session meeting, the applicant requested to withdraw the case. On February 11, 2025, the Planning and Zoning Commission approved the withdrawal request by a vote of 7-0. On February 14, 2025, the applicant submit a subsequent application, which was tabled on March 11, 2025, in order to allow the applicant more time to address concerns from the Planning and Zoning Commission.

PURPOSE

On February 14, 2025, the applicant -- Tyler Adams of Greenlight Studio -- submitted an application requesting the approval of a PD Development Plan to allow the development of seven (7) townhomes on the subject property.

ADJACENT LAND USES AND ACCESS

The subject property is generally located northwest of the intersection of Ridge Road and Glen Hill Way. The land uses adjacent to the subject property are as follows:

North: Directly north of the subject property is a vacant 4.076-acre parcel of land (i.e. Lot 4, Block A, Harbor Village Addition). Beyond this is a 2.12-acre parcel of land (i.e. Lot 5, Block 5, Harbor Village Addition) developed with a Limited Service Hotel (i.e. Tru Hilton). These properties are situated within the Residential and Interior Subdistricts of Planned Development District 32 (PD-32). Following this is Summer Lee Drive, which is identified as a Minor Collector on the City's Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan.

South: Directly south of the subject property is a 0.8320-acre parcel of land (i.e. Lot 1, Block A, Lakeridge Est Addition) developed with a multi-tenant retail building (i.e. Feng Cha, Athletico, & Hollywood Feed) located within the Ridge Road Retail Subdistrict of Planned Development District 32 (PD-32). Beyond this is Glen Hill Way, which is identified as a R2 (i.e. residential, two [2] lane, undivided roadway) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Following this are three (3) parcels of land (i.e. Lots 1-3, Block A, Willis-Sealock Addition) developed with retail buildings (i.e. Family Dentistry Rockwall & Ridge Road Animal Hospital) zoned General Retail (GR) District. South of this is a 1.46-acre parcel of land (i.e. Lot 1, Block A, AJ Squared Addition) developed with a multi-tenant Medical Office Building, zoned Planned Development District 18 (PD-18).

East: Directly east of the subject property are two (2) parcel of land (i.e. Lot 1, Block A, Murphy Plaza No. 2 Addition & Lot 1, Block A, Murphy Plaza) developed with multi-tenant Office Buildings, zoned General Retail (GR) District. Beyond this is Ridge Road [SH-740], which is identified as a A4D (i.e. arterial, four [4] lane, divided roadway) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Following this is a 2.0034-acre parcel of land (i.e. Lot 1, Block A, RDI Retail Addition) developed with a General Retail Building (i.e. Walgreens) zoned Planned Development District 9 (PD-9) for General Retail (GR) District land uses.

West: Directly west of the subject property is Glen Hill Way, which is identified as a R2 (i.e. residential, two [2] lane, undivided roadway) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Beyond this is a vacant 2.1690-acre parcel of land (i.e. Lot 2, Block A, Harbor Hills Addition) situated within the Interior Subdistrict of Planned Development District 32 (PD-32). Following this is a 1.25-acre tract of land (i.e. Tract 137 of the E. Teal Survey, Abstract No. 207) that serves as the Glen Hills Cemetery. West of this is the Lago Vista Subdivision, which contains 92 single-family homes and is zoned Planned Development District 18 (PD-18) for Single-Family 10 (SF-10) District land uses.

MAP 1: LOCATION MAP
YELLOW: SUBJECT PROPERTY



CHARACTERISTICS OF THE REQUEST

The applicant has submitted a concept plan showing the establishment of seven (7) townhomes northwest of the intersection of Glen Hill Way and Ridge Road [SH-740] across the street from the Glen Hills Cemetery. Based on the proposed concept plan, the townhomes are split into two (2) buildings, with four (4) townhomes facing directly onto Glen Hill Way and the remaining three (3) townhomes facing onto an internal mew street. In addition, there will be three (3) guest parking spaces, two (2) park benches, and two (2) dog waste stations. The townhomes incorporate elements of the Tuscan architectural style and utilize earth tones. The internal mew street will be constructed of concrete, have no sidewalks, and have pedestrian lighting.

CONFORMANCE WITH PLANNED DEVELOPMENT DISTRICT 32 (PD-32)

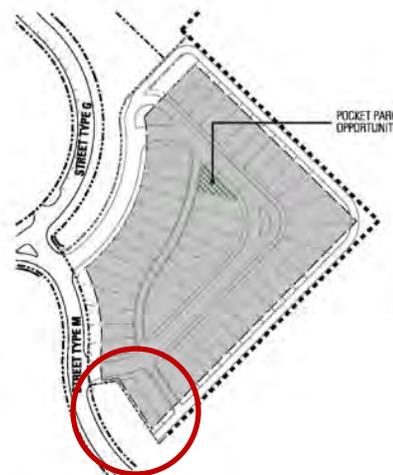
According to the *Subdistrict Plan* contained in Planned Development District 32 (PD-32) [Ordinance No. 17-22] the subject property is situated within the *Residential Subdistrict*, which -- according to Ordinance No. 17-22 -- is intended "...for zero lot line single-family residential housing." In this case, the applicant is proposing to change the zero lot homes into townhomes. This inherently changes the intent of the district, where the proposed townhomes do not meet the requirements stipulated for the *Residential Subdistrict*. Based on this, staff has requested that the applicant submit a *PD Development Plan* for the project. According to Ordinance No. 17-22, "(a) [PD] development plan shall be required if a proposed development within any *Subdistrict* that does not meet the intent of the *PD Concept Plan* or the *Subdistrict Plan*, or requires waivers not provided for in Section 9.3 [Waivers of Design Standards] of this ordinance." The following is a summary of the form-based code requirements for the *Residential Subdistrict*, and how the proposed project conforms to these requirements:

TABLE 1: BUILDING PLACEMENT AND PARKING COMPLIANCE THE RESIDENTIAL SUBDISTRICT

| ORDINANCE PROVISIONS | RESIDENTIAL SUBDISTRICT | CONFORMANCE TO THE STANDARDS |
|--------------------------------|---|--|
| BUILD TO LINE: | 10-Foot | -10-Feet; <i>IN CONFORMANCE</i> |
| BUILDING FORM: | Side Yard Setback 8-Feet and 0-Feet. | No Side Setbacks Between Townhomes; <i>NOT IN CONFORMANCE</i> |
| | 65% of the Façade must be at the Build to Line. | The Units Front Curved Streets and Do Not Meet This Requirement; <i>NOT IN CONFORMANCE</i> |
| | Minimum Lot Size is 30' x 90' | The are 35' Wide, but are as Short as 50-Feet; <i>NOT IN CONFORMANCE</i> |
| MAXIMUM LOT COVERAGE LAND USE: | 61% | <61%; <i>IN CONFORMANCE</i> |
| | GROUND FLOOR Residential | Residential; <i>IN CONFORMANCE</i> |
| | UPPER FLOORS Residential | Residential; <i>IN CONFORMANCE</i> |
| BUILDING HEIGHT: | | |
| MAXIMUM BUILDING HEIGHT | 3-Stories and 36-Feet | 3-Stories and 33-Feet; <i>IN CONFORMANCE</i> |
| ENCROACHMENTS | 5-Feet | No Encroachments; <i>IN CONFORMANCE</i> |
| GARAGE LOCATION | Garages Must be Rear Loaded | Three (3) Units are Front Loaded; <i>NOT IN CONFORMANCE</i> |

Staff has also provided the concept plan for the *Residential Subdistrict* in Figure 1. Staff reviewed the proposed building elevations, perspectives, and renderings for conformance to the *Design Guidelines* contained in Resolution 10-40 and found that the proposed elevations generally conform to the *Tuscan* architectural style. That being said, based on the proposed concept plan a mew street will run between the two (2) townhome buildings. The *Design Guidelines* detail that mew streets should include "...enhanced pedestrian paving, enhanced landscape planting, special accent lighting, and may also include enhanced vehicular pavement." In this case, there are no proposed sidewalks and there will be decorative lighting. The landscape plan also delineates plantings along the mew both in open space and townhome lots. Staff should note, per the Chapter 2, *Streets*, of the Engineering Standards of Design and

FIGURE 1: RESIDENTIAL SUBDISTRICT CONCEPT PLAN
RED CIRCLE = APPROXIMATE LOCATION OF SUBJECT PROPERTY



Construction, pavers or stamped concrete may not be utilized in utility easements.

In addition to the *Residential Subdistrict* requirements of Planned Development District 32 (PD-32) and the *Design Guidelines*, the applicant's request fails to meet the anti-monotony requirements of the Unified Development Code (UDC). According to Subsection 03.01(D), *General Residential District Standards*, of Article 05, *District Development Standards*, of the Unified Development Code (UDC), "(t)he front building elevations of a home shall not repeat along any block face without at least five (5) intervening homes of differing appearance on the same side of the street and six (6) intervening homes of differing appearance on the opposite side of the street ... (i)dentical building material blends and colors may not occur on adjacent (*side-by-side*) properties." This section continues with "(h)omes are considered to differ in appearance if any three (3) elements are different: (a) The number of stories of the home ... (b) The garage location/orientation on the home ... (c) The roof type and layout of the home ... [and] (d) The articulation of the front façade of the home." In this case, the proposed townhomes have the same number of stories, incorporate the same garage location/orientation, and utilize the same color and material blends. Given this, the proposed building elevations do not meet the anti-monotony standards.

Finally, the applicant is requesting to entitle the subject property with seven (7) townhome units. As shown in *Table 2 (above)*, there are seven (7) remaining single-family residential units (*i.e. zero-lot-line or patio homes*) within Planned Development District 32 (PD-32). The concept plan for PD-32 – *established with Ordinance No. 10-21* – allocated a pool of 1,161 *urban residential units (i.e. condominiums and/or townhomes)* and 49 *single-family residential units (i.e. zero-lot-line or patio homes)*. On September 19, 2016, the City Council approved a PD Development Plan [*Case No. Z2016-026; Ordinance No. 16-48*] that converted 36 of the *single-family residential units* to townhome units. Following this approval, the PD Development Plan was amended [*Case No. Z2018-001; Ordinance No. 18-16*] to include an additional six (6) townhome units, for a total of 42 townhomes. The remaining seven (7) single-family residential units have yet to be allocated to a project, and previous requests to increase the number of units in the Planned Development District 32 (PD-32) have not been approved. In this case, the applicant is requesting to convert the remaining single-family residential units (*i.e. zero-lot-line or patio homes*) into townhomes.

With all this being said, a *PD Development Plan* is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission.

INFRASTRUCTURE

Based on the applicant's concept plan and the proposed density, the following infrastructure is required to be constructed to provide adequate public services for the proposed development:

- (1) Roadways. Until the completion of an infrastructure study the level of roadway improvements cannot be determined. The proposed new street cannot utilize stamped concrete or pavers if the street will act as a utility easement as well. A *Streetscape Plan* showing how the sidewalks and pedestrian areas will be designed will be required to be submitted with the site plan. In addition, the applicant will be required to update the Traffic Impact Analysis (TIA) for the *Harbor District* per the City Council's direction from the November 5, 2018 City Council meeting.
- (2) Wastewater and Water. An infrastructure study will be required to determine the necessary improvements needed to sufficiently serve the subject property. Staff recommends that this infrastructure study be initiated prior to the submission of a site plan.
- (3) Drainage. The applicant shall be required to pay a stormwater pro-rata fee of \$7,226.59 per acre.

NOTIFICATIONS

On February 21, 2025, staff mailed 47 notices to property owners and occupants within 500-feet of the subject property. Staff also notified the Lago Vista Homeowner's Association (HOA), which was the only HOA within 1,500-feet of the subject property participating in the Neighborhood Notification Program. Additionally, staff posted a sign on the subject property, and advertised the public hearings in the Rockwall Herald Banner as required by the Unified Development Code (UDC). At the time this report was drafted, staff has received four (4) notices in favor and two (2) notices in opposition of the applicant's request.

CONDITIONS OF APPROVAL

If the City Council chooses to approve the applicant's request for a PD Development Plan for seven (7) townhomes, staff would propose the following conditions of approval:

- (1) The applicant shall be responsible for maintaining compliance with the concept plan and development standards contained in the PD Development Plan ordinance and which are detailed as follows:
 - (a) The development of the subject property shall generally conform to the *Concept Plan* depicted in *Exhibit 'B'* of the PD Development Plan ordinance; and,
 - (b) The development of the subject property shall generally conform to the proposed *Conceptual Building Elevations* depicted in *Exhibit 'C'* of this ordinance and to the design guidelines contained in *Resolution No. 10-40*; and,
 - (c) The proposed townhome development shall not contain more than seven (7) townhomes; and,
 - (d) All building materials and color schemes proposed for this development should conform to the requirements stipulated by Planned Development District 32 (PD-32) [*as amended*]; and,
 - (e) Prior to the issuance of a building permit the applicant shall submit and seek approval for a detailed PD Site Plan that demonstrates compliance with all applicable standards of Planned Development District 32 (PD-32) [*as amended*] and with the requirements approved in the PD Development Plan ordinance.
- (2) Any construction resulting from the approval of this Planned Development District shall conform to the requirements set forth by the Unified Development Code (UDC), the International Building Code (IBC), the Rockwall Municipal Code of Ordinances, city adopted engineering and fire codes and with all other applicable regulatory requirements administered and/or enforced by the state and federal government.

PLANNING AND ZONING COMMISSION

On April 15, 2025, the Planning and Zoning Commission approved a motion to recommend approval of the PD development plan by a vote of 5-1, with Commissioner Hagaman dissenting and Commissioner Thompson absent.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF ONLY

PLANNING & ZONING CASE NO. _____

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING: _____

CITY ENGINEER: _____

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE) ¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE) ¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE) ¹
- REPLAT (\$300.00 + \$20.00 ACRE) ¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE) ¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE) ¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE) ^{1 & 2}
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE) ¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00) ²

NOTES:

¹: IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.

²: A ~~\$1,000.00~~ FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS **Glen Hill Way**

SUBDIVISION **Harbor Hills Addition**

LOT **3** BLOCK **A**

GENERAL LOCATION **Parcel on Glen Hill Way north adjacent of 2930 South Ridge Road**

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING **PD-32 Residential Subdistrict**

CURRENT USE **Vacant**

PROPOSED ZONING **Unchanged**

PROPOSED USE **Single Family Attached**

ACREAGE **.87** LOTS [CURRENT] **1** LOTS [PROPOSED] **13**

SITE PLANS AND PLATS: BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

OWNER **ZAPA Investmants LLC**

APPLICANT **Greenlight Studio**

CONTACT PERSON **Matt Zahm**

CONTACT PERSON **Tyler Adams**

ADDRESS **201 W Kaufman St.**

ADDRESS **100 N. Cottonwood Drive
Suite 104**

CITY, STATE & ZIP **Richardson, Texas 75081**

CITY, STATE & ZIP **Richardson, Texas 75080**

PHONE **972.682.1435**

PHONE **214.810.4535**

E-MAIL **MZahm@zconstructors.com**

E-MAIL **tyler@gldevco.com**

NOTARY VERIFICATION [REQUIRED]

BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Matt D. Zahm [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

"I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 270.00 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE 13th DAY OF December 2024. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

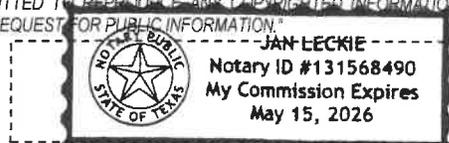
GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 13th DAY OF December 2024.

OWNER'S SIGNATURE

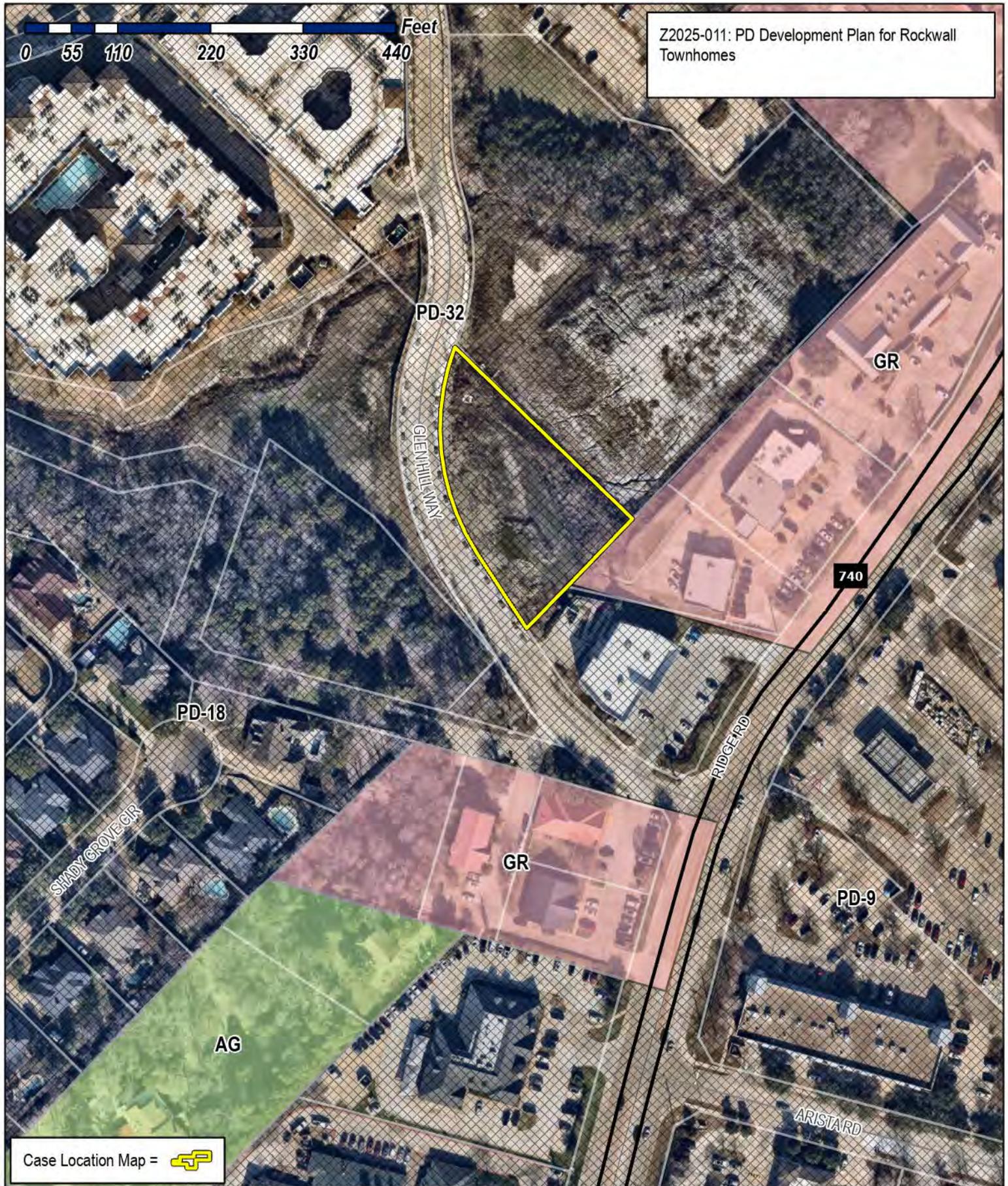
Matt D. Zahm

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS

Jan Leckie



MY COMMISSION EXPIRES 05/15/2026



Z2025-011: PD Development Plan for Rockwall Townhomes

Case Location Map = 



City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75087
 (P): (972) 771-7745
 (W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.

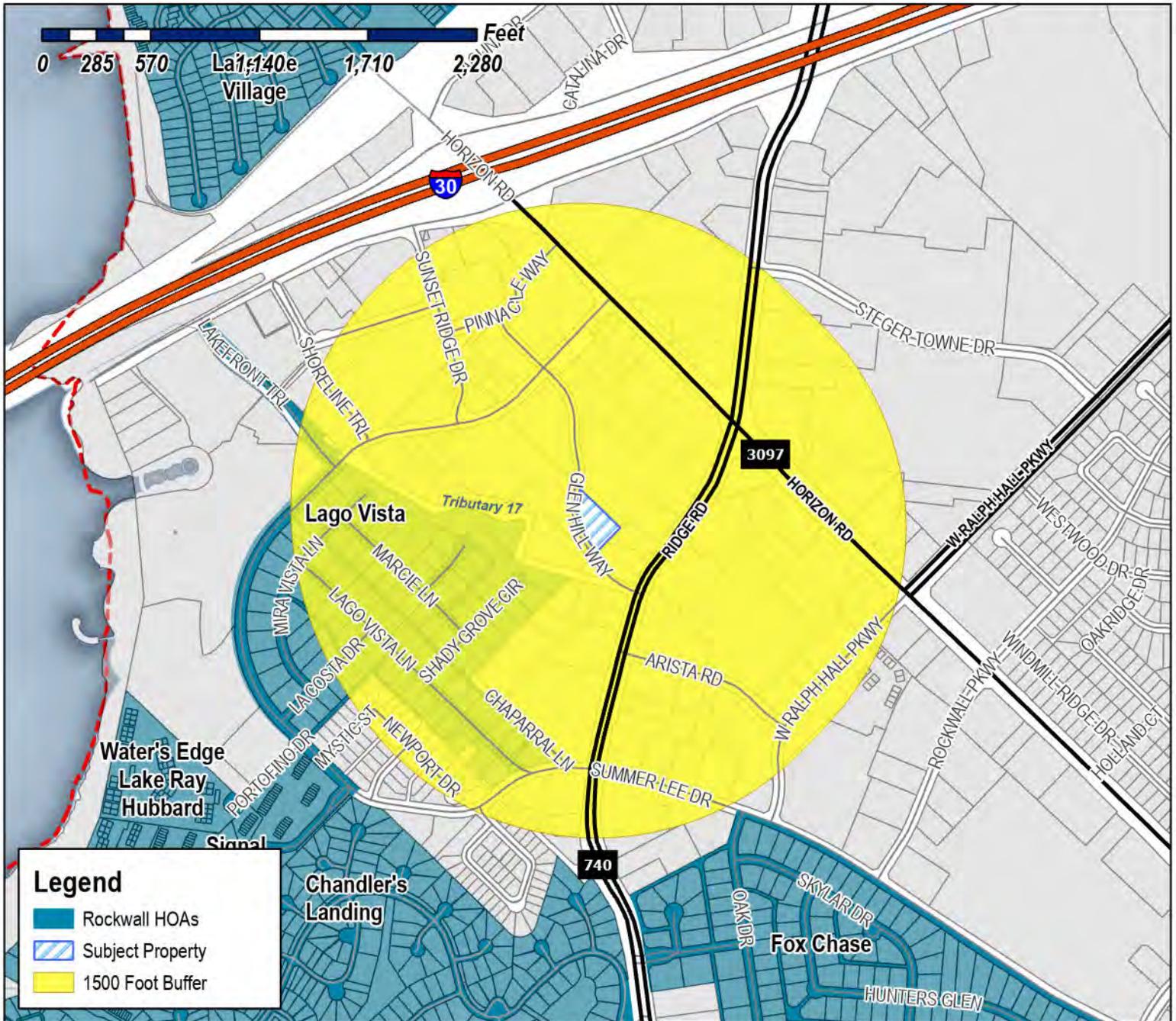




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Case Number: Z2025-011
Case Name: PD Development Plan for Rockwall Townhomes
Case Type: Zoning
Zoning: Planned Development District 32 (PD-32)
Case Address: Glen Hill Way

Date Saved: 2/20/2025
 For Questions on this Case Call (972) 771-7745



From: [Zavala, Melanie](#)
Cc: [Miller, Ryan](#); [Lee, Henry](#); [Ross, Bethany](#); [Guevara, Angelica](#)
Subject: Neighborhood Notification Program (Z2025-011)
Date: Thursday, February 20, 2025 3:31:43 PM
Attachments: [Public Notice \(02.18.2025\).pdf](#)
[HOA Map \(02.20.2025\).pdf](#)

HOA/Neighborhood Association Representative:

Per your participation in the [Neighborhood Notification Program](#), you are receiving this notice to inform your organization that a zoning case has been filed with the City of Rockwall that is located within 1,500-feet of the boundaries of your neighborhood. As the contact listed for your organization, you are encouraged to share this information with the residents of your subdivision. Please find the attached map detailing the property requesting to be rezoned in relation to your subdivision boundaries. Additionally, below is the summary of the zoning case that will be published in the Rockwall Herald Banner on [Friday, February 21, 2025](#). The Planning and Zoning Commission will hold a public hearing on [Tuesday, March 11, 2025 at 6:00 PM](#), and the City Council will hold a public hearing on [Monday, March 17, 2025 at 6:00 PM](#). Both hearings will take place at 6:00 PM at City Hall, 385 S. Goliad, Rockwall, TX 75087.

All interested parties are encouraged to submit public comments via email to Planning@rockwall.com at least 30 minutes in advance of the meeting. Please include your name, address, and the case number your comments are referring to. These comments will be read into the record during each of the public hearings. Additional information on all current development cases can be found on the City's website: <https://sites.google.com/site/rockwallplanning/development/development-cases>.

-
Z2025-011: PD Development Plan for (8) Eight Townhomes

Hold a public hearing to discuss and consider a request by Tyler Adams of Greenlight Studio on behalf of Matt Zahm of ZAPA Investments, LLC for the approval of a [PD Development Plan](#) for eight (8) Townhomes on a 0.87-acre parcel of land identified as Lot 3, Block A, Harbor Hills Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 32 (PD-32), situated within the Interior Subdistrict and the Residential Subdistrict, generally located on the northeast side of Glen Hill Way, northwest of the intersection of Glen Hill Way and Ridge Road [FM-740], and take any action necessary.

Thank you,

Melanie Zavala

Planning & Zoning Coordinator | Planning Dept. | City of Rockwall
385 S. Goliad Street | Rockwall, TX 75087
[Planning & Zoning Rockwall](#)
972-771-7745 Ext. 6568



City of Rockwall

Planning & Zoning Department
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Rockwall, Texas 75087
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Case Number: Z2025-011
Case Name: PD Development Plan for Rockwall Townhomes
Case Type: Zoning
Zoning: Planned Development District 32 (PD-32)
Case Address: Glen Hill Way

Date Saved: 1/21/2025

For Questions on this Case Call: (972) 771-7745



KROGER TEXAS LP
1014 VINE STREET
CINCINNATI, OH 45202

HPA BORROWER 2016-1 LLC
120 S RIVERSIDE PLZ SUITE 2000
CHICAGO, IL 60606

SONG CORPORATION
1200 HORIZON RD
ROCKWALL, TX 75032

RESIDENT
1220 HORIZON RD
ROCKWALL, TX 75032

JAY & MALISHA LLC
1260 HERON LAKES CIR
MOBILE, AL 36693

NATIONAL TRANSFER SERVICES, LLC
1360 POST OAK BLVD STE 100 # 16-2
HOUSTON, TX 77056

RESIDENT
1489 SHADY GROVE CIR
ROCKWALL, TX 75032

THOMPSON CHARLES C & SHARON K
1496 SHADY GROVE CIR
ROCKWALL, TX 75032

DRAKE CARLA DEE & DENNIS HARTLINE
1501 SHADY GROVE CIR
ROCKWALL, TX 75032

HUBERT ROBIN AND MONICA A
1508 SHADY GROVE CIR
ROCKWALL, TX 75032

RESIDENT
1513 SHADY GROVE CIR
ROCKWALL, TX 75032

HASSANIZADEH MAHMOUD & VASHTI
1523 PARKSIDE CIR
ROCKWALL, TX 75032

ROCKWALL HARBOR HILL, LTD
15653 HIGHWAY 243
KAUFMAN, TX 75142

CROCHERON AVENUE LLC
18 BEVERLY ROAD
DOUGLASTON, NY 11363

WHITNEY SARDIS LLC
2001 MARCUS AVENUE SUITE N118
LAKE SUCCESS, NY 11042

RESIDENT
2400 SUMMER LEE DR
ROCKWALL, TX 75032

RESIDENT
2500 SUMMER LEE DR
ROCKWALL, TX 75032

RESIDENT
2500 SUMMER LEE DR
ROCKWALL, TX 75032

SELMA HOSPITALITY INC
2560 ROYAL LN STE 210
DALLAS, TX 75229

SELMA HOSPITALITY INC
2560 ROYAL LN STE 210
DALLAS, TX 75229

RESIDENT
2700 SUMMER LEE
ROCKWALL, TX 75032

HARBOR LAKE POINTE INVESTORS LLC
2701 SUNSET RIDGE DR SUITE 607
ROCKWALL, TX 75032

HARBOR LAKE POINTE INVESTORS LLC
2701 SUNSET RIDGE DR SUITE 607
ROCKWALL, TX 75032

RESIDENT
2890 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2910 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2911 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2918 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2930 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2935 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
2951 RIDGE RD
ROCKWALL, TX 75032

SEALOCK MICHAEL C & SONIA KAY
3010 RIDGE RD
ROCKWALL, TX 75032

JBE RIDGE LLC
3012 RIDGE RD STE 204
ROCKWALL, TX 75032

JBE RIDGE LLC
3012 RIDGE RD STE 204
ROCKWALL, TX 75032

RESIDENT
3014 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
3016 RIDGE RD
ROCKWALL, TX 75032

RESIDENT
3018 RIDGE RD
ROCKWALL, TX 75032

HAYWOOD CHARLES E AND LISA K
36 IONA DRIVE RD3 TAURANGA
NEW ZEALAND 3173,

KRUGER KARIN
400 CHAPARRAL LN
ROCKWALL, TX 75032

C. REAL ESTATE, LLC
5 TERRABELLA LANE
HEATH, TX 75032

AJ SQUARED LLC
5 TERRABELLA LN
HEATH, TX 75032

ATTICUS SUMMER LEE TOWNHOMES LLC
5339 ALPHA ROAD SUITE 300
DALLAS, TX 75240

KROGER TEXAS LP
751 FREEPORT PKWY
COPPELL, TX 75019

HP ROCKWALL 740 LTD
C/O GARY P HAMMER
7557 RAMBLER ROAD, SUITE 980
DALLAS, TX 75231

TX FLORENCE APARTMENTS LLC
9757 NE JUANITA DRIVE SUITE 300
KIRKLAND, WA 98034

GLEN HILLS CEMETERY
C/O INEZ GIBSON 512 COOL MEADOW COURT
DESOTO, TX 75115

2930 S RIDGE ROAD ROCKWALL, LLC
PO BOX 741209
HOUSTON, TX 77274

2930 S RIDGE ROAD ROCKWALL, LLC
PO BOX 741209
HOUSTON, TX 77274

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

Property Owner and/or Resident of the City of Rockwall:

You are hereby notified that the City of Rockwall Planning and Zoning Commission and City Council will consider the following application:

Z2025-011: PD Development Plan for (8) Eight Townhomes

Hold a public hearing to discuss and consider a request by Tyler Adams of Greenlight Studio on behalf of Matt Zahm of ZAPA Investments, LLC for the approval of a PD Development Plan for eight (8) *Townhomes* on a 0.87-acre parcel of land identified as Lot 3, Block A, Harbor Hills Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 32 (PD-32), situated within the *Interior Subdistrict* and the *Residential Subdistrict*, generally located on the northeast side of Glen Hill Way, northwest of the intersection of Glen Hill Way and Ridge Road [FM-740], and take any action necessary.

For the purpose of considering the effects of such a request, the Planning and Zoning Commission will hold a public hearing on Tuesday, March 11, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, March 17, 2025 at 6:00 PM. These hearings will be held in the City Council Chambers at City Hall, 385 S. Goliad Street.

As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Henry Lee
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, March 17, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

--- PLEASE RETURN THE BELOW FORM ---

Case No. Z2025-011: PD Development Plan for (8) Eight Townhomes

Please place a check mark on the appropriate line below:

- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Four horizontal grey bars for providing reasons for support or opposition.

Name:
Address:

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

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Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

PLEASE RETURN THE BELOW FORM

Case No. Z2025-011: PD Development Plan for (8) Eight Townhomes

Please place a check mark on the appropriate line below:

- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

I have no issues with townhomes in Rockwall
they would look nice on the way into the Harbor

Name: AJAY Gulati
Address: 3018 Ridge Road #100

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

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As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Henry Lee
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, March 17, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

----- PLEASE RETURN THE BELOW FORM -----

Case No. Z2025-011: PD Development Plan for (8) Eight Townhomes

Please place a check mark on the appropriate line below:

- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Empty lot does not look good.
 We need townhomes in Rockwell.
 I am 100% for this

Name: David Martinez
 Address: 3012 Rube Road Rockwell Texas.

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

Please place a check mark on the appropriate line below:

I am in favor of the request for the reasons listed below.

I am opposed to the request for the reasons listed below.

This will bring more residents to Rockwall County and revenue.
This notice was already filed out when it was sent the first time.

Name: Jennifer Parekh

Address: 3018 Ridge Road

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

Case No. Z2025-011: PD Development Plan for (8) Eight Townhomes

Please place a check mark on the appropriate line below:

I am in favor of the request for the reasons listed below.

I am opposed to the request for the reasons listed below.

So much trees ~~Barba~~ have been cut of due to the
condeminiu & the Hotel's construction. Need to preserve nature.
Also, it going to block our view of the lake and trees which
attract business.

Name: Rockwall Dental Care

Address: 2918 Ridge Rd

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

CITY OF ROCKWALL • PLANNING AND ZONING DEPARTMENT • 385 S. GOLIAD STREET • ROCKWALL, TEXAS 75087 • P: (972) 771-7745 • E: PLANNING@ROCKWALL.COM

Case No. Z2025-011: PD Development Plan for (8) Eight Townhomes

Please place a check mark on the appropriate line below:

I am in favor of the request for the reasons listed below.

I am opposed to the request for the reasons listed below.

Our house borders the green belt. We've already lost most of the woods view. These new apartments would be able to see directly into our upstairs bedrooms. The noise from the construction, the additional traffic on Ridge Rd that can already not handle the traffic at peak times. Don't we have enough townhomes already?

Name:

Shari Thompson

Address:

1496 Shady Grove Circle, Rockwall TX 75082

Tex. Loc. Gov. Code, Sec. 211.006(d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

CITY OF ROCKWALL • PLANNING AND ZONING DEPARTMENT • 385 S. GOLIAD STREET • ROCKWALL, TEXAS 75087 • P: (972) 771-7745 • E: PLANNING@ROCKWALL.COM

Please place a check mark on the appropriate line below:

I am in favor of the request for the reasons listed below.

I am opposed to the request for the reasons listed below.

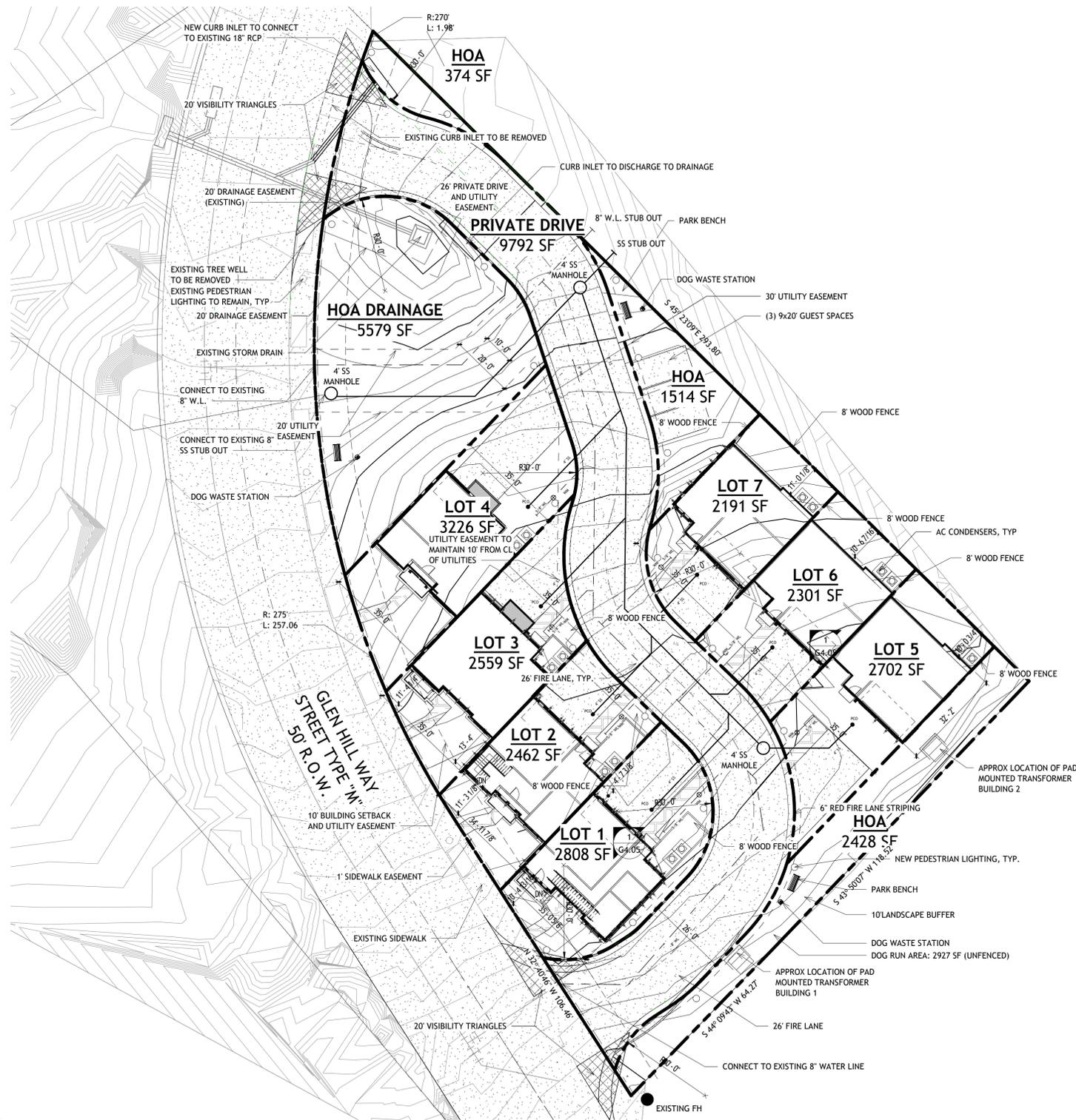
Luxury Townhomes would be nice on the side of town. The empty lot is an eyesore and it needs to be developed. Town homes would be a wonderful idea.

Name: VCA Ridge Road Animal Hospital

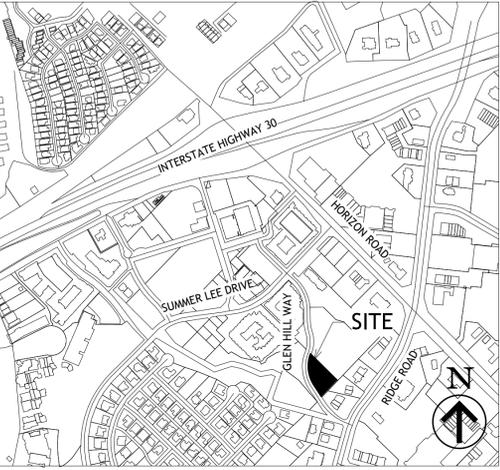
Address: 3010 Ridge Road Rockwell TX

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE



VICINITY MAP



| Property Schedule | |
|-------------------|----------|
| Name | Area |
| HOA | 2428 SF |
| HOA | 1514 SF |
| HOA | 374 SF |
| HOA DRAINAGE | 5579 SF |
| LOT 1 | 2808 SF |
| LOT 2 | 2462 SF |
| LOT 3 | 2559 SF |
| LOT 4 | 3226 SF |
| LOT 5 | 2702 SF |
| LOT 6 | 2301 SF |
| LOT 7 | 2191 SF |
| PRIVATE DRIVE | 9792 SF |
| Total | 37936 SF |

- SITE PLAN GENERAL NOTES**
- ALL WORK MUST MEET CITY 2023 STANDARDS OF DESIGN AND CONSTRUCTION.
 - RETAINING WALLS OVER 3' MUST BE ENGINEERED.
 - ALL RETAINING WALLS OVER 18" MUST BE ROCK OR STONE FACED.
 - ALL UTILITIES MUST BE UNDERGROUND.
 - ALL PROPOSED UTILITIES SHOWN ARE CONCEPTUAL IN NATURE.

- DRAINAGE NOTES**
- GLEN HILL WAY AS-BUILT FLOWS SHALL BE MAINTAINED.
 - PROPERTY OWNER RESPONSIBLE FOR MAINTENANCE, REPAIR, AND REPLACEMENT OF DETENTION/DRAINAGE SYSTEMS.

- WATER AND WASTEWATER NOTES**
- ANY UTILITY CONNECTION MADE UNDERNEATH AN EXISTING ROADWAY MUST BE BORED.
 - ALL UTILITY CROSSINGS MUST BE DRY BORE AND STEEL ENCASED.
 - NO TREES SHALL BE WITHIN 10' OF ANY PUBLIC WATER, SEWER, OR STORM LINE THAT IS 10" IN DIAMETER OR LARGER.
 - NO TREES TO BE WITHIN 5' OF ANY PUBLIC WATER, SEWER, OR STORM LINE THAT IS LESS THAN 10".
 - NO LANDSCAPE BERMS OR TREE PLANTINGS SHALL BE LOCATED ON TOP OF CITY UTILITIES OR WITHIN EASEMENTS.

- LIFE SAFETY GENERAL NOTES**
- EACH UNIT TO BE SEPARATED BY A CONTINUOUS ONE HOUR FIRE BARRIER.
 - BUILDINGS SHALL BE EQUIPPED WITH AN NFPA13D FIRE SPRINKLER SYSTEMS.

Site Data Summary Table

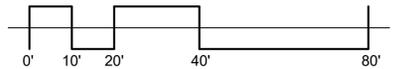
| General Site Data | |
|---------------------------------------|------------------------------------|
| Zoning | PD-32 Residential Subdistrict |
| Existing Use | Vacant |
| Proposed Land Use | Single Family Attached (Townhomes) |
| Lot Area Combined (SF) | 37,935 |
| Lot Area Combined (Acreage) | 0.87 |
| Building Footprint Area (Approximate) | 9,363 |
| Area Covered by Canopy | 0 |
| Single Family Attached Units | 8 |
| Unit Density Per Acre | 9.2 |
| Building 1 Area | 16,506 |
| Building 2 Area | 10,521 |
| Total Building Area | 27,027 |
| Maximum Building Height (# Stories) | 3 |
| Maximum Building Height (Feet) | 50' to Roof Ridge |
| Lot Coverage | 25% |
| Floor Area Ratio | 5/7 |

Parking

| | |
|----------------------------------|-----|
| Required Parking 2 Per unit | 16 |
| Parking Provided | |
| Uncovered Surface Spaces (Guest) | 3 |
| Garage Spaces | 16 |
| Tandem (Driveway) Spaces | 16 |
| Total Parking Provided | 35 |
| Accessible Parking Required | N/A |
| Accessible Parking Provided | 0 |

Landscaping Area

| Open Space Required | |
|---|--------|
| Required area per zoning | N/A |
| Total Open Space Provided | 4,822 |
| Other Landscape area within the lot | 10,672 |
| Total Landscape Area | 15,494 |
| Impervious Area | |
| Building Area | 9,363 |
| Sidewalks, Pavement, and other Impervious Flatwork | 9,896 |
| Other Impervious Area (Decorative Stamped Concrete) | 3,182 |
| Total Impervious Area | 22,441 |
| Total Landscape Area, Impervious Area, Permeable Area | 37,935 |
| Total Area for Outdoor Storage | None |



1 Site Plan Copy 1
1" = 20'-0"

GREENLIGHT

STUDIO, LLC
www.greenlightcompany

Applicant:
Greenlight Studio LLC
100 N. Cottonwood Drive
Suite 104
Richardson, Texas 75080
v: 214.810.4335

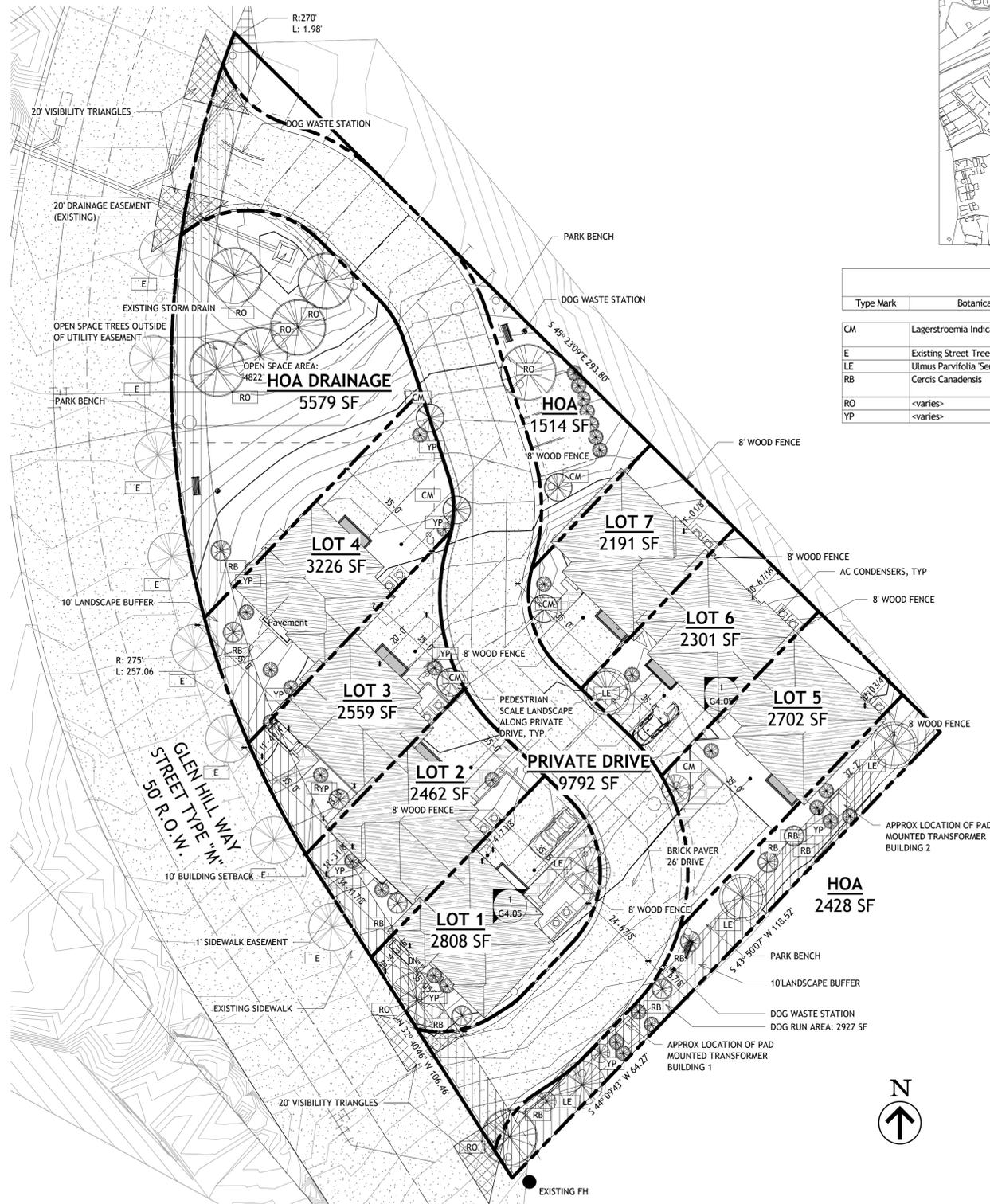
Owner:
Zappa Investments, LLC
201 W. Kaufman St.
Richardson, Texas 75081
v: 972.682.1435

PD Amendment Plan

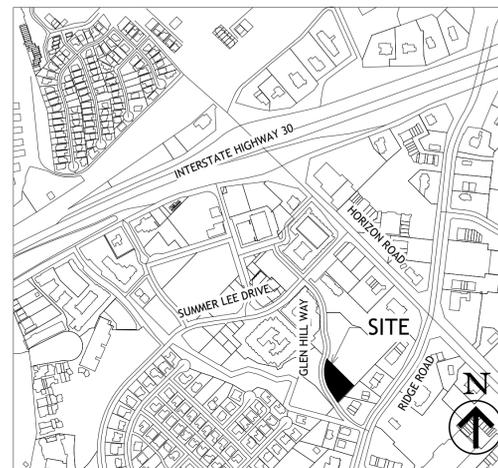
Rockwall Townhomes
Glen Hill Way
Lot 3 Block A Harbor Hills Addition
Rockwall, Texas

City of Rockwall, Texas
E. Teal Survey Abstract A207

Prepared April 1, 2025



VICINITY MAP



| Planting Schedule | | | | | |
|-------------------|------------------------------------|---------------|----------|-------|-----------------|
| Type Mark | Botanical Name | Common Name | Size | Count | Comments |
| CM | Lagerstroemia Indica 'Basham Pink' | Crape Myrtle | 30 Gal | 6 | Container Grown |
| E | Existing Street Tree | Existing | Existing | 8 | Existing |
| LE | Ulmus Parvifolia 'Sempervirens' | Lacebark Elm | 4' Cal. | 5 | Nursery Grown |
| RB | Cercis Canadensis | Texas Red Bud | 30 Gal | 11 | Container Grown |
| RO | <varies> | <varies> | <varies> | 8 | <varies> |
| YP | <varies> | <varies> | <varies> | 33 | <varies> |

LANDSCAPE PLAN NOTES

- APPROVAL OF IRRIGATION PLAN IS REQUIRED PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. AN IRRIGATION PLAN MUST BE PREPARED AND SEALED BY A LICENSED IRRIGATOR IN THE STATE OF TEXAS AND WILL MEET THE REQUIREMENTS OF UDC. ALL LANDSCAPED AND TURF AREAS MUST BE IRRIGATED BY AN AUTOMATED IRRIGATION SYSTEM, UNLESS ALTERNATIVE WATER WISE IRRIGATION SYSTEM IS APPROVED BY THE CITY. ALL LANDSCAPE BEDS SHALL BE SEPARATED FROM TURF AREAS BY EDGING.
- THE DEVELOPER SHALL ESTABLISH GRASS AND MAINTAIN THE SEEDING AREA, INCLUDING WATERING, UNTIL A "PERMANENT STAND OF GRASS" IS OBTAINED AT WHICH TIME THE PROJECT WILL BE ACCEPTED BY THE CITY. A "STAND OF GRASS" SHALL CONSIST OF 75% TO 80% COVERAGE AND A MINIMUM OF ONE-INCH IN HEIGHT AS DETERMINED BY THE CITY.
- ALL LANDSCAPE BUFFERS AND PUBLIC RIGHT OF WAY LOCATED ADJACENT TO A PROPOSED DEVELOPMENT SHALL BE IMPROVED WITH GRASS (I.E. SOD - HYDRO MULCH SHALL BE PROHIBITED IN THESE AREAS) PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
- TREES MUST BE PLANTED AT LEAST FIVE FEET FROM WATER, SEWER, AND STORM LINES.

Site Data Summary Table

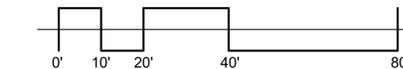
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| Total Impervious Area | 22,441 |
| Total Landscape Area, Impervious Area, Permeable Area | 37,935 |
| Total Area for Outdoor Storage | None |



1 Conceptual Landscape Plan
1" = 20'-0"

GREENLIGHT

STUDIO, LLC
www.greenlightcompany

Applicant:

Greenlight Studio LLC
100 N. Cottonwood Drive
Suite 104
Richardson, Texas 75080
vt: 214.810.4333

Owner:

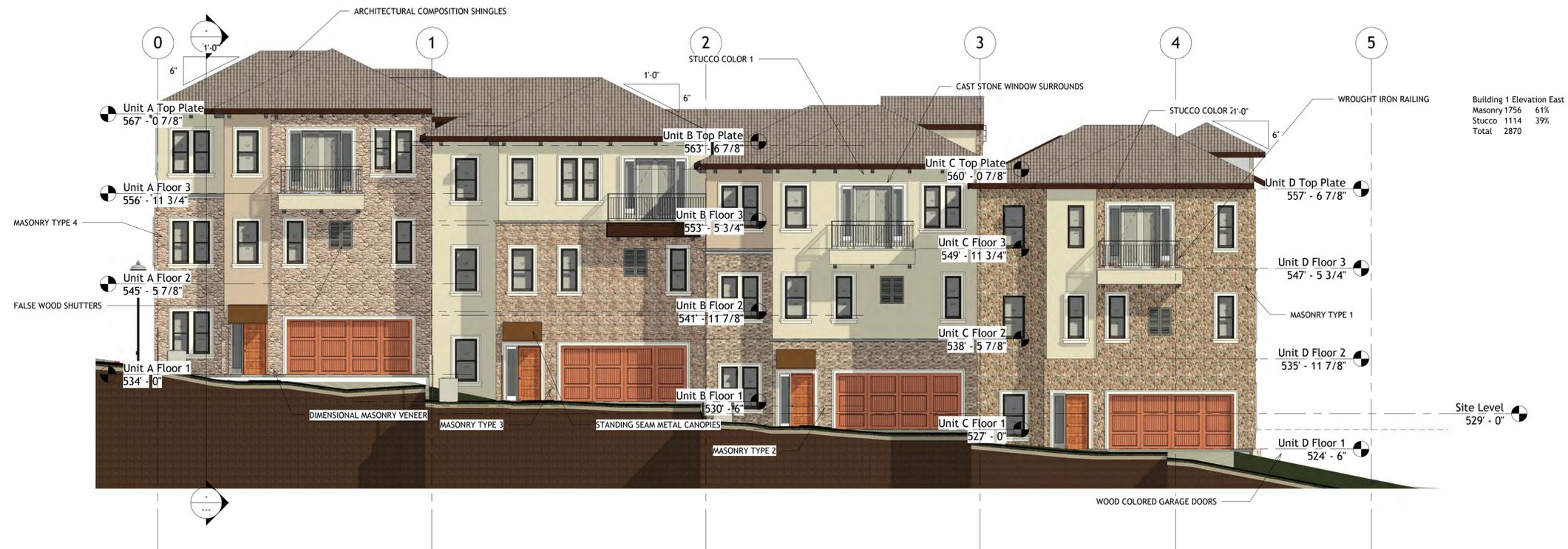
Zappa Investments, LLC
201 W Kaufman St
Richardson, Texas 75081
vt: 972.662.1435

Concept Landscape Plan

Rockwall Townhomes
Glen Hill Way
Lot 3 Block A Harbor Hills Addition
Rockwall, Texas

City of Rockwall, Texas
E. Teal Survey Abstract A207

Prepared April 1, 2025



Building 1 Elevation East

| | | |
|--------------|-------------|-----|
| Masonry | 1756 | 61% |
| Stucco | 1114 | 39% |
| Total | 2870 | |

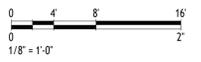
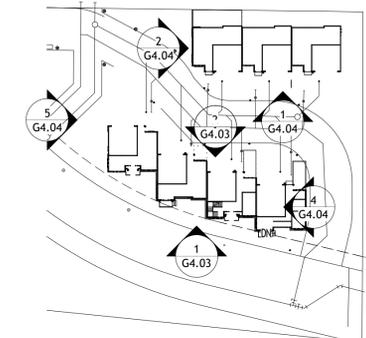
② Building 1 Facade - East
1/8" = 1'-0"



Building 1 Elevation West

| | | |
|--------------|-------------|-----|
| Masonry | 1982 | 60% |
| Stucco | 1333 | 40% |
| Total | 3315 | |

① Building 1 Facade - West
1/8" = 1'-0"



GREENLIGHT

STUDIO, LLC
www.greenlightcompany

Applicant:

Greenlight Studio LLC
100 N. Cottonwood Drive
Suite 104
Richardson, Texas 75080
v: 214.810.4335

Owner:

Zopa Investments, LLC
201 W Kaufman St
Richardson, Texas 75081
v: 972.862.1435

Color Elevations

Rockwall Townhomes
Glen Hill Way
Lot 3 Block A Harbor Hills Addition
Rockwall, Texas

City of Rockwall, Texas
E. Teal Survey Abstract A207

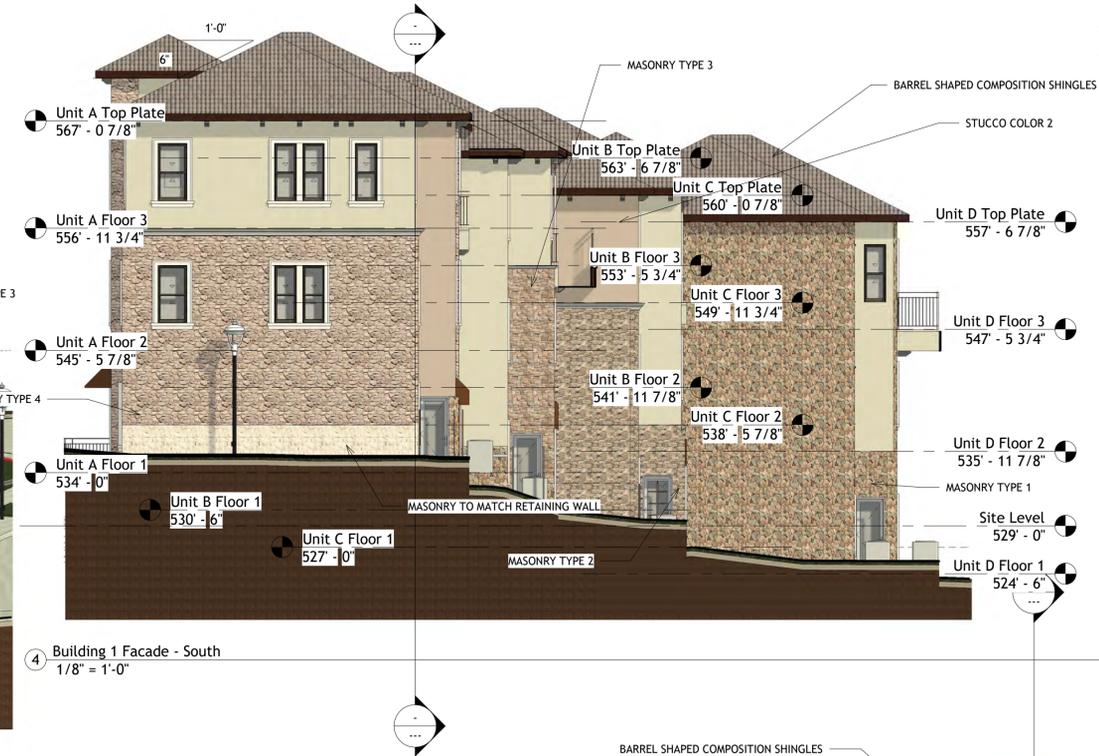
Prepared April 1, 2025

Building 1 Elevation North
 Masonry 1779 64%
 Stucco 1019 36%
 Total 2798



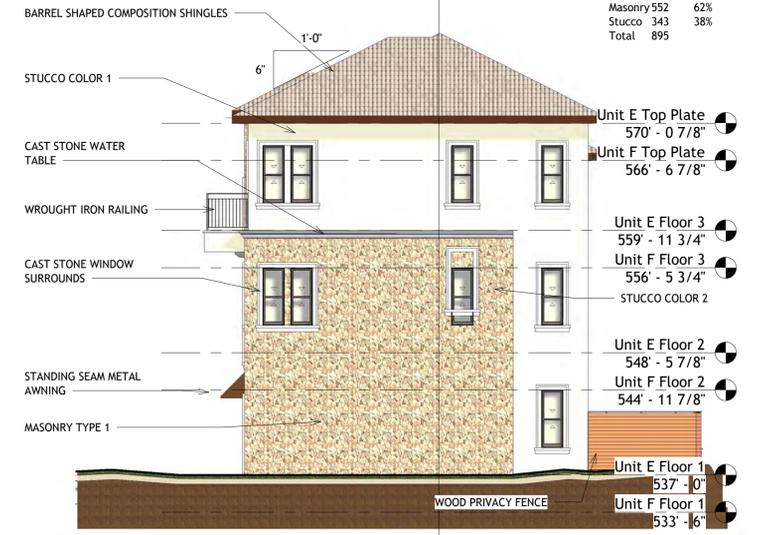
5 Building 1 Facade - North
 1/8" = 1'-0"

Building 1 Elevation South
 Masonry 1371 60%
 Stucco 924 40%
 Total 2295

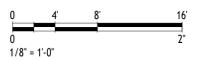


4 Building 1 Facade - South
 1/8" = 1'-0"

Building 2 Elevation South
 Masonry 552 62%
 Stucco 343 38%
 Total 895



3 Building 2 Facade - South
 1/8" = 1'-0"



GREENLIGHT

STUDIO, LLC
 www.greenlight.com

Applicant:

Greenlight Studio LLC
 100 N. Cottonwood Drive
 Suite 104
 Richardson, Texas 75080
 v: 214.810.4333

Owner:

Zappa Investments, LLC
 201 W. Kaufman St.
 Richardson, Texas 75081
 v: 972.862.1435

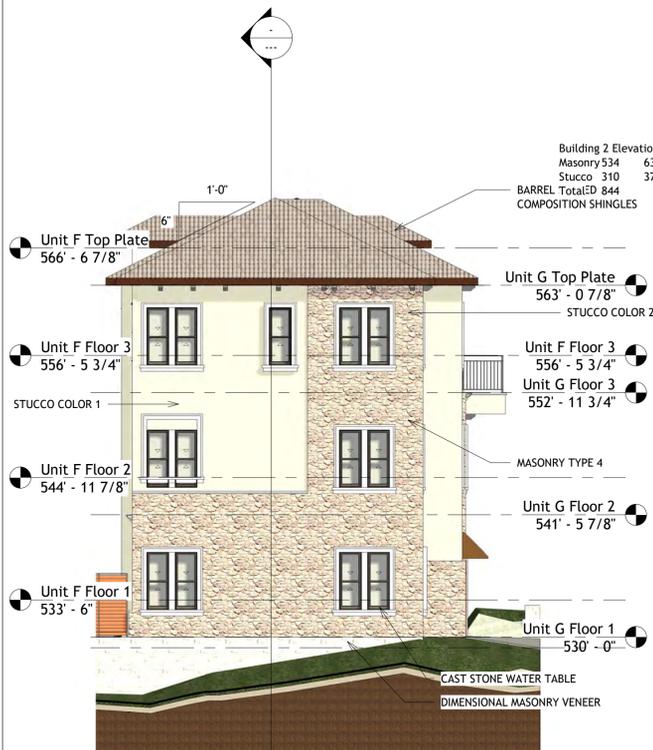
Color Elevations

Rockwall Townhomes
 Glen Hill Way
 Lot 3 Block A Harbor Hills Addition
 Rockwall, Texas

City of Rockwall, Texas
 E. Teal Survey Abstract A207

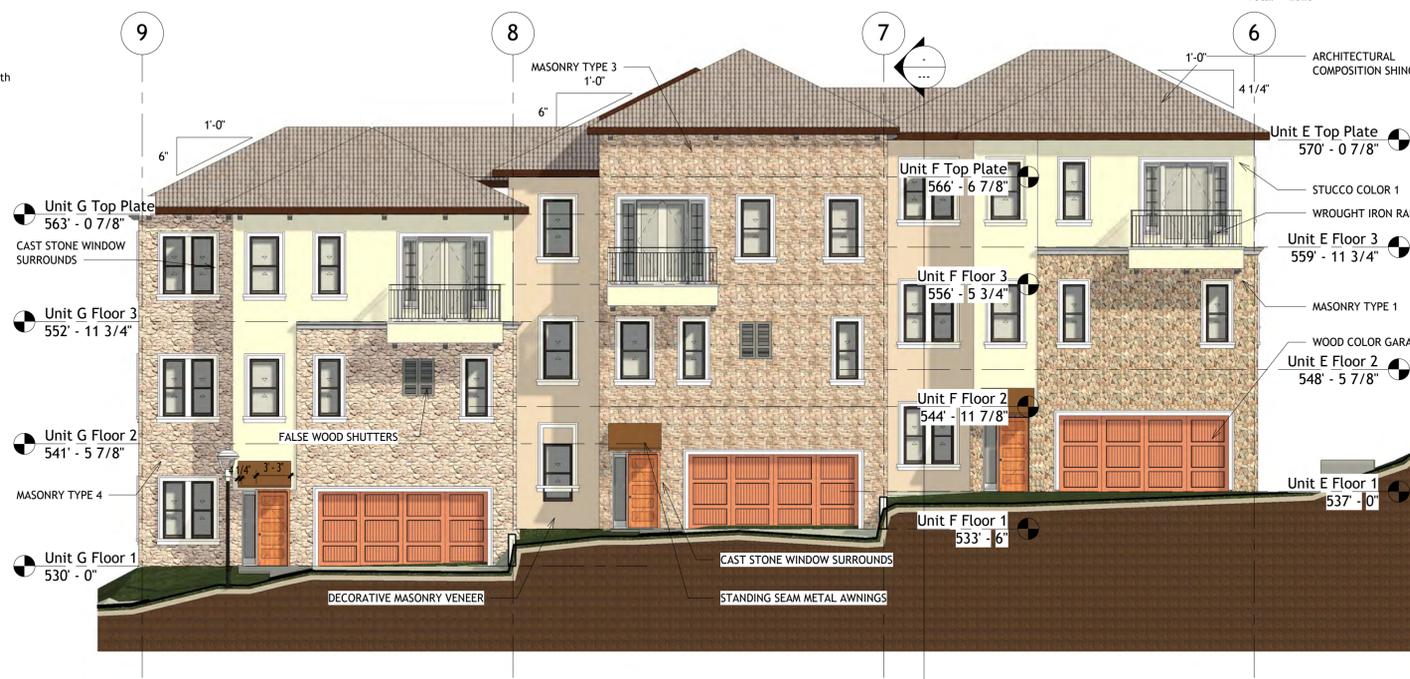
Prepared April 1, 2025

Building 2 Elevation North
 Masonry 534 63%
 Stucco 310 37%
 Total 844



2 Building 2 Facade - North
 1/8" = 1'-0"

Building 2 Elevation West
 Masonry 1452 62%
 Stucco 876 38%
 Total 2328



1 Building 2 Facade - West
 1/8" = 1'-0"

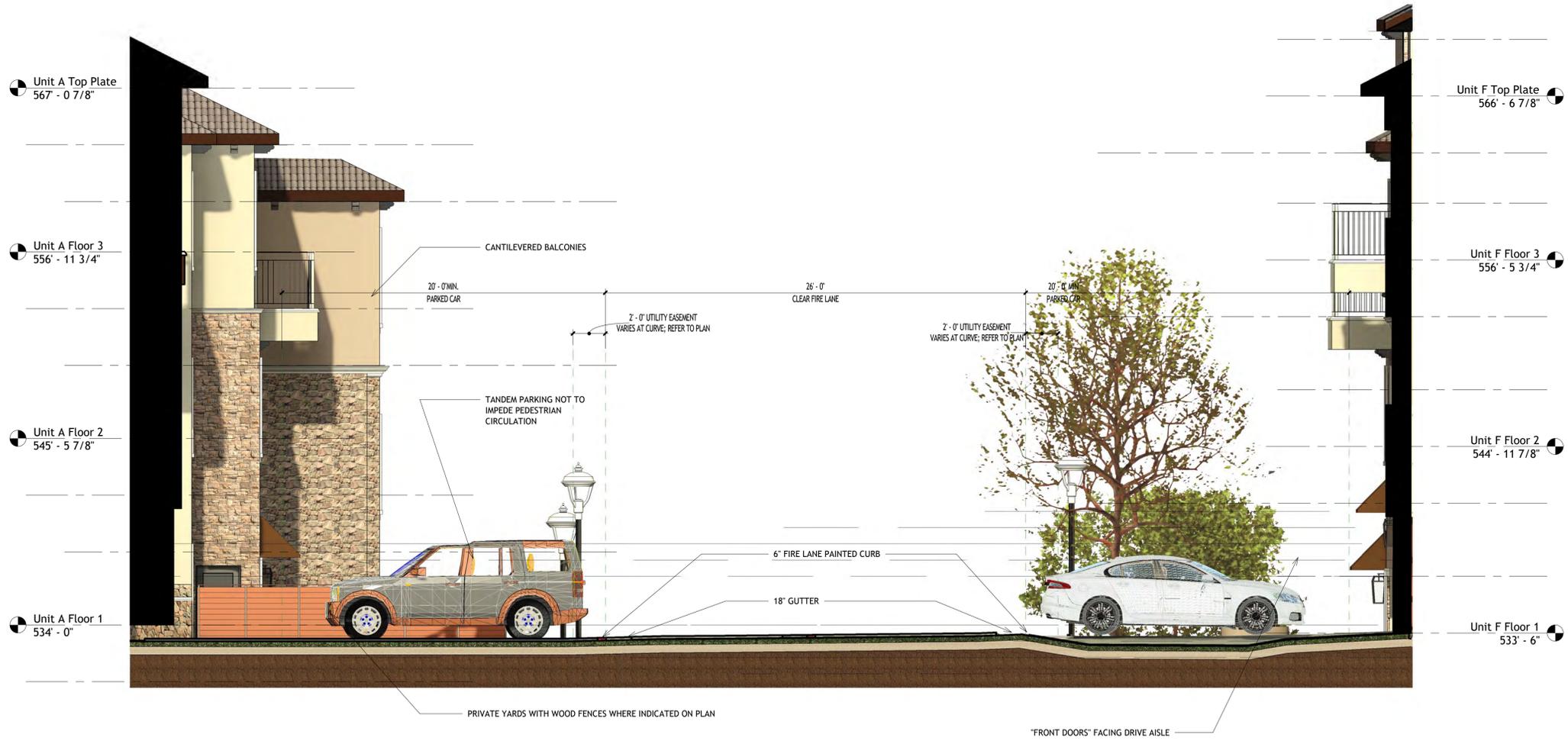




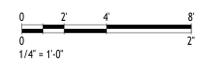








1 Drive Aisle Section
1/4" = 1'-0"



GREENLIGHT

STUDIO, LLC
www.greenlightcompany

Applicant:
Greenlight Studio LLC
100 N. Cottonwood Drive
Suite 104
Richardson, Texas 75080
v: 214.810.4335

Owner:
Zappa Investments, LLC
201 W Kaufman St
Richardson, Texas 75081
v: 972.862.1435

Drive Aisle Section

Rockwall Townhomes
Glen Hill Way
Lot 3 Block A Harbor Hills Addition
Rockwall, Texas

City of Rockwall, Texas
E. Teal Survey Abstract A207

Prepared April 1, 2025

EXHIBIT 'E-9':
Residential Sub-District

RESIDENTIAL SUB-DISTRICT

The *Residential Sub-District* is reserved for zero lot line single-family residential housing. A lower volume of traffic in this area of the Harbor District make this *Sub-District* ideal for single family use. Key characteristics of this *Sub-District* include adjacency to open space and steep slopes that offer views of the surrounding area. Existing slopes within this *Sub-District* are in the six (6) percent to 16% range. This *Sub-District* is predominately vegetated by massing of existing trees that should be preserved when possible.

BUILDING PLACEMENT

BUILD-TO-LINE (DISTANCE FROM ROW LINE)

| | |
|----------------------|-----|
| <i>Street Type M</i> | 10' |
|----------------------|-----|

BUILDING FORM

| | |
|----------------------|----|
| <i>Street Type N</i> | 5' |
| <i>Side</i> | 8' |
| <i>Zero Lot Line</i> | 0' |

NOTE: A minimum of 50% of the boundary with the Open Space Sub-District along *Street Type G* must be a public street (i.e. *Street Type M*).

BUILDING FORM

| | |
|---|-----------|
| <i>Street Type M Min. Façade Built to BTL</i> | 65% |
| <i>Maximum Lot Coverage</i> | 61% |
| <i>Minimum Lot Size</i> | 30' x 90' |

USE

| | |
|--------------|-------------|
| Ground Floor | Residential |
| Upper Floors | Residential |

| <i>HEIGHT</i> | <i>STORIES</i> | <i>HEIGHT</i> |
|--------------------------------|----------------|---------------|
| <i>Maximum Building Height</i> | 3 | 36' |

ENCROACHMENTS

| | |
|--------------|----|
| <i>Front</i> | 5' |
|--------------|----|

NOTE: Steps, stoops, and roof overhangs may encroach over the BTL as shown in the table above.

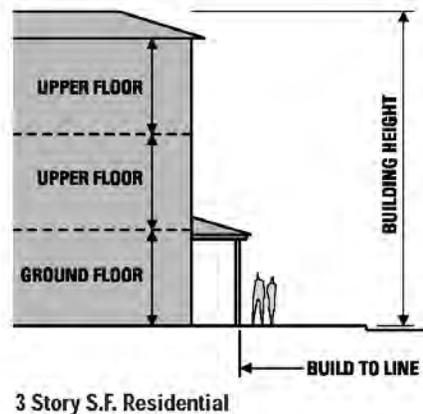


EXHIBIT 'E-9':
Residential Sub-District

RESIDENTIAL SUB-DISTRICT

FLOOR PLAN/ELEVATION REPETITION

To avoid repetition and a monotonous look to the *Sub-District* environment, the following guidelines apply regarding the repetition of floor plans and elevations:

CASE 1

When building a house having the same floor plan and the same elevation, whether on the same or opposite side of the street, four (4) full lots must be skipped. Different paint color must be used. See Figure A.

CASE 2

When building a house having the same floor plan and a different elevation on the same side of the street, three (3) full lots must be skipped. Different paint color must be used. See Figure B.

CASE 3

When building a house having the same floor plan and a different elevation on the opposite side of the street, two (2) full lots must be skipped. Different paint color must be used. See Figure B.

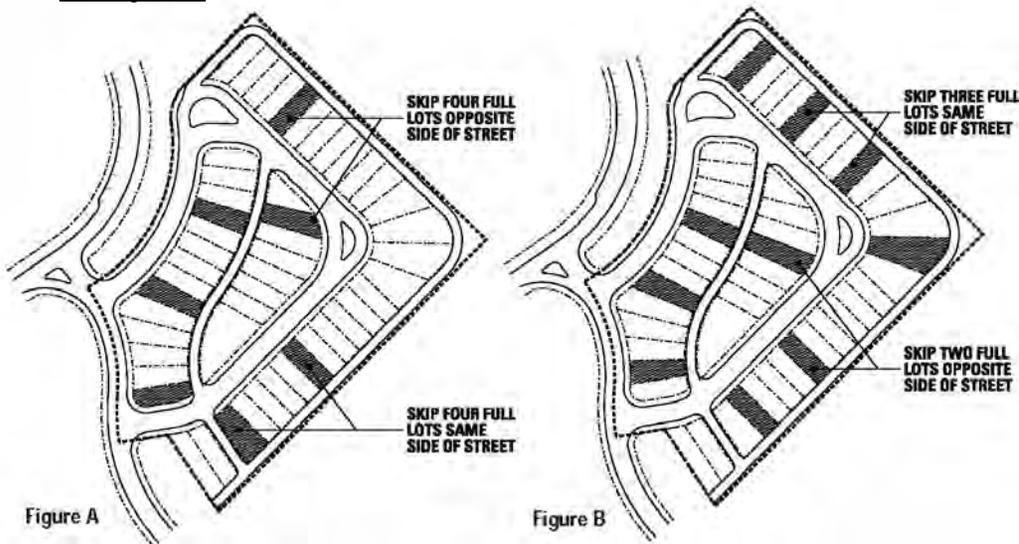


Figure A

Figure B

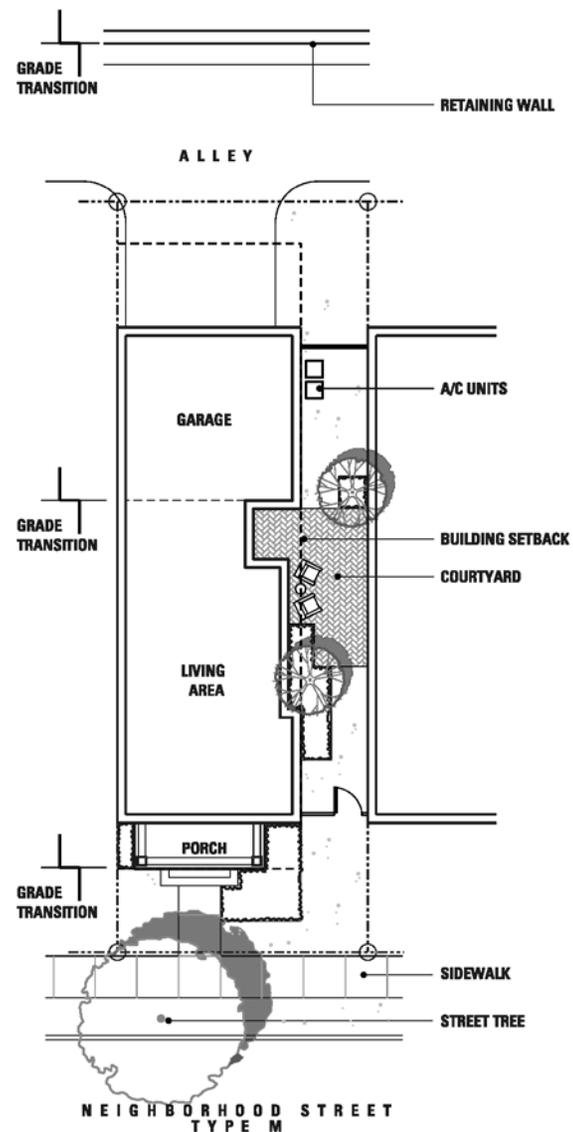
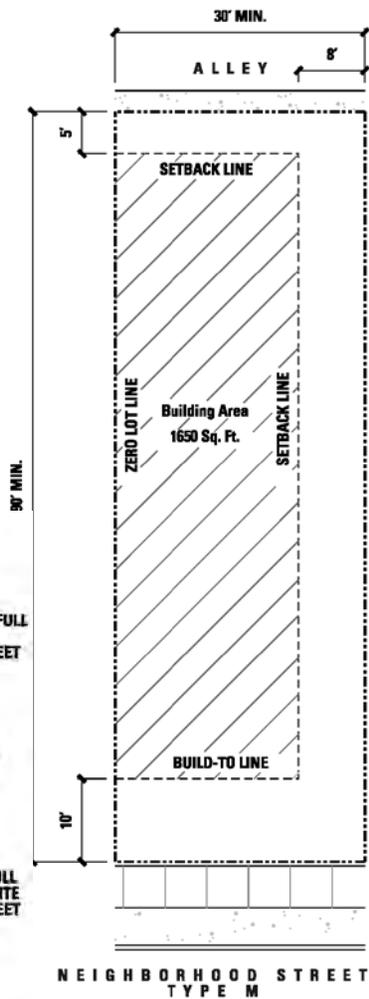


EXHIBIT 'E-9':
Residential Sub-District

RESIDENTIAL SUB-DISTRICT

ARCHITECTURAL GUIDELINES

SITE LAYOUT, GRADING AND DRAINAGE

- (1) Runoff onto adjacent properties is prohibited.
- (2) Finished floor elevations shall be set in order to provide positive drainage away from all buildings.
- (3) Create smooth slope transitions, blending with natural grades, with maximum slopes in lawn areas being 4:1.
- (4) Do not run drainage swales from planting areas across paved areas.

LANDSCAPE STRUCTURES

- (1) Basketball goals, play structures, swing sets and similar elements may only be located in side or rear yards.
- (2) All vertical construction including garages, gazebos, arbors, barbeques and fireplaces must comply with front, side and rear setbacks.
- (3) Use materials, forms, styles and colors which match or are complementary to the architectural style and detailing of the house.

POOLS AND WATER FEATURES

- (1) Swimming pools, spas and water features must comply with front, side and rear setbacks. Above grade pools or spas are not permitted.
- (2) Fencing or design solutions which provide enclosures in accordance with local ordinances and regulations must be provided.
- (3) Mechanical equipment for pools and spas must be located so as to not be visible from adjacent properties or the street. Locate or enclose equipment to minimize noise intrusion.

LIGHTING, UTILITIES AND SERVICE

- (1) Exterior ornamental and accent lighting may be used in moderation to complement landscape and architectural features. Light fixtures shall have appropriate light cutoff shields to prevent unwanted glare to neighboring properties and streets.
- (2) Post mounted light fixtures and bollard lighting are not permitted. Pathway lighting may be used, limited to 18-inches in height.

- (3) Colored, fluorescent and neon lighting is prohibited. Obtrusive HID security lighting fixtures are prohibited.
- (4) Exposed exterior floodlights or wall packs are not permitted.
- (5) All site utilities shall be underground. Transformers, meters and other utility equipment shall be screened by planting or by architectural means. Avoid clipped hedges around utility boxes.

IRRIGATION DESIGN

- (1) Irrigation is required for all home sites and shall be an automatic, underground system with rain and freeze sensors.
- (2) Design irrigation systems to separately circuit irrigation zones for plant materials having different watering requirements, such as lawns and shrubs.
- (3) Prevent overspray onto streets, pedestrian walks, driveways and buildings.

DRIVEWAYS

- (1) Driveways shall be paved with concrete, pavers, or integrally colored concrete with charcoal grey color. Chemical staining enhancements are not permitted. Driveways may not be painted with an opaque coating.

GARAGES

- (1) Garages must be rear loaded from an alley.
- (2) All garages must hold a minimum of one and a maximum of two cars. Guest parking spaces shall be on street.

BUILDING MASSING

- (1) The house footprint shall be composed of squares and rectangles placed at right angles to one another. The geometry of each of these shapes shall be uninterrupted by adjacent shapes.
- (2) All building footprints shall be at right angles to the property lines. Compose house forms to create exterior space.
- (3) Massing may be either symmetrical or asymmetrical. The dominant architectural mass shall be located near the center of the composition.

ROOFS

- (1) The individual shapes which compose the house footprint shall each be roofed by a symmetrical gable, hip, or a simple roof form.
- (2) The geometry of each roof form shall be uninterrupted by adjacent roof forms. Valleys are discouraged, except for dormers.

WALL MATERIALS

- (1) Exterior wall materials shall be any of the following:
 - A. Fiber cement board siding and shingles, primed and painted, "Hardie" type boards. Siding shall be installed horizontally only, lapped clap board style with narrow lap exposures preferred over wide exposures.
 - B. Portland cement stucco with integral color
 - C. Brick masonry
 - D. Stone masonry
 - E. Cast stone masonry
 - F. Fireplace masses and chimneys shall be clad in noncombustible materials to match exterior house materials.
 - G. Aluminum, masonite, and vinyl siding are not permitted.
- (2) Exterior wall materials may change at a vertical line which delineates a change in building form, or along horizontal lines.

EXTERIOR FINISHES

- (1) All wood and composite surfaces shall be finished with paint, applied over an appropriate primer. Opaque and semi-opaque stain may be used.

TRIM

- (1) All trim, where used, shall be cement fiber board, cedar, or quality wood products. Trim shall be a minimum 3/4-inch thick with flat surfaces. Built up assemblies, shaped profiles, and ornamental embellishments are to be avoided.

EXHIBIT 'E-9':
Residential Sub-District

RESIDENTIAL SUB-DISTRICT

ARCHITECTURAL GUIDELINES

DOORS

- (1) All exterior doors shall be wood or metal, hinged, of rail and stile construction, with one (1) to six (6) panels. Dormers above doors are encouraged. Panels shall be glazed or flat wood (not raised panels). Doors shall be painted.
- (2) Exterior doors shall be swing type, of solid wood or wood veneer, and shall be painted, or clad in pre-finished metal.

SCREEN DOORS

- (1) Screen doors are allowed. Screen doors shall be of wood rail and stile construction, with one (1) to six (6) panels. All panels shall be screened. Screen doors shall be painted.
- (2) Storm doors are not permitted at the front door but are allowed at side and rear doors.

GARAGE DOORS

- (1) Garage doors shall be overhead roll-up construction with horizontal insulated metal panels.
- (2) Garages may have maximum 18-foot wide doors.

EXTERIOR DOOR HARDWARE

- (1) Oversize or overly ornamented door hardware or escutcheon trim plates are not permitted on the front door.

WINDOWS

- (1) Windows shall be casements, awnings, or double or single hung. Each window shall be glazed by a single panel of insulated glass, or may be divided into no more than four (4) panels. Fake mullions are discouraged.
- (2) Windows may be of wood, thermally broken aluminum or aluminum-clad wood construction. Vinyl or vinyl-clad construction is not permitted.
- (3) All operable windows shall be equipped with insect screens.
- (4) Bay windows shall be glazed a minimum of 80% of their width.

EXTERIOR WINDOW SHUTTERS

- (1) Window shutters are permitted provided they are proportioned to fully cover the windows they would protect. Shutters may be wood or synthetic.

AWNINGS

- (1) Window awnings are not permitted, with the exception of shutter awnings.

GLAZING

- (1) Window and door glazing shall be clear, insulated, double-pane, low-emissivity glass. Applied window films are not permitted.

ROOFS

- (1) Roofs may be covered in composite asphalt shingles. Composition shingles must be minimum 25-year dimensional, laminated type. Flat, three (3) tab shingles will not be permitted. An example of approved roof shingle type is Tamko "Heritage 30." Standing seam metal roofing and other metal roof types may be used.
- (2) All roof penetrations, other than chimneys, shall be grouped as far from frontages as possible, shall not be visible from the street and shall be painted as needed to match roof. Skylights shall not be visible from the street.
- (3) All soffit venting shall be 2-inch continuous venting or perforated "Hardisoffit" panels. No box vents shall be used. No vinyl material may be used.
- (4) Dormers shall be appropriately proportioned, hip or gable, with a minimum 6:12 pitch. They shall be glazed a minimum of 90% of their face and must be open to the interior roof area beyond.
- (5) Roof ventilation dormers are permitted only on the side and rear elevations and must be vented a minimum of 90% of their face.
- (6) Dormers shall have minimum overhangs and eaves. Stucco or "Hardi" Siding or Panels are recommended in dormer gables.
- (7) All exposed metal flashing shall be unpainted galvalume, paint-grip or galvanized steel.

- (8) Roof attic vents shall be continuous ridge vents such as "Cool Ridge." Powered attic vents are not allowed. Turbine vents may be allowed by variance, but must not be visible from the street.
- (9) Satellite dishes and roof antennae shall not be visible from the street.

CHIMNEYS

- (1) All chimneys must be fitted with a galvanized metal chimney cap with a 'roof' that is the same size and shape as the chimney it covers. The roof form may be flat or hipped.
- (2) The chimney cap must include metal mesh, screen or grid to prevent animals from entering the chimney.
- (3) Acceptable finishes are natural 'paint-grip' galvanized or painted galvanized.

RAIN GUTTERS

- (1) Rain gutters and downspouts may be of any profile compatible with the style of the house and roof. Gutters and downspouts shall be located in a way as to not draw attention to themselves.
- (2) Gutters and downspouts shall be unpainted galvalume, "paint-grip" steel or zinc finishes, although prefinished metal finishes may be permitted.
- (3) Downspouts shall terminate at splash blocks, gravel, brick or concrete.

PAINT

- (1) All exterior finish coat paint shall be acrylic latex or latex enamel.

EXHIBIT 'E-9':
Residential Sub-District

RESIDENTIAL SUB-DISTRICT

ARCHITECTURAL GUIDELINES

PORCH CONSTRUCTION

- (1) Porch columns shall be wood posts, minimum six (6) inch by six (6) inch, painted and free of ornamentation.
- (2) Porches shall have wood railings. Railing details must be included with plan submissions for approval.
- (3) Porch floors shall be concrete, stone or brick. Porch construction shall be concrete slab foundation, built as part of the house.
- (4) Porches and covered balconies may be screened at the rear of the homes only.

ELEMENTS NOT PERMITTED

The following list of elements are not permitted in the development:

- ☑ Window A/C Units
- ☑ Exterior Fluorescent Lighting
- ☑ Solar Powered Landscape Lighting
- ☑ Soffit/Cornice Lighting Visible from a Public ROW
- ☑ Skylights Visible from a Public ROW
- ☑ Antennas or Satellite Dishes Greater Than 18-inch Diameter
- ☑ Decorative Flags - Properly Displayed American Flags are Allowed
- ☑ Concrete Statuary Visible from Public ROW
- ☑ Plastic Furniture or Artificial Plants Visible from a Public ROW
- ☑ Asphalt or Gravel Drives



ARCHITECTURAL DETAIL CHARACTER



RESIDENTIAL STREET EDGE CONDITION



RESIDENTIAL STREET CHARACTER



RESIDENTIAL STREET EDGE CONDITION

EXHIBIT 'E-9':
Residential Sub-District



CITY OF ROCKWALL

ORDINANCE NO. 25-XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING PLANNED DEVELOPMENT DISTRICT 32 (PD-32) [ORDINANCE NO. 17-22] AND THE UNIFIED DEVELOPMENT CODE [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, AS HERETOFORE AMENDED, SO AS TO APPROVE A PD DEVELOPMENT PLAN FOR EIGHT (8) TOWNHOMES ON A 0.871-ACRE PARCEL OF LAND IDENTIFIED AS LOT 3, BLOCK A, HARBOR HILLS ADDITION, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS AND MORE FULLY DESCRIBED AND DEPICTED HEREIN BY EXHIBIT 'A'; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City has received a request from Tyler Adams of Greenlight Studio on behalf of Matt Zahm of ZAPA Investments, LLC for the approval of a *PD Development Plan* for eight (8) townhomes to be situated within the *Residential Subdistrict*, on a 0.871-acre parcel of land identified as Lot 3, Block A, Harbor Hills Addition, City of Rockwall, Rockwall County, Texas and more fully described and depicted in *Exhibit 'A'* of this ordinance, which hereinafter shall be referred to as the *Subject Property* and incorporated by reference herein; and

WHEREAS, the Planning and Zoning Commission of the City of Rockwall and the governing body of the City of Rockwall in compliance with the laws of the State of Texas and the ordinances of the City of Rockwall have given the requisite notices by publication and otherwise, and have held public hearings and afforded a full and fair hearing to all property owners generally and to all persons interested in and situated in the affected area, and in the vicinity thereof, and the governing body in the exercise of its legislative discretion, has concluded that Planned Development District 32 (PD-32) [*Ordinance No. 17-22*] and the Unified Development Code [*Ordinance No. 20-02*] should be amended as follows:

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS:

SECTION 1. That Planned Development District 32 (PD-32) [*Ordinance No. 17-22*] and the Unified Development Code [*Ordinance No. 20-02*], as heretofore amended, shall be further amended by adopting this *PD Development Plan* and amending the official zoning map of the City of Rockwall for the *Subject Property*; and,

SECTION 2. That development of the *Subject Property* shall generally be in accordance with the *Concept Plan*, depicted in *Exhibit 'B'* of this ordinance, attached hereto and incorporated herein by reference as *Exhibit 'B'*, which is deemed hereby to be a condition of approval of the amended zoning classification for the *Subject Property*; and,

SECTION 3. That development of the *Subject Property* shall generally be in accordance with the *Conceptual Building Elevations*, depicted in *Exhibit 'C'* of this ordinance, attached hereto and incorporated herein by reference as *Exhibit 'C'*, which is deemed hereby to be a condition of approval of the amended zoning classification for the *Subject Property*; and,

SECTION 4. That the *Subject Property* shall be used only in the manner and for the purposes provided for in Planned Development District 32 (PD-32) [*Ordinance No. 17-22*], the Unified Development Code [*Ordinance No. 20-02*], and in compliance with the following conditions and requirements:

- (1) The development of the subject property shall generally conform to the *Concept Plan* depicted in *Exhibit 'B'* of this ordinance.
- (2) The development of the subject property shall generally conform to the proposed *Conceptual Building Elevations* depicted in *Exhibit 'C'* of this ordinance and to the design guidelines contained in *Resolution No. 10-40*.
- (3) The proposed townhome development shall not contain more than eight (8) townhomes.
- (4) All building materials and color schemes proposed for this development should conform to the requirements stipulated by Planned Development District 32 (PD-32) [*as amended*].
- (5) Prior to the issuance of a building permit the applicant shall submit and seek approval for a detailed *PD Site Plan* that demonstrates compliance with all applicable standards of Planned Development District 32 (PD-32) [*as amended*] and with the requirements approved in this ordinance.

SECTION 5. That any person, firm, or corporation violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction shall be punished by a penalty of fine not to exceed the sum of *Two Thousand Dollars* (\$2,000.00) for each offense and each and every day such offense shall continue shall be deemed to constitute a separate offense;

SECTION 6. That if any section, paragraph, or provision of this ordinance or the application of that section, paragraph, or provision to any person, firm, corporation or situation is for any reason judged invalid, the adjudication shall not affect any other section, paragraph, or provision of this ordinance or the application of any other section, paragraph or provision to any other person, firm, corporation or situation, nor shall adjudication affect any other section, paragraph, or provision of the Unified Development Code, and the City Council declares that it would have adopted the valid portions and applications of the ordinance without the invalid parts and to this end the provisions for this ordinance are declared to be severable;

SECTION 7. The standards in this ordinance shall control in the event of a conflict between this ordinance and any provision of the Unified Development Code or any provision of the City Code, ordinance, resolution, rule, regulation, or procedure that provides a specific standard that is different from and inconsistent with this ordinance. References to zoning district regulations or other standards in the Unified Development Code (including references to the *Unified Development Code*), and references to overlay districts, in this ordinance or any of the Exhibits hereto are those in effect on the date this ordinance was passed and approved by the City Council of the City of Rockwall, Texas;

SECTION 8. That this ordinance shall take effect immediately from and after its passage;

**PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS,
THIS THE 7th DAY OF APRIL, 2025.**

Trace Johannesen, *Mayor*

ATTEST:

Kristy Teague, *City Secretary*

APPROVED AS TO FORM:

Frank J. Garza, *City Attorney*

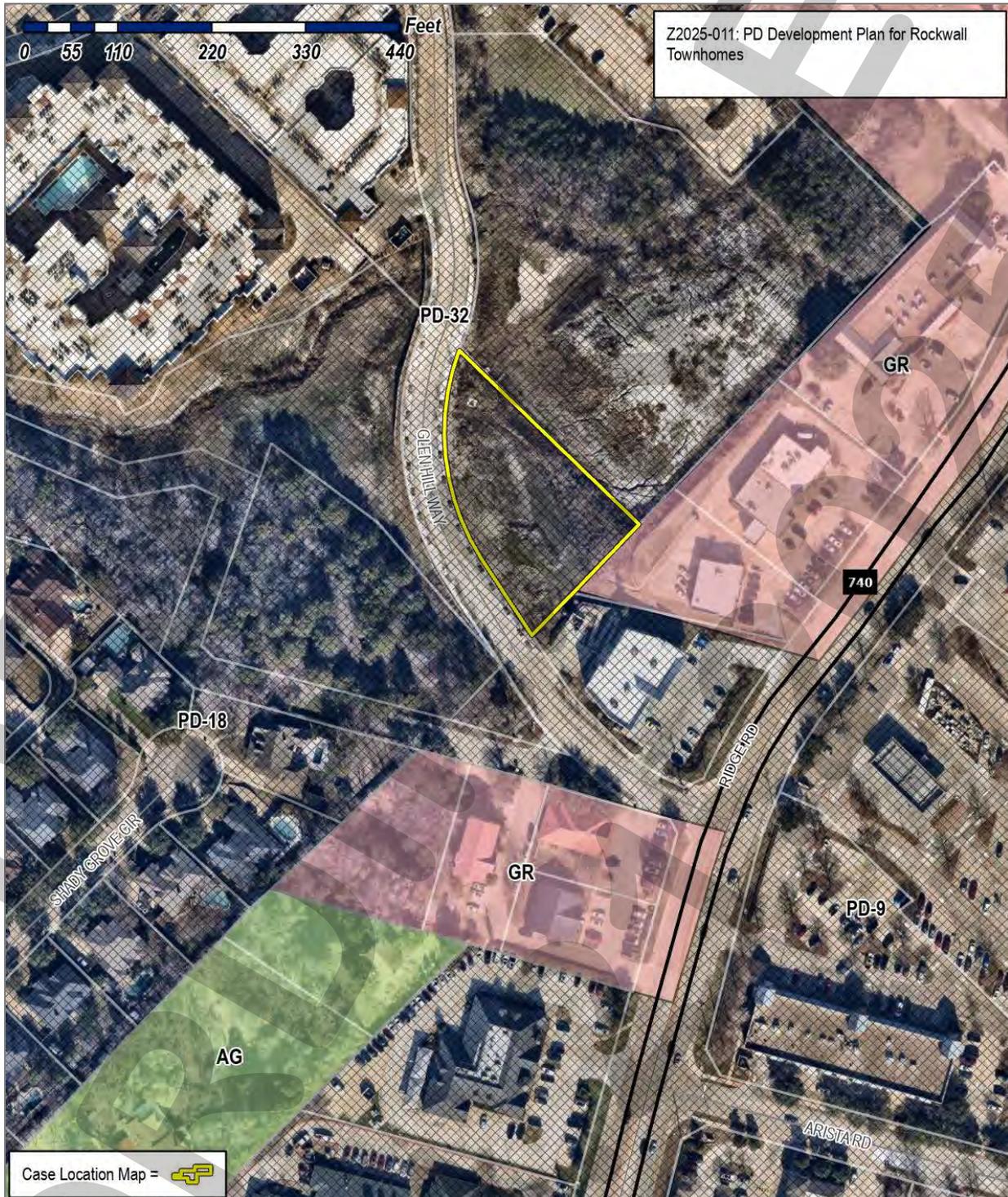
1st Reading: March 17, 2025

2nd Reading: April 7, 2025

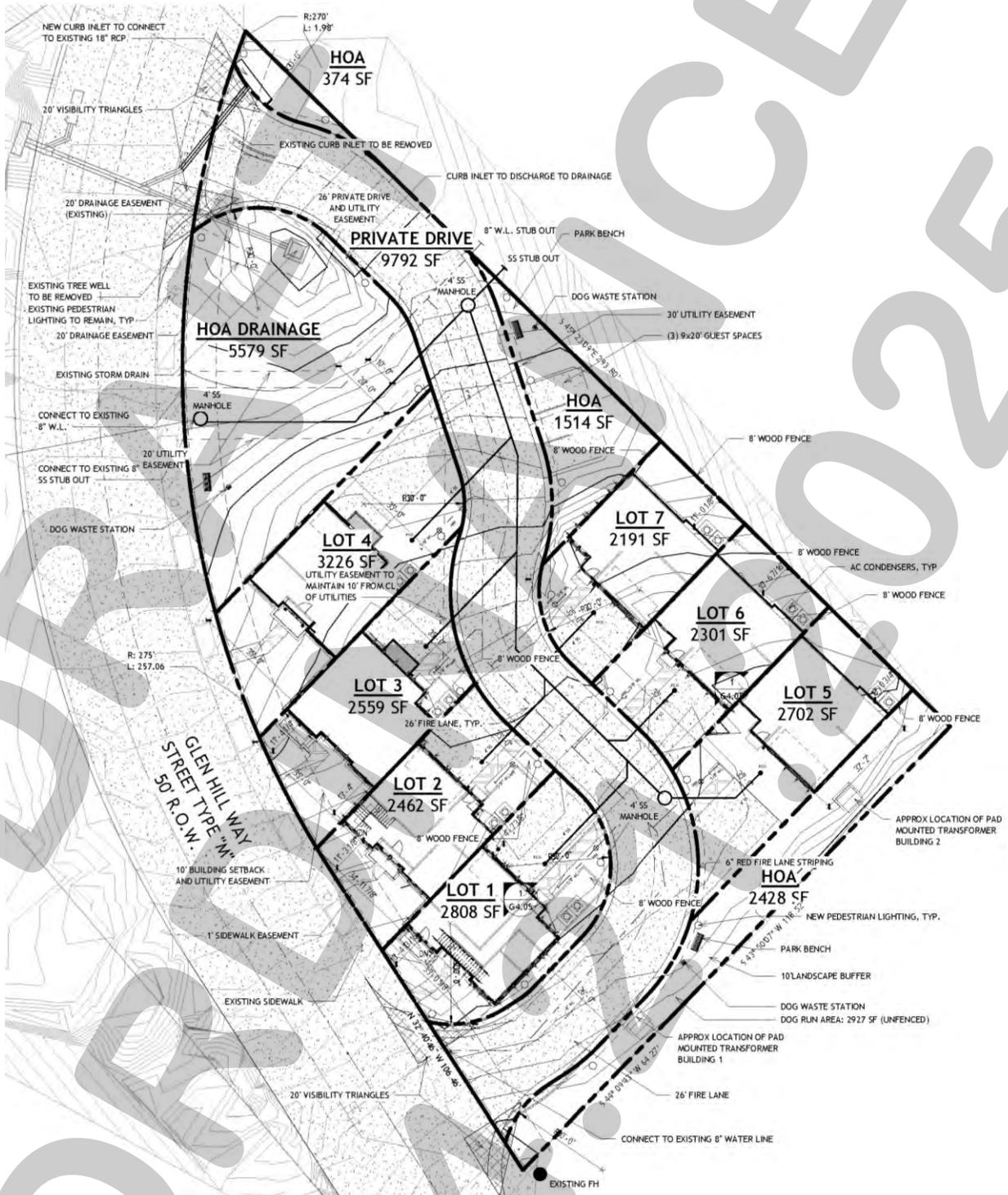
DRAFT
ORDINANCE
04.21.2025

**Exhibit 'A':
Location Map**

Legal Description: Lot 3, Block A, Harbor Hills Addition



**Exhibit 'B':
Concept Plan**



**Exhibit 'C':
Conceptual Building Elevations**





CITY OF ROCKWALL
CITY COUNCIL MEMORANDUM

PLANNING AND ZONING DEPARTMENT
385 S. GOLIAD STREET • ROCKWALL, TX 75087
PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
DATE: April 21, 2025
APPLICANT: David Gamez
CASE NUMBER: Z2025-012; *Specific Use Permit (SUP) for Residential Infill at 614 E. Boydston Avenue*

On April 15, 2025, the Planning and Zoning Commission held a public hearing on *Case No. Z2025-012*, and approved a motion to continue the public hearing to the April 29, 2025 Planning and Zoning Commission meeting. The purpose of this action was due to the applicant not being present at the public hearing. According to Subsection 02.03, *Procedures for Zoning Applications*, of Article 11, *Development Applications and Review Procedures*, of the **Unified Development Code (UDC)**, “(a) public hearing that was noticed in the manner prescribed by Subsection 02.03(A) [Article 11; UDC] may be postponed by announcing the postponement at the time and place of the noticed public hearing. The postponement of a public hearing shall be to a specific time and date no later than 30-days from the first or most recent public hearing. A postponed public hearing shall be presumed to be held in the same location as the initial public hearing, unless a different location is announced. The announcement of a postponement at a public hearing shall be sufficient notice and no additional notice is required.” This means that the City Council will need to announce the new public hearing date of May 5, 2025. No further action or motions are required. Should the City Council have any questions, staff will be available at the April 21, 2025 City Council Meeting.



CITY OF ROCKWALL

CITY COUNCIL CASE MEMO

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
DATE: April 21, 2025
APPLICANT: Brandon Spruill; *on behalf of Hallie Fleming*
CASE NUMBER: Z2025-013; *Specific Use Permit (SUP) for a Residential Infill at 588 Cornelius Road*

SUMMARY

Hold a public hearing to discuss and consider a request by Brandon Spruill of Spruill Homes on behalf of Hallie Fleming for the approval of a Specific Use Permit (SUP) for *Residential Infill in an Established Subdivision* on a ten (10) acre tract of land identified Tract 22-02 of the W. M. Dalton Survey, Abstract No. 72, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, addressed as 588 Cornelius Road, and take any action necessary.

BACKGROUND

The subject property was annexed into the City of Rockwall on December 2, 2019 by Ordinance No. 20-03 [*i.e. Case No. A2019-001*]. At the time of annexation, the subject property was zoned Agricultural (AG) District. According to Rockwall Central Appraisal District (RCAD), currently situated on the subject property is a 2,700 SF metal pole barn that was constructed in 2012.

PURPOSE

The applicant -- *Brandon Spruill* -- is requesting the approval of a Specific Use Permit (SUP) for the purpose of constructing a single-family home on the subject property in accordance with Subsection 02.03(B)(11), *Residential Infill in or Adjacent to an Established Subdivision*, of Article 04, *Permissible Uses*, of the Unified Development Code (UDC).

ADJACENT LAND USES AND ACCESS

The subject property is addressed as 588 Cornelius Road. The land uses adjacent to the subject property are as follows:

North: Directly north of the subject property is a 19.00-acre tract land of land (*i.e. Tract 22-05 of the W. M. Dalton Survey, Abstract No. 72*) zoned Planned Development District 91 (PD-91) that makes up part of the Winding Creek Subdivision. Beyond that is Clem Road, which is identified as a *Minor Collector* on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. North of this is a vacant 101.43-acre tract of land (*i.e. Tract 4 of the J. M. Glass Survey, Abstract No. 88*) zoned Agricultural (AG) District.

South: Directly south of the subject property is Cornelius Road, which is identified as a M4U (*i.e. major collector, four [4] lane, undivided roadway*) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Beyond this is the Peoples Tract Addition, which consists of eight (8) lots [*i.e. 291, 333, 375, 401, 451, 487, 525 & 555 Cornelius Road*] and is zoned Single-Family Estate 1.5 (SFE-1.5) District. South of this is Phase I of the Terraces Subdivision, which consists of 263 lots and is zoned Planned Development District 93 (PD-93).

East: Directly east of the subject property is part of the Lee Acres Addition, which consists of four (4) lots and is partially zoned Agricultural (AG) District. Beyond this is the boundary for the city limits of the City of Rockwall. East of this, there are multiple residential properties situated within the City's Extraterritorial Jurisdiction (ETJ).

West: Directly west of the subject property is a 4.00-acre tract of vacant land (i.e. Tract 22-07 of the W.M. Dalton Survey, Abstract No. 72). Beyond this is the Maytona Ranch Estates Subdivision, which was established on September 26, 1983, consists of 19 residential lots, and is 100% developed. All of these properties are zoned Agricultural (AG) District. West of this is the boundary for the city limits of the City of Rockwall. there are multiple residential properties situated within the City's Extraterritorial Jurisdiction (ETJ). Beyond this, there are multiple residential properties situated within the City's Extraterritorial Jurisdiction (ETJ).

CHARACTERISTICS OF THE REQUEST AND CONFORMANCE TO THE CITY'S CODES

Article 13, *Definitions*, of the Unified Development Code (UDC) defines *Residential Infill in or Adjacent to an Established Subdivision* as “(t)he new development of a single-family home or duplex on an existing vacant or undeveloped parcel of land or the redevelopment of a developed parcel of land for a new single-family home or duplex within an established subdivision that is mostly or entirely built-out.” An *established subdivision* is further defined in Subsection 02.03(B) (11) of Article 04, *Permissible Uses*, of the UDC as “...a subdivision that consists of five (5) or more lots, that is 90.00% developed, and that has been in existence for more than ten (10) years.” In this case, the subject property is near the Maytona Ranch Estates Subdivision, which is 100% developed, consists of 19 residential lots, and has been in existence since September 26, 1983. The *Permissible Use Charts* contained in Article 04, *Permissible Uses*, of the UDC, requires a Specific Use Permit (SUP) for *Residential Infill in or Adjacent to an Established Subdivision* in all single-family zoning districts, the Two-Family (2F) District, the Downtown (DT) District, and the Residential-Office (RO) District. This property, being adjacent to an established subdivision, requires a Specific Use Permit (SUP).

In addition, Subsection 02.03(B)(11) of Article 04, *Permissible Uses*, of the UDC states that, “...the Planning and Zoning Commission and City Council shall consider the proposed size, location, and architecture of the home compared to the existing housing ... [and] (a)ll housing proposed under this section [i.e. *Residential Infill in or Adjacent to an Established Subdivision*] shall be constructed to be architecturally and visually similar or complimentary to the existing housing ...” The following is a summary of observations concerning the housing along Cornelius Road compared to the house proposed by the applicant:

| Housing Design and Characteristics | Existing Housing along Cornelius Road | Proposed Housing |
|------------------------------------|--|---|
| Building Height | One (1) and Two (2) Story | One (1) Story |
| Building Orientation | All homes face onto the street they front on. | The front elevation of the home will face onto Cornelius Road. |
| Year Built | 1880- 2018 | N/A |
| Building SF on Property | 2450 SF – 4,507 SF | 4,862 SF |
| Building Architecture | Single Family Homes, Two (2) Barns, and Multiple Vacant Lots | Comparable Architecture to the surrounding Single-Family Homes |
| Building Setbacks: | | |
| Front | 40-Feet | X>40-Feet |
| Side | 6- feet | X>6-Feet |
| Rear | 10-Feet | X>10-Feet |
| Building Materials | Brick, Siding and Metal | Wood, Stone, and Stucco |
| Paint and Color | Gray, Red & White | N/A |
| Roofs | Metal Roofs | Tile and TPO Roof |
| Driveways/Garages | Driveways all front the same street the single-family home faces. Homes consist of detached garages or no garages. | The proposed garage will be a front entry garage and will face onto Cornelius Road. |

Staff should note that the proposed single-family home meets all of the density and dimensional requirements for the Agricultural (AG) District as stipulated by the Unified Development Code (UDC). For the purpose of comparing the proposed home to the existing single-family housing located adjacent to or in the vicinity of the subject property, staff has provided photos of the properties along Cornelius Road and also provided the proposed building elevations in the attached packet. The approval of this request is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission and a finding that the proposed house will not have a negative impact on the existing subdivision.

NOTIFICATIONS

On March 18, 2025, staff mailed 82 notices to property owners and occupants within 500-feet of the subject property. There are no Homeowners Associations (HOAs) within 1,500-feet of the subject property participating in the Neighborhood Notification Program. Additionally, staff posted a sign on the subject property, and advertised the public hearings in the Rockwall Herald Banner as required by the Unified Development Code (UDC). At the time this report was drafted, staff had not received any notices back regarding the applicant's request.

CONDITIONS OF APPROVAL

If City Council chooses to approve of the applicant's request for a Specific Use Permit (SUP) for *Residential Infill in an Established Subdivision*, then staff would propose the following conditions of approval:

- (1) The applicant shall be responsible for maintaining compliance with the operational conditions contained in the Specific Use Permit (SUP) ordinance and which are detailed as follows:
 - (a) Development of the *Subject Property* shall generally conform to the *Residential Plot Plan* as depicted in *Exhibit 'B'* of the draft ordinance.
 - (b) Construction of a single-family home on the *Subject Property* shall generally conform to the *Building Elevations* depicted in *Exhibit 'C'* of the draft ordinance.
 - (c) Once construction of the single-family home has been completed, inspected, and accepted by the City, the Specific Use Permit (SUP) shall expire, and no further action by the property owner shall be required.
- (2) Any construction resulting from the approval of this Specific Use Permit (SUP) shall conform to the requirements set forth by the Unified Development Code (UDC), the International Building Code (IBC), the Rockwall Municipal Code of Ordinances, city adopted engineering and fire codes and with all other applicable regulatory requirements administered and/or enforced by the state and federal government.

PLANNING AND ZONING COMMISSION

On April 15, 2025, the Planning and Zoning Commission approved a motion to recommend approval of the Specific Use Permit (SUP) by a vote of 6-0, with Commissioner Thompson absent.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY
PLANNING & ZONING CASE NO.

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING:

CITY ENGINEER:

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST (SELECT ONLY ONE BOX):

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE)¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE)¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE)¹
- REPLAT (\$300.00 + \$20.00 ACRE)¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE)¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE)¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE)^{1 & 2}
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE)¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00)²

NOTES:

¹: IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.
²: A \$1,000.00 FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION (PLEASE PRINT)

ADDRESS 588 Cornelius Rd

SUBDIVISION

LOT

BLOCK

GENERAL LOCATION North Stodghill Rd & Cornelius Rd

ZONING, SITE PLAN AND PLATTING INFORMATION (PLEASE PRINT)

CURRENT ZONING Agriculture

CURRENT USE Agriculture

PROPOSED ZONING Residential

PROPOSED USE Residential Home

ACREAGE 10

LOTS [CURRENT] 1

LOTS [PROPOSED] 1

- SITE PLANS AND PLATS:** BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION (PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED)

OWNER HALLIE FLEMING

APPLICANT SPRUILL HOMES

CONTACT PERSON

CONTACT PERSON BRANDON SPRUILL

ADDRESS 508 N ALAMO RD
ROCKWALL, TX

ADDRESS 1205 VALLEY TRAIL

CITY, STATE & ZIP 75087

CITY, STATE & ZIP HEATH, TX 75032

PHONE 214 763 1123

PHONE 214 226 7796

E-MAIL

E-MAIL BRANDON@SPRUILLHOMES.COM

NOTARY VERIFICATION (REQUIRED)

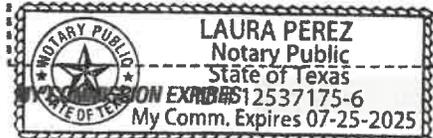
BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Hallie Fleming [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 215 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE _____ DAY OF _____, 20____. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 12th DAY OF March, 2025.

OWNER'S SIGNATURE

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS





Z2025-013: Specific Use Permit (SUP) for Residential Infill at 588 Cornelius Road

Case Location Map = 



City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75087
 (P): (972) 771-7745
 (W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.

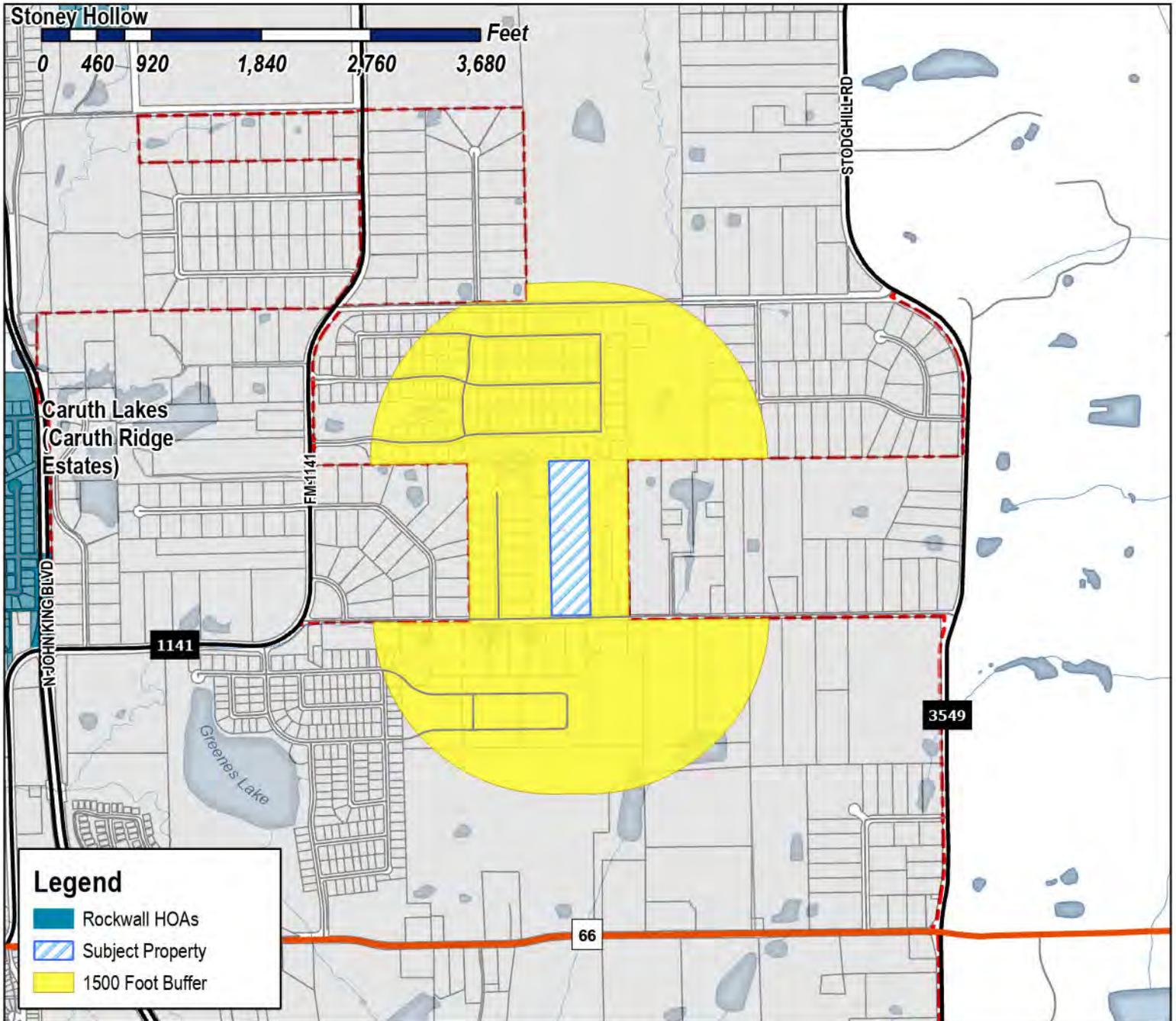




City of Rockwall

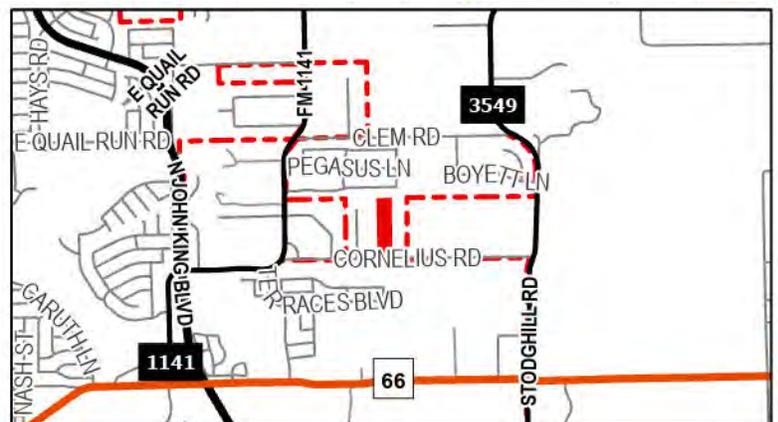
Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

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Case Number: Z2025-013
Case Name: Specific Use Permit (SUP) for Residential Infill
Case Type: Zoning
Zoning: Agricultural (AG) District
Case Address: 588 Cornelius Road

Date Saved: 3/14/2025
 For Questions on this Case Call (972) 771-7745

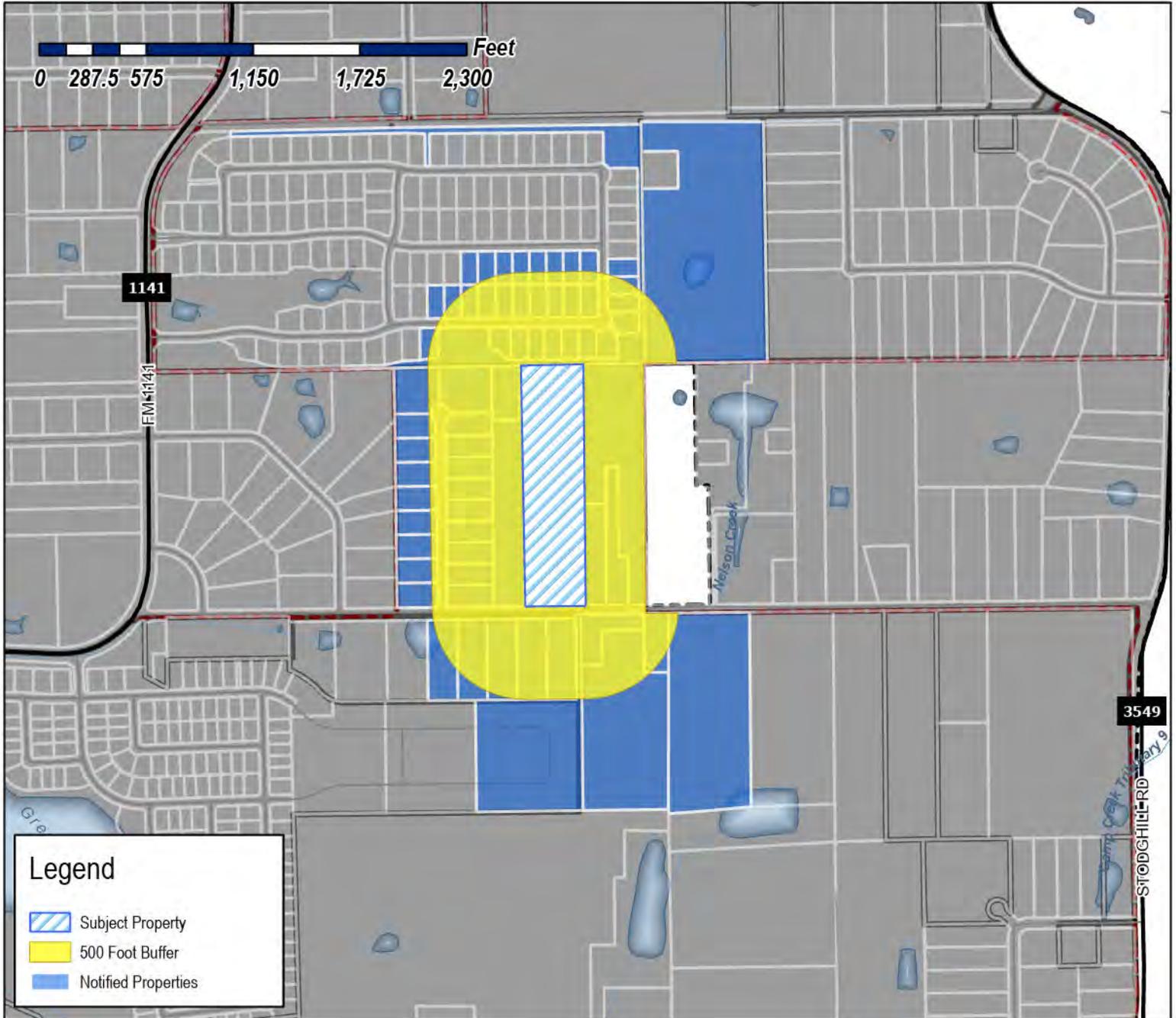




City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

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Case Number: Z2025-013
Case Name: Specific Use Permit (SUP) for Residential Infill
Case Type: Zoning
Zoning: Agricultural (AG) District
Case Address: 588 Cornelius Road

Date Saved: 3/14/2025

For Questions on this Case Call: (972) 771-7745



WINDING CREEK ROCKWALL HOMEOWNERS
ASSOCIATION
1024 S GREENVILLE AVE STE 230
ALLEN, TX 75002

THOMAS STEPHEN R & SHARON C
1200 MARILYN JAYNE DR
ROCKWALL, TX 75087

RANDY AND KAY WILSON REVOCABLE TRUST
RANDY SCOTT WILSON AND KAY MARIE
WILSON- CO TRUSTEES
1201 MARILYN JAYNE
ROCKWALL, TX 75087

JAMES BRIAN & KIMBERLY
1202 MARILYN JAYNE DR
ROCKWALL, TX 75087

SMITH DANIEL T
1203 MARILYN JAYNE DR
ROCKWALL, TX 75087

WALKER GRACIE & LEON
1204 MARILYN JAYNE DR
ROCKWALL, TX 75087

JAMES DAVID L
1205 MARILYN JAYNE DRIVE
ROCKWALL, TX 75087

COOK ROBERT M ETUX
1206 MARILYN JAYNE DR
ROCKWALL, TX 75087

CANTRELL CARL DEAN
1207 MARILYN JAYNE DR
ROCKWALL, TX 75087

RODRIGUEZ CONRADO JR
1208 MARILYN JAYNE DR
ROCKWALL, TX 75087

BARTON FORRIS WOODROW JR & DIXIE D
1209 MARILYN JAYNE DR
ROCKWALL, TX 75087

EMRA JOHN AND SHERYL AND
PAUL AND KATIE EMRA
1210 MARILYN JAYNE LANE
ROCKWALL, TX 75087

RYLANDER GREGORY L & PAMELA J
1211 MARILYN JAYNE
ROCKWALL, TX 75087

CONFIDENTIAL
1212 MAYILYN JAYNE LN
ROCKWALL, TX 75087

CONFIDENTIAL
1213 MARILYN JAYNE LN
ROCKWALL, TX 75087

RESIDENT
1214 MARILYN JAYNE LN
ROCKWALL, TX 75087

CONFIDENTIAL
1215 MARILYN JAYNE DRIVE
ROCKWALL, TX 75087

COUCH MITCHELL E AND PATRICIA M
1216 MARILYN JAYNE DR
ROCKWALL, TX 75087

KELLY JAMES B & DEA S
1217 MARILYN JAYNE DR
ROCKWALL, TX 75087

MENDOZA ROBERT E AND SANDRA J WILKINS
1218 MARILYN JAYNE DR
ROCKWALL, TX 75087

RESIDENT
1400 QUASAR DR
ROCKWALL, TX 75087

RESIDENT
1404 QUASAR DR
ROCKWALL, TX 75087

RESIDENT
1408 QUASAR DR
ROCKWALL, TX 75087

RESIDENT
1412 QUASAR DR
ROCKWALL, TX 75087

SINGH GAGANPREET K AND PAWANPREET
151 STEVENSON DR
FATE, TX 75087

RESIDENT
1518 QUASAR DR
ROCKWALL, TX 75087

MCCALLUM CRAIG WILLIAM
1983 N STODGHILL RD
ROCKWALL, TX 75087

RESIDENT
2207 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2208 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2212 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2215 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2216 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2217 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2220 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2221 PEGASUS LN
ROCKWALL, TX 75087

FORMAN LINDSEY & TANNER
2221 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2301 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2302 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2303 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2305 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2306 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2307 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2309 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2310 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2311 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2313 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2314 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2315 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2317 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2318 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2319 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2321 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2322 PHOENIX LN
ROCKWALL, TX 75087

RESIDENT
2323 PEGASUS LN
ROCKWALL, TX 75087

RESIDENT
2325 PHOENIX LN
ROCKWALL, TX 75087

SHADDOCK HOMES LTD
2400 Dallas Pkwy STE 560
Plano, TX 75093

LEE GREGORY P & LAUREN E
2908 PRESTON TRL
ROCKWALL, TX 75087

RESIDENT
401 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
451 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
487 CORNELIUS RD
ROCKWALL, TX 75087

FLEMING HALLIE B
508 N ALAMO RD
ROCKWALL, TX 75087

RESIDENT
520 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
525 CORNELIUS RD
ROCKWALL, TX 75087

WINDSOR HOMES CUMBERLAND LLC
5310 Harvest Hill Rd Ste 162
Dallas, TX 75230

RESIDENT
555 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
588 CORNELIUS RD
ROCKWALL, TX 75087

PEOPLES DONNIE
589 CORNELIUS
ROCKWALL, TX 75087

PEOPLES DONNIE
589 CORNELIUS
ROCKWALL, TX 75087

PEOPLES DONNIE
589 CORNELIUS
ROCKWALL, TX 75087

RESIDENT
600 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
611 CLEM RD
ROCKWALL, TX 75087

RESIDENT
614 CORNELIUS RD
ROCKWALL, TX 75087

LEE JAMES H AND BARBARA
628 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
657 CORNELIUS RD
ROCKWALL, TX 75087

RESIDENT
657 CORNELIUS RD
ROCKWALL, TX 75087

PEWITT RONNY M & JENNIFER L
668 CORNELIUS RD
ROCKWALL, TX 75087

PERKINS RALPH TRENT & AMY CAIN
701 Cornelius Rd
Rockwall, TX 75087

FALCON PLACE SF LTD
8214 Westchester Dr Ste 900
Dallas, TX 75225

RESIDENT
839 CORNELIUS RD
ROCKWALL, TX 75087

SOUTHALL MARK P & KATHY L
P. O. BOX 2214
ROCKWALL, TX 75087

ESTATE OF MICHAEL L PEOPLES SR
ANDREA DANLEY - INDEPENDENT EXECUTOR
PO Box 154
Fate, TX 75132

ESTATE OF MICHAEL L PEOPLES SR
PO Box 154
FATE, TX 75132

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

Property Owner and/or Resident of the City of Rockwall:

You are hereby notified that the City of Rockwall Planning and Zoning Commission and City Council will consider the following application:

Z2025-013: Specific Use Permit (SUP) for a Residential Infill

Hold a public hearing to discuss and consider a request by Brandon Spruill of Spruill Homes on behalf of Hallie Fleming for the approval of a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision* on a ten (10) acre tract of land identified Tract 22-02 of the W. M. Dalton Survey, Abstract No. 72, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, addressed as 588 Cornelius Road, and take any action necessary.

For the purpose of considering the effects of such a request, the Planning and Zoning Commission will hold a public hearing on Tuesday, April 15, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, April 21, 2025 at 6:00 PM. These hearings will be held in the City Council Chambers at City Hall, 385 S. Goliad Street.

As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Angelica Guevara
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, April 21, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

PLEASE RETURN THE BELOW FORM

Case No. Z2025-013: Specific Use Permit (SUP) for a Residential Infill

Please place a check mark on the appropriate line below:

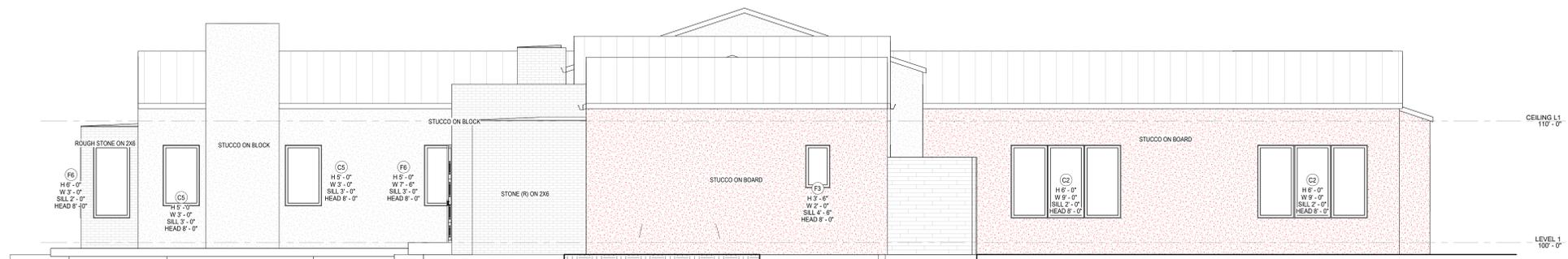
- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Four horizontal grey bars for providing reasons for support or opposition.

Name: [Grey box]
Address: [Grey box]

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

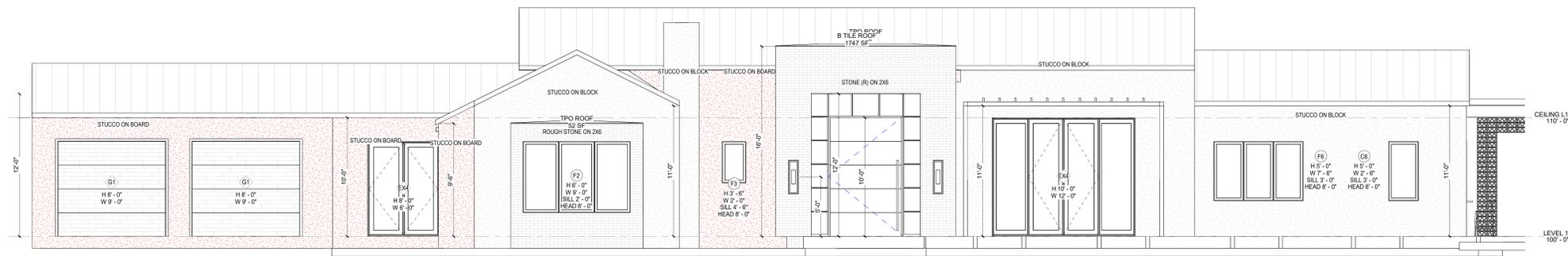
PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE



4 EAST ELEVATION
SCALE: 1/4" = 1'-0"



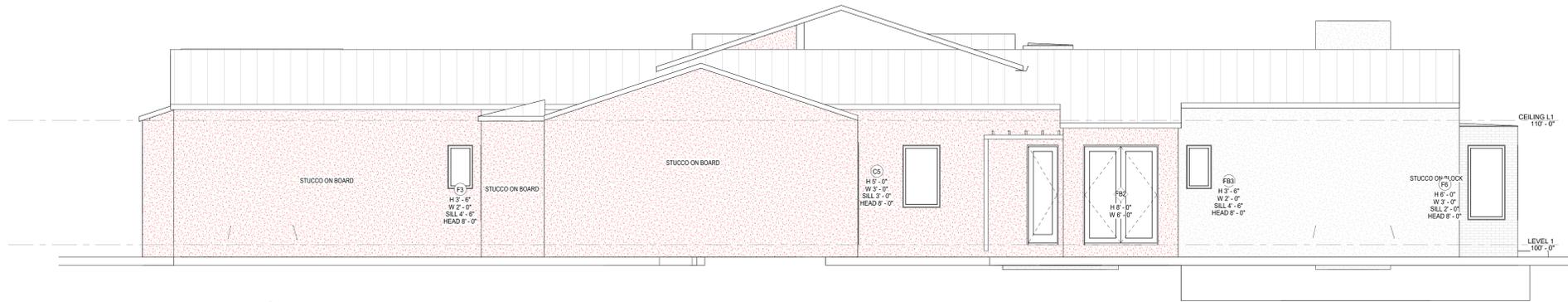
6 SOUTH ELEVATION 2
SCALE: 1/4" = 1'-0"



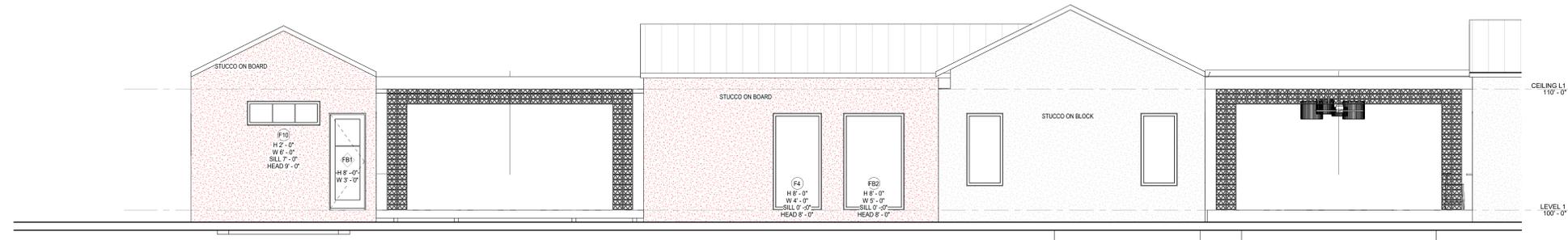
5 SOUTH ELEVATION 1
SCALE: 1/4" = 1'-0"



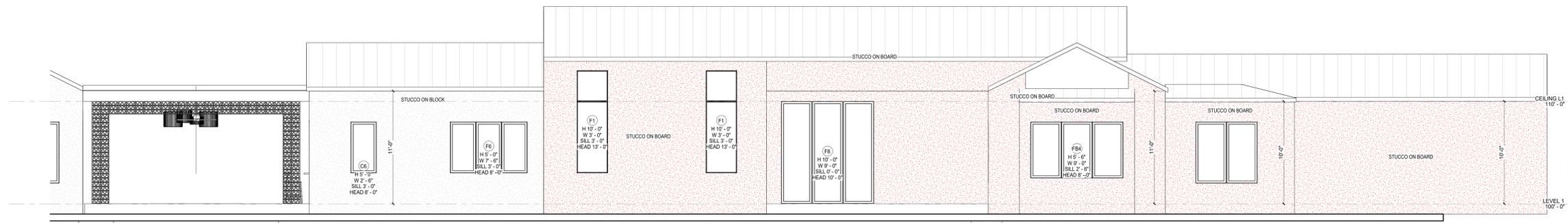
1 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



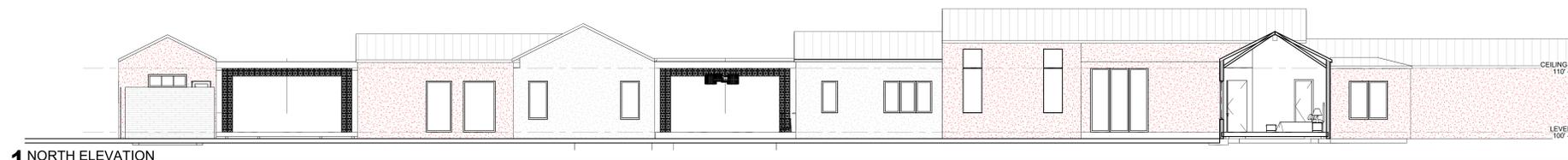
4 WEST ELEVATION
SCALE: 1/4" = 1'-0"



3 NORTH ELEVATION 2
SCALE: 1/4" = 1'-0"



2 NORTH ELEVATION 1
SCALE: 1/4" = 1'-0"



1 NORTH ELEVATION
SCALE: 1/8" = 1'-0"

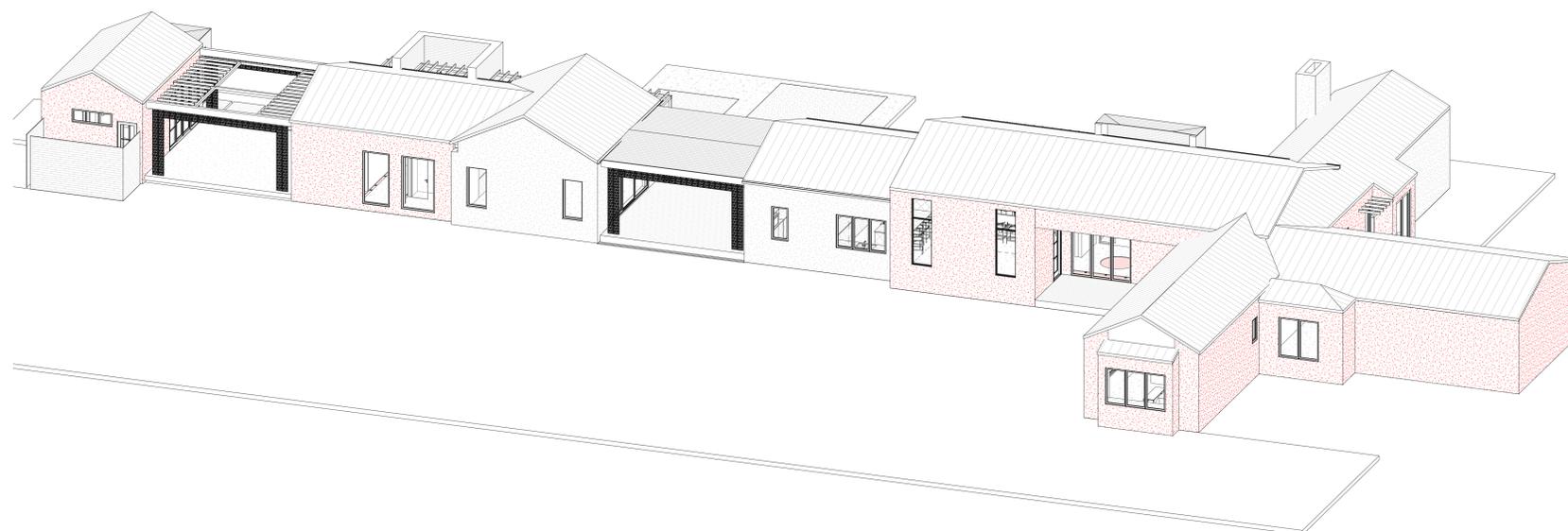
ISSUE FOR PERMIT | 04/04/2025
PROJECT ADDRESS | 588 CORNELIUS | ROCKWALL | TEXAS
OWNER | HALLIE DAVIDSON

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THESE DOCUMENTS HAVE BEEN
PREPARED SPECIFICALLY FOR
HALLIE DAVIDSON
THEY ARE NOT SUITABLE FOR USE ON
OTHER LOCATIONS WITHOUT THE
APPROVAL OF M·Gray A/E.

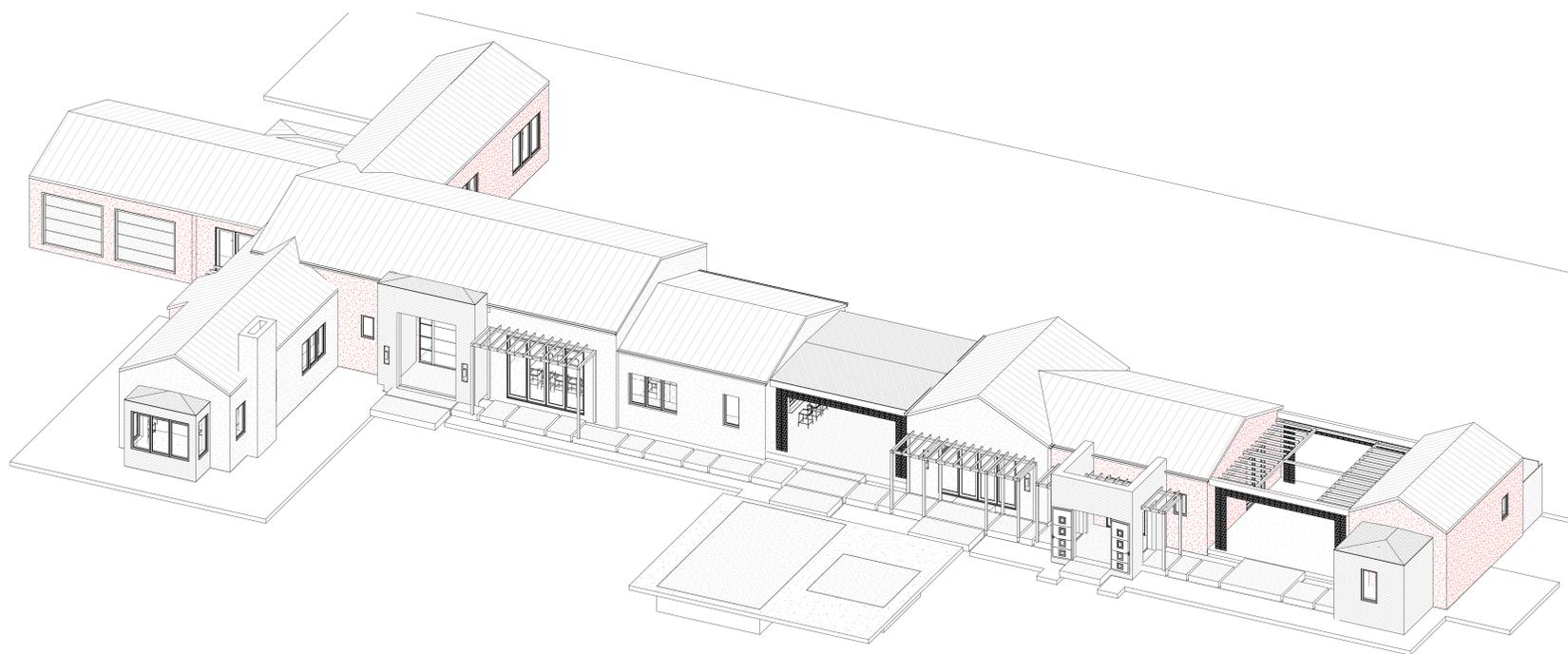
SCALE: As indicated

ELEVATIONS
A-502

SHEET SIZE: 30"x42" PRINTED FULL SIZE
4/4/2025 3:54:16 PM



2 ISO 2
SCALE



1 ISO 1
SCALE









04/04/2025

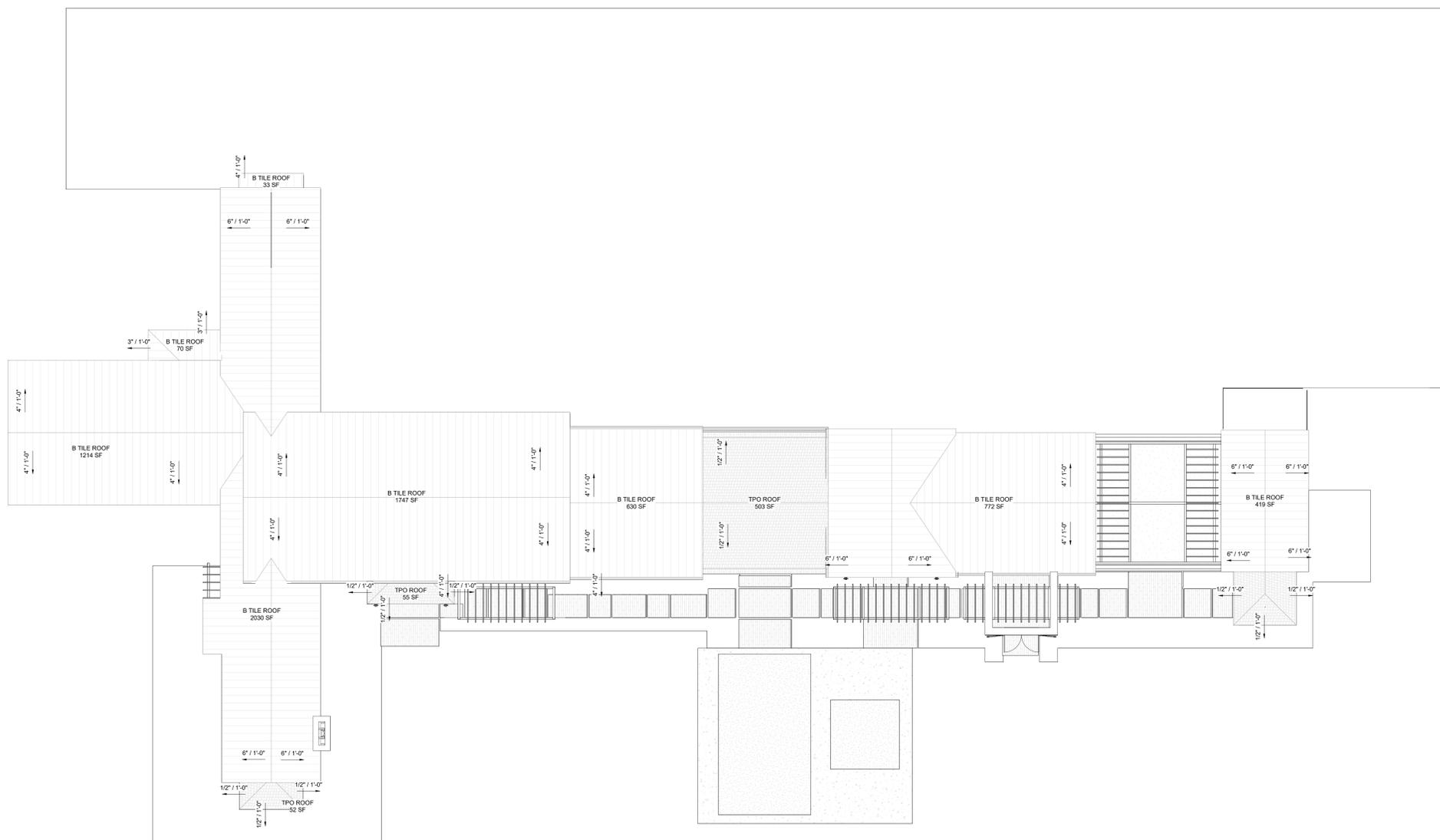
ARCHITECT OF RECORD
M:GRAY ARCHITECTURE
469-855-6275
mgray@mgray.com

INTERIOR DESIGNER

STRUCTURAL ENGINEER

LANDSCAPE ARCHITECT

CONTRACTOR



1 ROOF
SCALE: 1/8" = 1'-0"

ISSUE FOR PERMIT | 04/04/2025

PROJECT ADDRESS | 588 CORNELIUS | ROCKWALL | TEXAS

OWNER | HILLE BRANDSPORT

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HILLE BRANDSPORT
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OTHER LOCATIONS WITHOUT THE
APPROVAL OF M:GRAY, P.C.

SCALE: 1/8" = 1'-0"

ROOF PLAN
A-310



04/04/2025

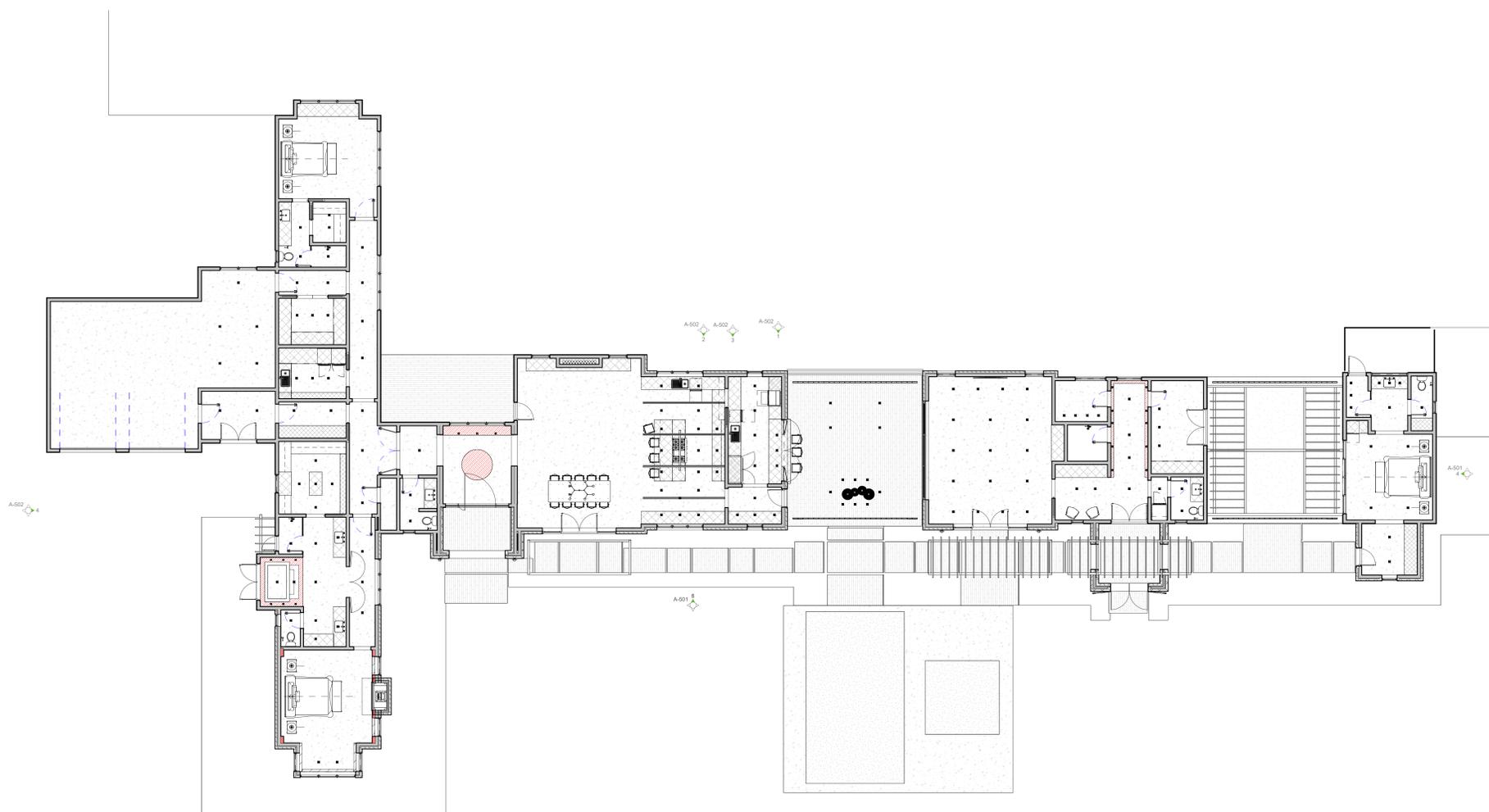
ARCHITECT OF RECORD
M:GRAY ARCHITECTURE
469-855-6275
mgray@mgray.com

INTERIOR DESIGNER

STRUCTURAL ENGINEER

LANDSCAPE ARCHITECT

CONTRACTOR



1 LEVEL 1 OVERALL
SCALE: 1/8" = 1'-0"

ISSUE FOR PERMIT | 04/04/2025
PROJECT ADDRESS | 588 CORNELIUS | ROCKWALL | TEXAS
OWNER | HILLE DANKERFORT

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HILLE DANKERFORT
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OTHER LOCATIONS WITHOUT THE
APPROVAL OF M:GRAY ARCHITECTURE

SCALE: 1/8" = 1'-0"

LEVEL 1 PLAN
A-201



04/04/2025

ARCHITECT OF RECORD
M:GRAY ARCHITECTURE
469-855-6275
mgray@mgray.com

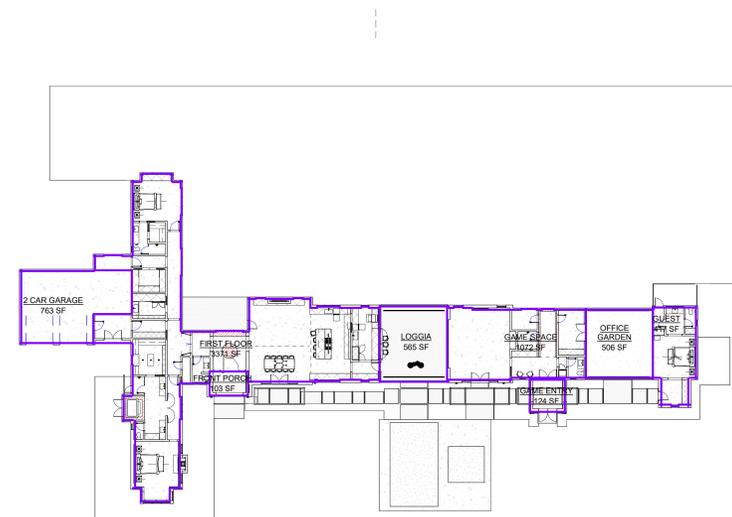
INTERIOR DESIGNER

STRUCTURAL ENGINEER

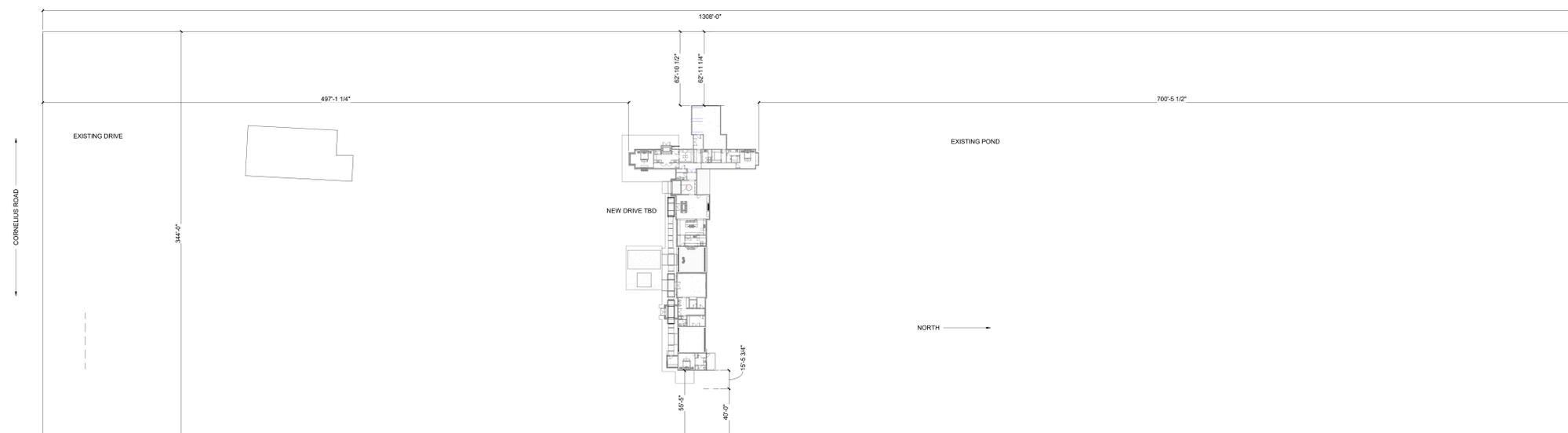
LANDSCAPE ARCHITECT

CONTRACTOR

| BUILDING AREA | | | |
|---------------------|---------------|---------|----------|
| Level | Name | Area | Comments |
| LEVEL 1 | GAME SPACE | 1072 SF | AC |
| LEVEL 1 | GUEST | 417 SF | AC |
| LEVEL 1 | FIRST FLOOR | 3371 SF | AC |
| AC | | 4860 SF | |
| LEVEL 1 | 2 CAR GARAGE | 763 SF | NDN AC |
| LEVEL 1 | LOGGIA | 565 SF | NDN AC |
| LEVEL 1 | FRONT PORCH | 193 SF | NDN AC |
| LEVEL 1 | OFFICE GARDEN | 506 SF | NDN AC |
| LEVEL 1 | GAME ENTRY | 124 SF | NDN AC |
| NON AC | | 2061 SF | |
| TOTAL UNDER ROOF- 8 | | 6822 SF | |



1 LEVEL 1
SCALE: 1" = 20'-0"



3 SITE
SCALE: 1" = 40'-0"



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

ADJACENT HOUSING ATTRIBUTES

| ADDRESS | HOUSING TYPE | YEAR BUILT | HOUSE SF | ACCESSORY BUILDING | EXTERIOR MATERIALS |
|--------------------|--------------------|------------|----------|--------------------|--------------------|
| 401 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 451 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 487 Cornelius Road | Barn | 1985 | 7,986 | N/A | N/A |
| 520 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 525 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 555 Cornelius Road | Single-Family Home | 1960 | 2,450 | 592 | Siding |
| 588 Cornelius Road | Barn | 2012 | 2,700 | 80 | Metal |
| 589 Cornelius Road | Single-Family Home | 1880 | 4,507 | 5380 | Siding |
| 614 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 628 Cornelius Road | Single-Family Home | 2018 | 2,632 | 2310 | Metal |
| 635 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| 657 Cornelius Road | Vacant | N/A | N/A | N/A | N/A |
| AVERAGES: | | 1971 | 4,055 | 2,091 | |



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



401 Cornelius Road



451 Cornelius Road



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



487 Cornelius Road



520 Cornelius Road



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

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525 Cornelius Road



555 Cornelius Road



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



588 Cornelius Road



589 Cornelius Road



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



614 Cornelius Road



628 Cornelius Road



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-013

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



635 Cornelius Road



657 Cornelius Road

CITY OF ROCKWALL

ORDINANCE NO. 25-XX

SPECIFIC USE PERMIT NO. S-3XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE (UDC) [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) FOR RESIDENTIAL INFILL IN AN ESTABLISHED SUBDIVISION TO ALLOW THE CONSTRUCTION OF A SINGLE-FAMILY HOME ON A TEN (10) ACRE PARCEL OF LAND, IDENTIFIED AS TRACT 22-02 OF THE W.M. DALTON SURVEY, ABSTRACT NO. 72, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; AND MORE SPECIFICALLY DESCRIBED AND DEPICTED IN EXHIBIT 'A' OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City has received a request by Brandon Spruill of Spruill Homes on behalf of Hallie Fleming for the approval of a Specific Use Permit (SUP) for Residential Infill in an Established Subdivision for the purpose of constructing a single-family home on a ten (10) acre parcel of land identified as Tract 22-02 of the W.M. Dalton Survey, Abstract No. 72, City of Rockwall, Rockwall County, Texas, zoned Agricultural (AG) District, addressed as 588 Cornelius Road, and being more specifically described and depicted in *Exhibit 'A'* of this ordinance, which herein after shall be referred to as the *Subject Property* and incorporated by reference herein; and

WHEREAS, the Planning and Zoning Commission of the City of Rockwall and the governing body of the City of Rockwall, in compliance with the laws of the State of Texas and the ordinances of the City of Rockwall, have given the requisite notices by publication and otherwise, and have held public hearings and afforded a full and fair hearing to all property owners generally, and to all persons interested in and situated in the affected area and in the vicinity thereof, the governing body in the exercise of its legislative discretion has concluded that the Unified Development Code (UDC) [*Ordinance No. 20-02*] of the City of Rockwall should be amended as follows:

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Rockwall, Texas;

SECTION 1. That the Unified Development Code (UDC) [*Ordinance No. 20-02*] of the City of Rockwall, as heretofore amended, be and the same is hereby amended so as to grant a Specific Use Permit (SUP) for Residential Infill in an Established Subdivision to allow for the construction of a single-family home in an established subdivision in accordance with Article 04, *Permissible Uses*, of the Unified Development Code (UDC) [*Ordinance No. 20-02*] on the *Subject Property*; and,

SECTION 2. That the Specific Use Permit (SUP) shall be subject to the requirements set forth in Subsection 03.01, *General Residential District Standards*, and Subsection 02.01, *Agricultural*

(AG) District, of Article 05, District Development Standards, of the Unified Development Code (UDC) [Ordinance No. 20-02] -- as heretofore amended and may be amended in the future -- and with the following conditions:

2.1 OPERATIONAL CONDITIONS

The following conditions pertain to the construction of a single-family home on the *Subject Property* and conformance to these operational conditions are required:

- 1) The development of the *Subject Property* shall generally conform to the Residential Plot Plan as depicted in *Exhibit 'B'* of this ordinance.
- 2) The construction of a single-family home on the *Subject Property* shall generally conform to the Building Elevations depicted in *Exhibit 'C'* of this ordinance.
- 3) Once construction of the single-family home has been completed, inspected, and accepted by the City of Rockwall, this Specific Use Permit (SUP) shall expire, and no further action by the property owner shall be required.

2.2 COMPLIANCE

Approval of this ordinance in accordance with Subsection 02.02, *Specific Use Permits (SUP)* of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC) will require the *Subject Property* to comply with the following:

- 1) Upon obtaining a *Building Permit*, should the contractor operating under the guidelines of this ordinance fail to meet the minimum operational requirements set forth herein and outlined in the Unified Development Code (UDC), the City may (*after proper notice*) initiate proceedings to revoke the Specific Use Permit (SUP) in accordance with Subsection 02.02(F), *Revocation*, of Article 11, *Development Applications and Revision Procedures*, of the Unified Development Code (UDC) [Ordinance No. 20-02].

SECTION 3. That the official zoning map of the City be corrected to reflect the changes in zoning described herein.

SECTION 4. That all ordinances of the City of Rockwall in conflict with the provisions of this ordinance be, and the same are hereby repealed to the extent of that conflict.

SECTION 5. Any person, firm, or corporation violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction shall be punished by a penalty of fine not to exceed the sum of *TWO THOUSAND DOLLARS (\$2,000.00)* for each offence and each and every day such offense shall continue shall be deemed to constitute a separate offense.

SECTION 6. If any section or provision of this ordinance or the application of that section or provision to any person, firm, corporation, situation or circumstance is for any reason judged invalid, the adjudication shall not affect any other section or provision of this ordinance or the application of any other section or provision to any other person, firm, corporation, situation or circumstance, and the City Council declares that it would have adopted the valid portions and applications of the ordinance without the invalid parts and to this end the provisions of this ordinance shall remain in full force and effect.

SECTION 7. That this ordinance shall take effect immediately from and after its passage.

PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS,
THIS THE 5th DAY OF MAY, 2025.

Trace Johannesen, *Mayor*

ATTEST:

Kristy Teague, *City Secretary*

APPROVED AS TO FORM:

Frank J. Garza, *City Attorney*

1st Reading: April 21, 2025

2nd Reading: May 5, 2025

**Exhibit 'A':
Location Map**

Address: 588 Cornelius Road

Legal Description: Tract 22-02 of the W. M. Dalton Survey, Abstract No. 72

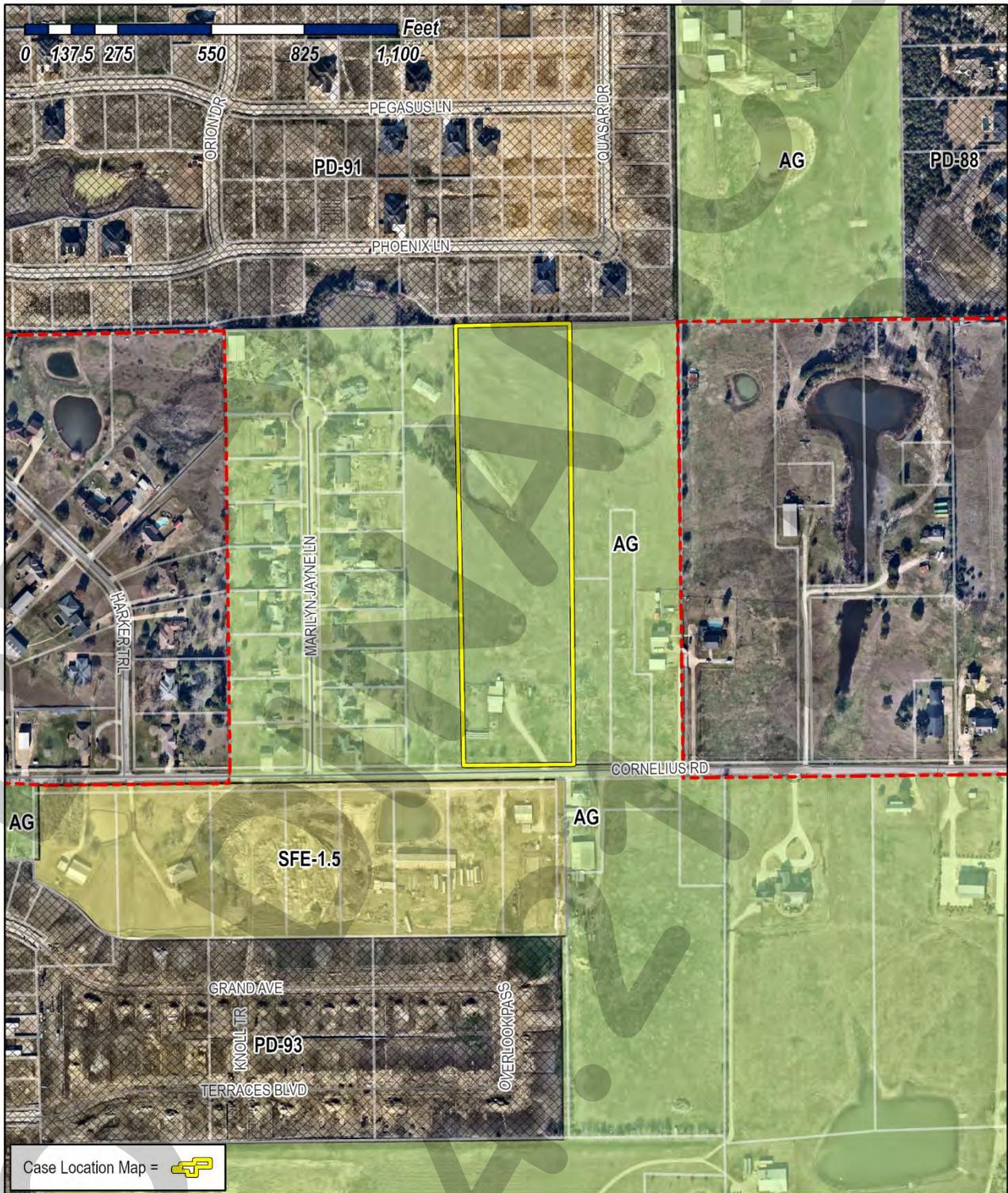


Exhibit 'B':
Residential Plot Plan

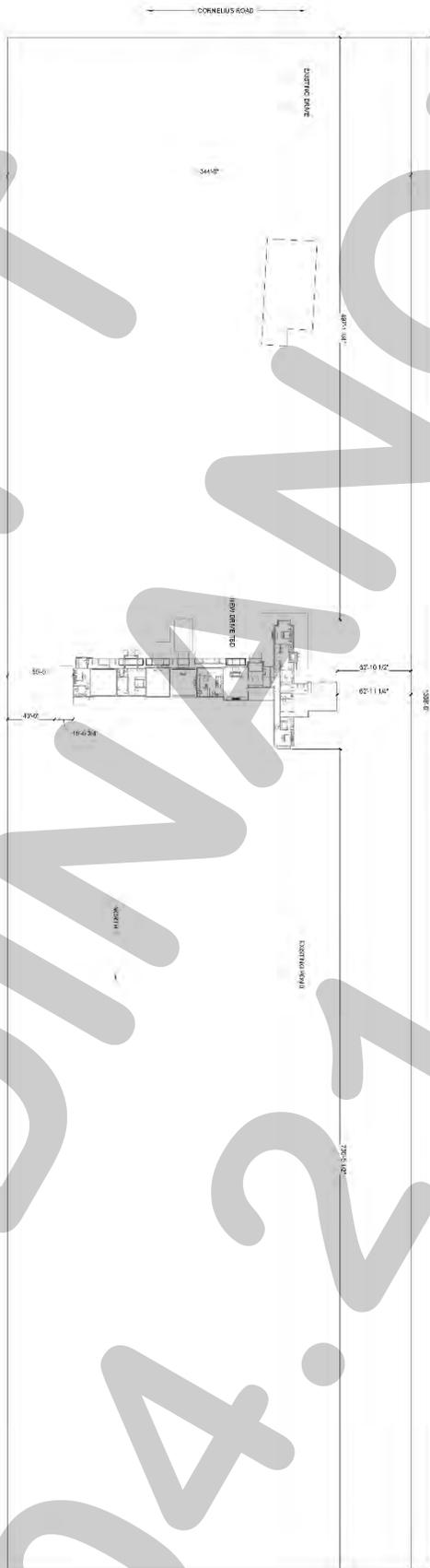
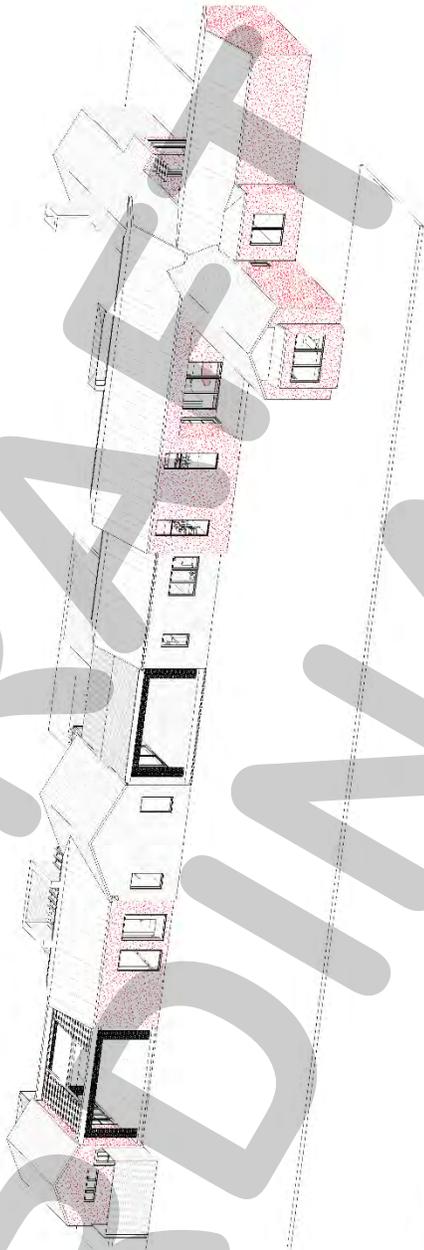


Exhibit 'C':
Building Elevations



2
ISO 2

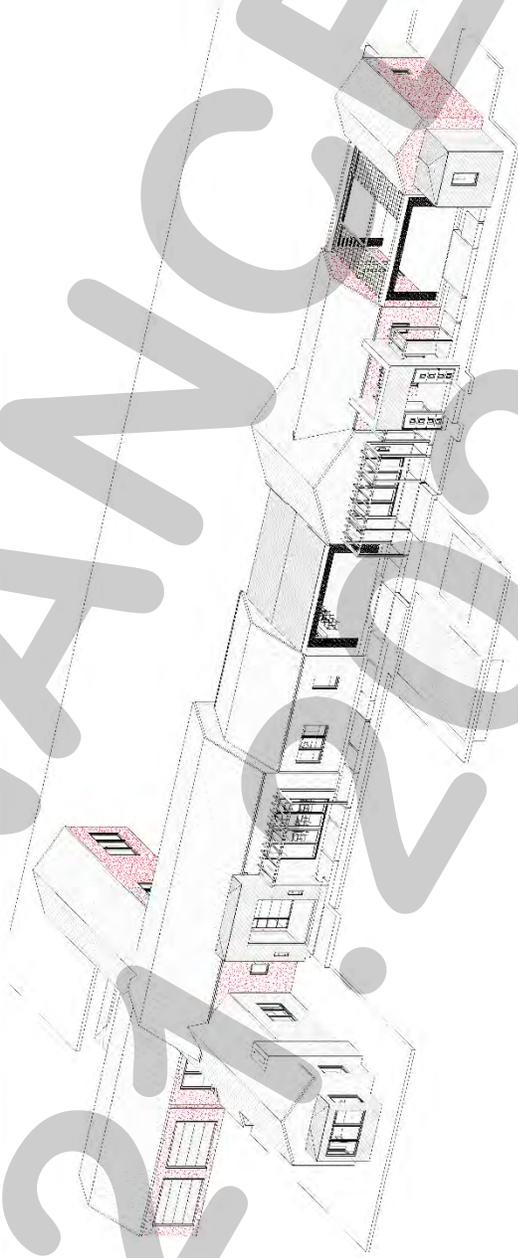
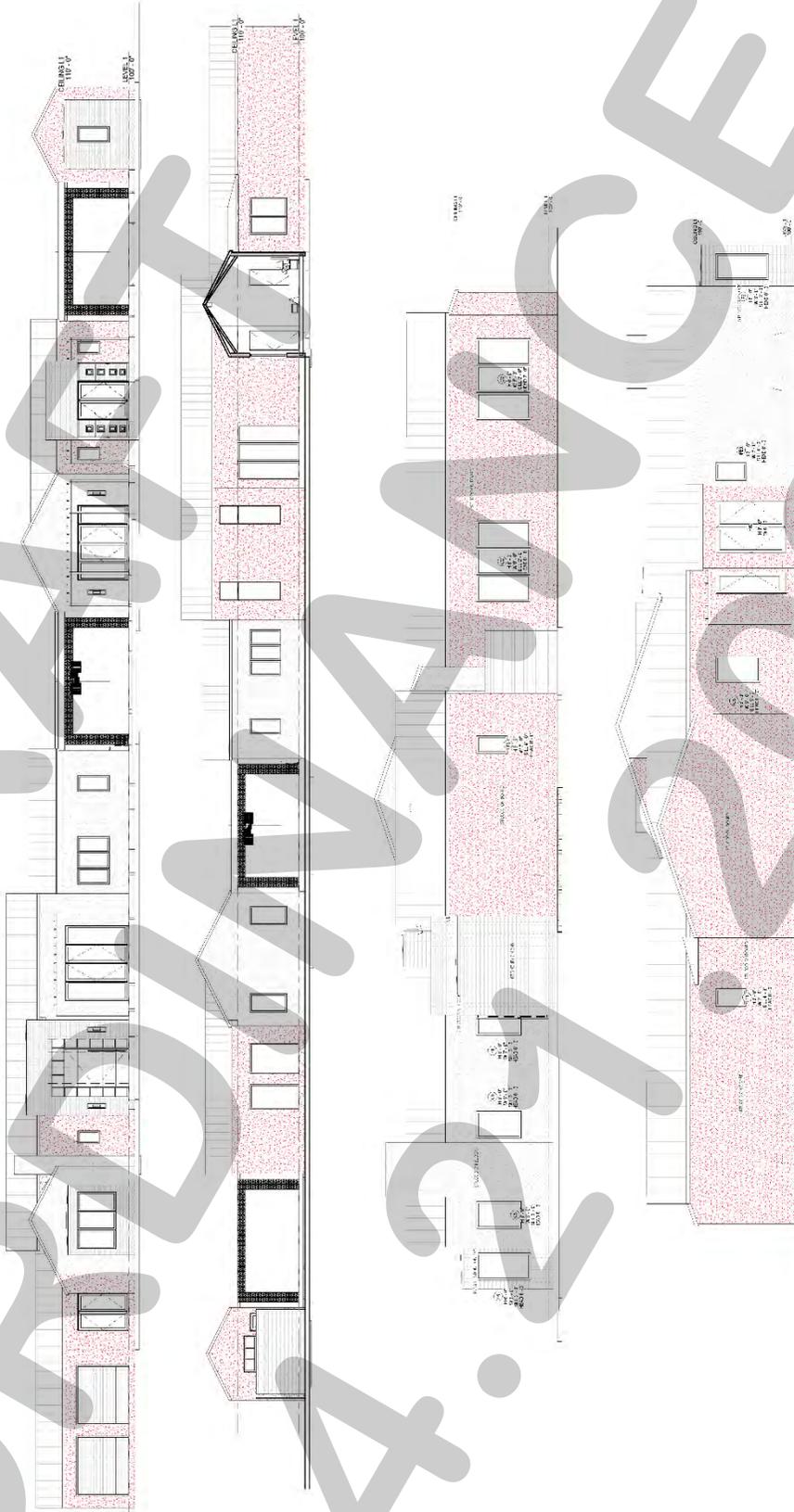


Exhibit 'C':
Building Elevations





CITY OF ROCKWALL

CITY COUNCIL CASE MEMO

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
DATE: April 21, 2025
APPLICANT: Alexander Trujillo
CASE NUMBER: Z2025-014; *Specific Use Permit for a Minor Automotive Repair Garage*

SUMMARY

Hold a public hearing to discuss and consider a request by Alexander Trujillo for the approval of a *Specific Use Permit (SUP)* for *Minor Automotive Repair Garage* on a 2.692-acre parcel of land identified Lot 5, Block A, Platinum Storage Addition, City of Rockwall, Rockwall County, Texas, zoned Commercial (C) District, situated within the IH-30 Overlay (IH-30 OV) District, addressed as 1460 T. L. Townsend Drive, Suite 116, and take any action necessary.

BACKGROUND

The subject property was annexed on September 5, 1960 by *Ordinance No. 60-02 [i.e. Case A1960-002]*. On July 7, 2014, the City Council approved *Ordinance No. 14-25 [i.e. Case No. Z2014-012]* allowing a mini-warehouse use on the property that contained the subject property. On May 4, 2015, the subject property was platted as Lot 1, Block 1, Platinum Storage Addition as part of *Case No. P2015-013*. On December 21, 2015, the City Council approved a Replat [*i.e. Case No. P2015-041*] to subdivide the existing lot into two (2) parcels of land [*i.e. Lots 2 & 3, Block A, Platinum Storage Addition*] that established the subject property. On June 28, 2016, the Planning and Zoning Commission approved a Site Plan [*i.e. Case No. SP2016-014*] to allow the construction of two (2) office/warehouses on the subject property. On July 5, 2016, the City Council approved variances [*i.e. four (4)-sided architecture and secondary material requirements*] in conjunction with an approved Site Plan [*i.e. Case No. SP2016-014*] for two (2) proposed buildings on the subject property. Currently situated on the subject property are two (2) office/warehouse facilities, consisting of one (1) 10,900 SF building and one (1) 8,680 SF building.

PURPOSE

The applicant -- *Alexander Trujillo* -- is requesting the approval of a Specific Use Permit (SUP) to allow a *Minor Automotive Repair Garage* in a Commercial (C) District on the subject property.

ADJACENT LAND USES AND ACCESS

The subject property is addressed as 1460 T.L. Townsend Drive. The land uses adjacent to the subject property are as follows:

North: Directly north of the subject property is a five (5) acre parcel of land (*i.e. Lot 2, Block A, Park Place Business Centre*) that is developed with a 74,660 SF *Commercial Indoor Amusement* facility (*i.e. Shenanigans*). This property is zoned Light Industrial (LI) District. Beyond this is the eastbound lanes of the IH-30 Frontage Road, followed by the main lanes of IH-30, and the westbound lanes of the IH-30 Frontage Road. Following this is a 4.194-acre parcel of land (*i.e. Lot 1, Block A, Emerus Emergency Hospital Addition*) that is zoned Light Industrial (LI) District and is developed with a *Hospital* (*i.e. Baylor Emerus Emergency*).

South: Directly south of the subject property is a 10.062-acre tract of land (*i.e. Tract 2-4, Abstract 65, of the J Cadle Survey*), which is vacant and is zoned Commercial (C) District. Beyond this is Old SH-276 which is identified as a *Minor Collector* on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Beyond this thoroughfare is an 8.24-acre parcel of land (*i.e. Lot 8, Block 1, Meadowcreek Business Center*).

Phase 2), which is developed with a 5,110 SF *Retail Store with Gasoline Sales* (i.e. *Racetrac*) that is zoned Commercial (C) District.

East: Directly east of the subject property is a 3.0047-acre tract of land (i.e. *Tract 2-2 of the J. Cadle Survey, Abstract No. 65*) owned by the City of Heath. Beyond this is T. L. Townsend Drive, which is identified as a A4D (i.e. *arterial, four [4] lane, divided roadway*) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Beyond this is a 4.967-acre parcel of land (i.e. *Lot 4, Bodin Industrial Tract Addition*) owned by the City of Rockwall. Beyond this parcel are several parcels of land developed with industrial land uses that make up the remainder of the Bodin Industrial Tract Subdivision. All of these properties are zoned Light Industrial (LI) District.

West: Directly west of the subject property is a 2.857-acre parcel of land (i.e. *Lot 4, Block A, Platinum Storage Addition*) developed with a *Mini-Warehouse Facility*. South of this is a 20.2904-acre parcel of land (i.e. *Lot 1R, Block 1, Rockwall Centre Corners Addition*) developed with a wholesale grocery store (i.e. *Costco*). All of these properties are zoned Commercial (C) District.

MAP 1: LOCATION MAP
YELLOW: SUBJECT PROPERTY



CHARACTERISTICS OF THE REQUEST

The applicant has submitted an application and a zoning exhibit requesting a Specific Use Permit (SUP) for a *Minor Automotive Repair Garage* on the subject property. According to the Rockwall Central Appraisal District (RCAD) there is an existing 61,980 SF multi-tenant industrial building situated on the subject property. Within this building the applicant is proposing to lease a 1,545 SF space for their *Minor Automotive Repair Garage*, which will provide vehicle window tinting and vinyl wraps by appointment only. As part of the applicant's operations, they will provide same day service and no vehicles will be stored outside overnight. Given this, staff has provided a condition of approval that the *Minor Automotive Repair Garage* shall not have any *Outside Storage* associated with the use. Staff has included all of the applicant's submitted materials in the attached packet for the Planning and Zoning Commission and City Council's review.

CONFORMANCE WITH THE CITY'S CODES

Subsection 02.01(F), *Retail and Personal Service Land Uses*, of Article 13, *Definitions*, of the Unified Development Code (UDC), defines a *Minor Automotive Repair Garage* as "(m)inor repair or replacement of parts, tires, tubes, and batteries; diagnostic services; minor motor services such as grease, oil spark plugs, and filter changing; tire alignment; tune-ups, emergency road service; replacement of starters, alternators, hoses, brake parts, mufflers; performing state inspections and making minor repairs necessary to pass said inspection; servicing of air-conditioning systems, and similar minor services for motor vehicles except heavy land vehicles, but not including any operation named under *Automobile Repair, Major*, or any other similar use. All work must be performed inside an enclosed building. Vehicles shall not be stored on site for longer than 14 days." In this case, the applicant's proposed use falls under this classification, and all of the work will be performed within an enclosed area with no vehicles being stored on site. According to the *Permissible Use Charts* contained in Article 04, *Permitted Uses*, of the Unified Development Code (UDC), a *Minor Automotive Repair Garage* requires a Specific Use Permit (SUP) in a Commercial (C) District. The purpose of this requirement is to acknowledge that the *Minor Automotive Repair Garage* land use is not appropriate within all of the City's commercial areas, and that the City Council should have discretionary oversight with regard to this land use and their impacts within these types of districts.

STAFF ANALYSIS

According to Subsection 02.02, *Specific Use Permits (SUP)*, of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC), "(t)he purpose of a Specific Use Permit (SUP) is to allow discretionary consideration of certain uses that would typically be considered incompatible within certain locations of a zoning district." In review of the surrounding land uses, many of the uses within the current multi-tenant building include office uses. Based on this, the proposed *Minor Automotive Repair Garage* will not be the typical use within this building; however, the applicant has indicated that the customers for the *Minor Automotive Repair Garage* will be by appointment only and will have similar business operations as an office suite. All that being said, the approval of a Specific Use Permit (SUP) is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission.

OURHOMETOWN VISION 2040 COMPREHENSIVE PLAN

According to the *Land Use Plan* contained in the OURHometown Vision 2040 Comprehensive Plan, the subject property is situated within the *IH-30 Corridor District*. This district is composed primarily of a *Special Commercial Corridor (SC)*. The *IH-30 Corridor District* classifies the *Special Commercial Corridor (SC)* into three (3) zones: *Preservation Zone*, *Transitional Zone*, and *Opportunity Zone*. In this case, the subject property is located within the *Preservation Zone*, which is defined as "(a) segment of the existing corridor that is being utilized with the highest and best uses for the properties in that zone, and should be maintained and supported." That being said, the proposed *Minor Automotive Repair Garage* is located within a multi-tenant commercial building that allows for a variety of uses permitted within the Commercial (C) District area. These uses naturally change over time due to tenant turnover; however, since this use is permitted only by Specific Use Permit (SUP), it is at the discretion of the Planning and Zoning Commission and City Council if the proposed *Minor Automotive Repair Garage* meets the *District Strategies* outlined within the OURHometown Vision 2040 Comprehensive Plan.

NOTIFICATIONS

On March 18, 2025, staff mailed 16 notices to property owners and occupants within 500-feet of the subject property. In addition, the Meadow Creek Estates Homeowner's Association (HOA) was notified which is the only Homeowner's Association (HOA) within 1,500-feet of the subject property participating in the Neighborhood Notification Program. Additionally, staff posted a sign on the subject property, and advertised the public hearings in the Rockwall Herald Banner as required by the Unified Development Code (UDC). Staff has received no notices in favor or opposed to the applicant's request.

CONDITIONS OF APPROVAL

If the City Council chooses to approve of the applicant's request for a Specific Use Permit (SUP) for a *Minor Automotive Repair Garage* within a Commercial (C) District, then staff would propose the following conditions of approval:

- (1) The applicant shall be responsible for maintaining compliance with the operational conditions contained in the SUP ordinance and which are detailed as follows:
 - (a) The development of the Subject Property shall generally conform to the Site Plan as depicted in Exhibit 'B' of the SUP ordinance.
 - (b) The Minor Automotive Repair Garage shall not have any *Outside Storage* associated with the use.
- (2) Any construction resulting from the approval of this zoning change shall conform to the requirements set forth by the Unified Development Code (UDC), the International Building Code (IBC), the Rockwall Municipal Code of Ordinances, city adopted engineering and fire codes and with all other applicable regulatory requirements administered and/or enforced by the state and federal government.

PLANNING AND ZONING COMMISSION

On April 15, 2025, the Planning and Zoning Commission approved a motion to recommend approval of the Specific Use Permit (SUP) by a vote of 6-0, with Commissioner Thompson absent.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY

PLANNING & ZONING CASE NO. _____

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING: _____

CITY ENGINEER: _____

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

MASTER PLAT (\$100.00 + \$15.00 ACRE) ¹

PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE) ¹

FINAL PLAT (\$300.00 + \$20.00 ACRE) ¹

REPLAT (\$300.00 + \$20.00 ACRE) ¹

AMENDING OR MINOR PLAT (\$150.00)

PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

SITE PLAN (\$250.00 + \$20.00 ACRE) ¹

AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

ZONING CHANGE (\$200.00 + \$15.00 ACRE) ¹

SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE) ^{1&2}

PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE) ¹

OTHER APPLICATION FEES:

TREE REMOVAL (\$75.00)

VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00) ²

NOTES:

¹: IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.

²: A \$1,000.00 FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS: 1460 T L Townsend Dr #116 Rockwall TX 75087

SUBDIVISION: _____ LOT: _____ BLOCK: _____

GENERAL LOCATION: _____

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING: Commercial (C) District CURRENT USE: Vacant

PROPOSED ZONING: Land use Permitted Specific Use Permit PROPOSED USE: Automotive Tint + Wraps

ACREAGE: _____ LOTS [CURRENT]: _____ LOTS [PROPOSED]: _____

SITE PLANS AND PLATS: BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

| | | | |
|---|--------------------|------------------------------------|--|
| <input checked="" type="checkbox"/> OWNER | Alexander Trujillo | <input type="checkbox"/> APPLICANT | |
| CONTACT PERSON | | CONTACT PERSON | |
| ADDRESS | 2110 Glaston Rd | ADDRESS | |
| CITY, STATE & ZIP | Forney TX 75126 | CITY, STATE & ZIP | |
| PHONE | 972-900-5706 | PHONE | |
| E-MAIL | | E-MAIL | |

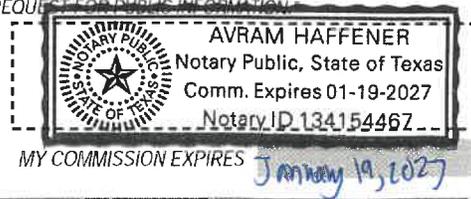
NOTARY VERIFICATION [REQUIRED]

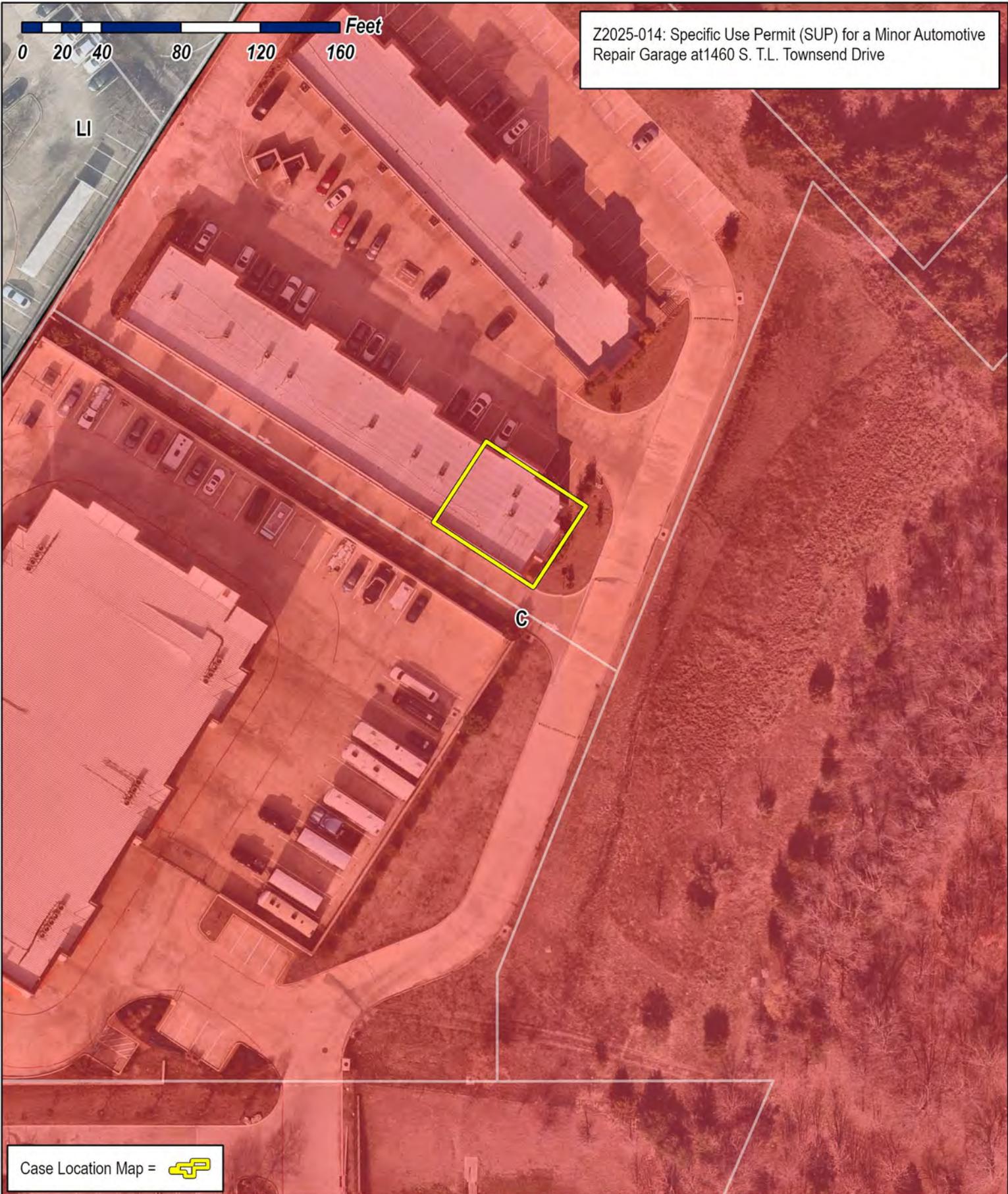
BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Alexander Trujillo Arias [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

¹ I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 200.00 + 215.00 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE 14th DAY OF March, 2025. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION.

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 14th DAY OF March, 2025.

OWNER'S SIGNATURE: _____
NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS: _____





City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75087
 (P): (972) 771-7745
 (W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.

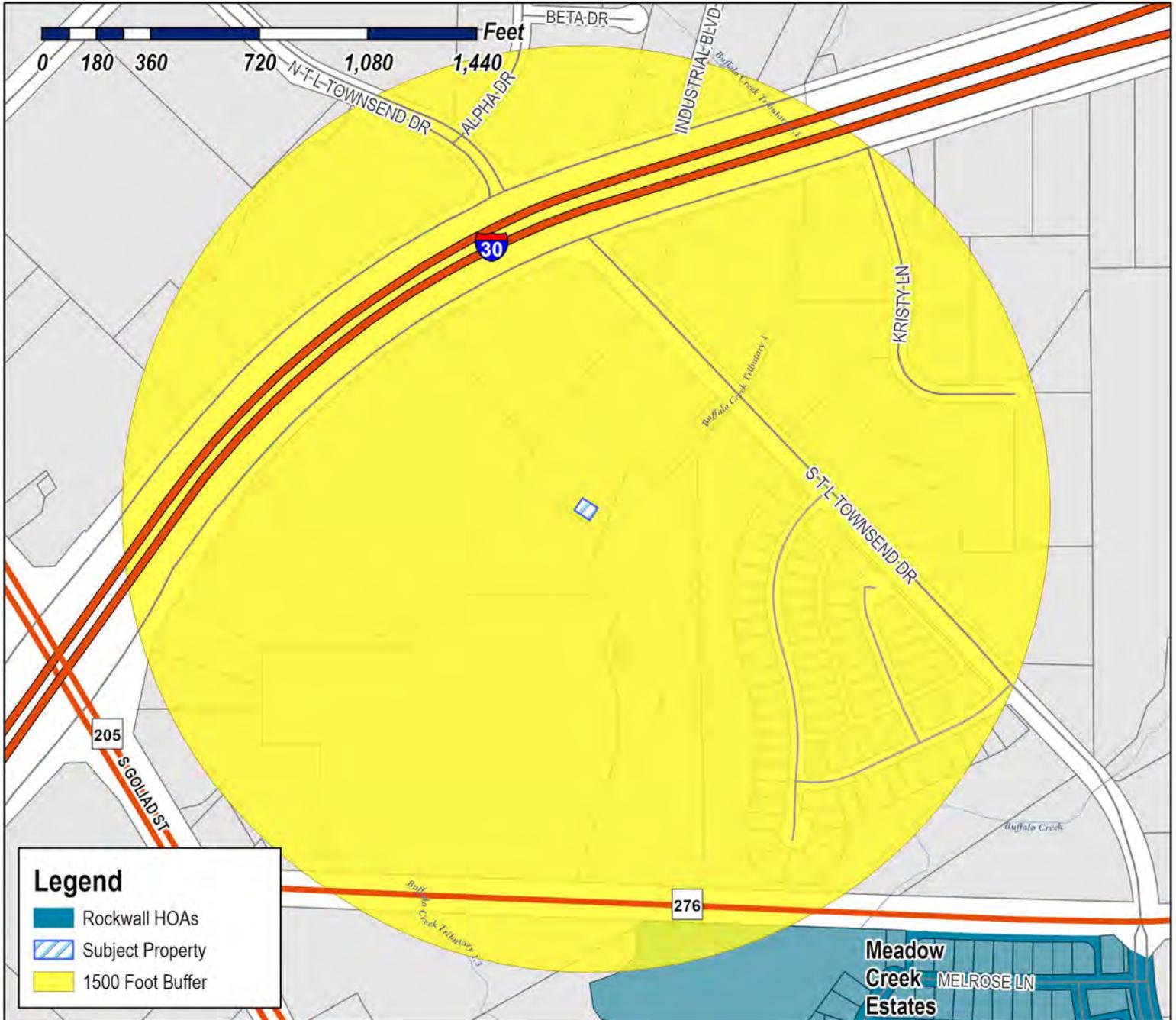




City of Rockwall

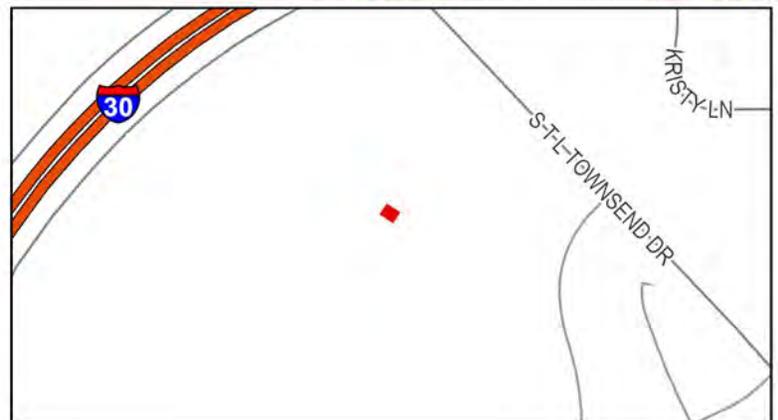
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Case Number: Z2025-014
Case Name: SUP for a Minor Automotive Repair Garage
Case Type: Zoning
Zoning: Commercial (C) District
Case Address: 1460 S. T.L. Townsend

Date Saved: 3/14/2025
 For Questions on this Case Call (972) 771-7745





Neighborhood Notification Program [Z2025-014]

From Zavala, Melanie <MZavala@rockwall.com>

Date Wed 3/19/2025 9:50 AM

Cc Miller, Ryan <RMiller@rockwall.com>; Lee, Henry <HLee@rockwall.com>; Ross, Bethany <bross@rockwall.com>; Guevara, Angelica <AGuevara@Rockwall.com>

 2 attachments (862 KB)

Public Notice (03.17.2025).pdf; HOA Map (03.19.2025).pdf;

HOA/Neighborhood Association Representative:

Per your participation in the *Neighborhood Notification Program*, you are receiving this notice to inform your organization that a zoning case has been filed with the City of Rockwall that is located within 1,500-feet of the boundaries of your neighborhood. As the contact listed for your organization, you are encouraged to share this information with the residents of your subdivision. Please find the attached map detailing the property requesting to be rezoned in relation to your subdivision boundaries. Additionally, below is the summary of the zoning case that will be published in the Rockwall Herald Banner on Friday, March 21, 2025. The Planning and Zoning Commission will hold a public hearing on Tuesday, April 15, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, April 21, 2025 at 6:00 PM. Both hearings will take place at 6:00 PM at City Hall, 385 S. Goliad, Rockwall, TX 75087.

All interested parties are encouraged to submit public comments via email to Planning@rockwall.com at least 30 minutes in advance of the meeting. Please include your name, address, and the case number your comments are referring to. These comments will be read into the record during each of the public hearings. Additional information on all current development cases can be found on the City's website: <https://sites.google.com/site/rockwallplanning/development/development-cases>.

Z2025-014: Specific Use Permit (SUP) for a Minor Automotive Repair Garage

Hold a public hearing to discuss and consider a request by Alexander Trujillo for the approval of a *Specific Use Permit (SUP)* for *Minor Automotive Repair Garage* on a 2.692-acre parcel of land identified Lot 5, Block A, Platinum Storage Addition, City of Rockwall, Rockwall County, Texas, zoned Commercial (C) District, situated within the IH-30 Overlay (IH-30 OV) District, addressed as 1460 T. L. Townsend Drive, Suite 116, and take any action necessary.

Thank you,

Melanie Zavala

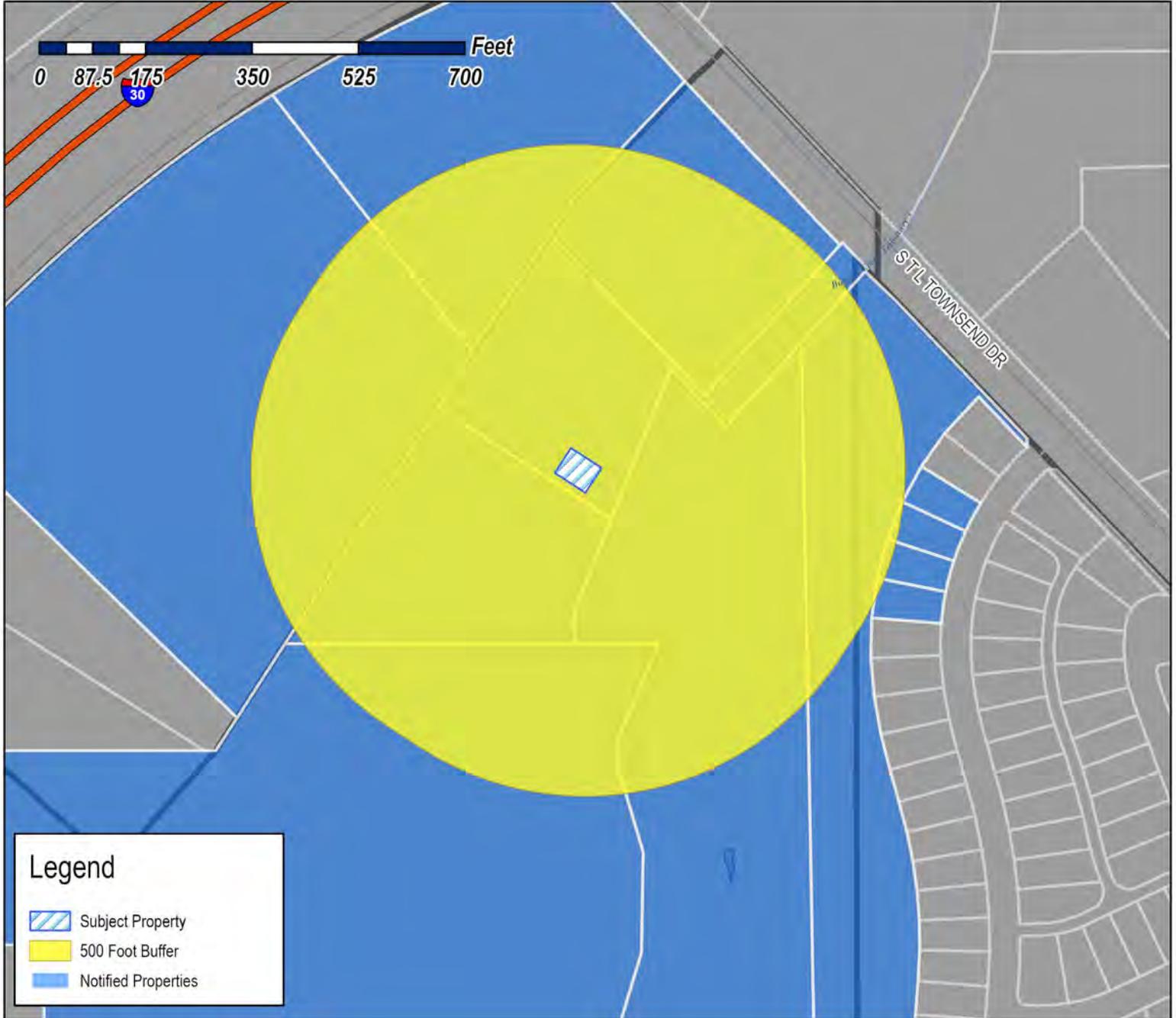
Planning & Zoning Coordinator | Planning Dept. | City of Rockwall
385 S. Goliad Street | Rockwall, TX 75087
[Planning & Zoning Rockwall](#)
972-771-7745 Ext. 6568



City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

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Case Number: Z2025-014
Case Name: SUP for a Minor Automotive Repair Garage
Case Type: Zoning
Zoning: Commercial (C) District
Case Address: 1460 S. T.L. Townsend Drive

Date Saved: 3/14/2025

For Questions on this Case Call: (972) 771-7745



RESIDENT
1225 HWY 276
ROCKWALL, TX 75032

RESIDENT
1245 HWY276 DR
ROCKWALL, TX 75032

JACKSON AUTOMOTIVE REAL ESTATE INV LLC
DBA TOYOTA OF ROCKWALL
1250 E INTERSTATE 30
ROCKWALL, TX 75087

RESIDENT
1290 I30
ROCKWALL, TX 75032

SARO PARTNERS LLC
1450 T L TOWNSEND
ROCKWALL, TX 75032

RESIDENT
1460 S TOWNSEND DR
ROCKWALL, TX 75032

RESIDENT
1480 S T L TOWNSEND DR
ROCKWALL, TX 75032

FARR TRAVIS AND MELLISA
1539 TROWBRIDGE CIRCLE
ROCKWALL, TX 75032

COURT MONDA J
1545 Trowbridge Cir
Rockwall, TX 75032

RESIDENT
1551 TROWBRIDGE CIR
ROCKWALL, TX 75032

MOTA SAMUEL
1557 Trowbridge Cir
Rockwall, TX 75032

CITY OF HEATH
200 LAURENCE DRIVE
HEATH, TX 75032

CTE PHASE I LP
2266 LAFAYETTE LNDG
ROCKWALL, TX 75032

FENG YI
2757 SCENIC DR
PLANO, TX 75025

MSC ROCKWALL LLC
725 PARK CENTER DRIVE
MATTHEWS, NC 28105

COSTCO WHOLESALE CORP
PROPERTY TAX DEPT 1049
999 LAKE DR
ISSAQUAH, WA 98027

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

Property Owner and/or Resident of the City of Rockwall:

You are hereby notified that the City of Rockwall Planning and Zoning Commission and City Council will consider the following application:

Z2025-014: Specific Use Permit (SUP) for Minor Automotive Repair Garage

Hold a public hearing to discuss and consider a request by Alexander Trujillo for the approval of a *Specific Use Permit (SUP)* for *Minor Automotive Repair Garage* on a 2.692-acre parcel of land identified Lot 5, Block A, Platinum Storage Addition, City of Rockwall, Rockwall County, Texas, zoned Commercial (C) District, situated within the IH-30 Overlay (IH-30 OV) District, addressed as 1460 T. L. Townsend Drive, Suite 116, and take any action necessary.

For the purpose of considering the effects of such a request, the Planning and Zoning Commission will hold a public hearing on Tuesday, April 15, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, April 21, 2025 at 6:00 PM. These hearings will be held in the City Council Chambers at City Hall, 385 S. Goliad Street.

As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Bethany Ross
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, April 21, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

PLEASE RETURN THE BELOW FORM

Case No. Z2025-014: Specific Use Permit (SUP) for Minor Automotive Repair Garage

Please place a check mark on the appropriate line below:

- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Name:

Address:

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE

Alexander Trujillo
Owner, Orchid Tint and Designs
1460 T L Townsend Dr #116
Rockwall, TX 75087
(972) 900 5706

3/14/2025

City of Rockwall Planning and Zoning Department

Dear Planning and Zoning Department,

I am writing to formally request a Special Use Permit to operate our new business, Orchid Tint and Designs, at 1460 T L Townsend Dr #116 Rockwall, TX 75087. Our business specializes in window tinting services, as well as paint protection wraps, within a commercial zone.

To provide additional clarity about our operations:

- ****By-Appointment-Only Service****: All vehicle services will be conducted strictly by appointment, ensuring a controlled and organized flow of customer visits.
- ****Same-Day Service****: Services will be completed on the same day, and no vehicles will be stored on-site overnight.
- ****Minimal Disruption****: Our operations are designed to minimize impact on neighboring businesses and traffic within the area.

We are committed to adhering to all regulations and maintaining a professional, clean, and community-conscious establishment. Our business will contribute to the local economy by providing specialized vehicle services while respecting the integrity of the commercial zone.

I kindly ask for your consideration of this request, and I am happy to provide any additional documentation or answer questions that might assist in your evaluation process. Thank you for your time and support.

Sincerely,
Alexander Trujillo
Owner, Orchid Tint and Designs



Orchid Consortium LLC



x Two Parking Spaces

Bay door.

CITY OF ROCKWALL

ORDINANCE NO. 25-XX

SPECIFIC USE PERMIT NO. S-3XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE (UDC) [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) TO ALLOW A *MINOR AUTOMOTIVE REPAIR GARAGE* ON A 2.692-ACRE PARCEL OF LAND IDENTIFIED LOT 5, BLOCK A, PLATINUM STORAGE ADDITION, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; AND MORE SPECIFICALLY DEPICTED IN *EXHIBIT 'A'* AND *EXHIBIT 'B'* OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City has received a request from Alexander Trujillo for the approval of a *Specific Use Permit (SUP)* for *Minor Automotive Repair Garage* on a 2.692-acre parcel of land identified Lot 5, Block A, Platinum Storage Addition, City of Rockwall, Rockwall County, Texas, zoned Commercial (C) District, situated within the IH-30 Overlay (IH-30 OV) District, addressed as 1460 T. L. Townsend Drive, Suite 116, and being more specifically depicted in *Exhibit 'A'* and *Exhibit 'B'* of this ordinance, which herein after shall be referred to as the *Subject Property* and incorporated by reference herein; and

WHEREAS, the Planning and Zoning Commission of the City of Rockwall and the governing body of the City of Rockwall, in compliance with the laws of the State of Texas and the ordinances of the City of Rockwall, have given the requisite notices by publication and otherwise, and have held public hearings and afforded a full and fair hearing to all property owners generally, and to all persons interested in and situated in the affected area and in the vicinity thereof, the governing body in the exercise of its legislative discretion has concluded that the Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall should be amended as follows:

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS:

SECTION 1. The Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall, as heretofore amended, be and the same is hereby amended so as to grant a Specific Use Permit (SUP) to allow a *Minor Automotive Repair Garage* in accordance with Article 04, *Permissible Uses*, of the Unified Development Code (UDC) [Ordinance No. 20-02] on the *Subject Property*; and,

SECTION 2. That the Specific Use Permit (SUP) shall be subject to the requirements set forth in Subsection 04.01, *Commercial (C) District*, of Article 05, *District Development Standards*, of the Unified Development Code (UDC) [Ordinance No. 20-02] -- as heretofore amended and may be amended in the future -- and with the following conditions:

Z2025-014: SUP for a Minor Automotive Repair Garage at 1460 T.L. Townsend Drive
Ordinance No. 25-XX; SUP # S-3XX

2.1 OPERATIONAL CONDITIONS

The following conditions pertain to the operation of the *Minor Automotive Repair Garage* on the *Subject Property* and conformance to these conditions is required for continued operation:

- 1) The development of the *Subject Property* shall generally conform to the *Site Plan* as depicted in *Exhibit 'B'* of this ordinance.
- 2) The *Minor Automotive Repair Garage* shall not have any *Outside Storage* associated with the use.

2.2 COMPLIANCE

Approval of this ordinance in accordance with Subsection 02.02, *Specific Use Permits (SUP)* of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC) will require the *Subject Property* to comply with the following:

- 1) Upon obtaining a *Certificate of Occupancy (CO)*, should the business owner operating under the guidelines of this ordinance fail to meet the minimum operational requirements set forth herein and outlined in the Unified Development Code (UDC), the City may (*after proper notice*) initiate proceedings to revoke the Specific Use Permit (SUP) in accordance with Subsection 02.02(F), *Revocation*, of Article 11, *Development Applications and Revision Procedures*, of the Unified Development Code (UDC) [*Ordinance No. 20-02*].

SECTION 3. That the official zoning map of the City be corrected to reflect the changes in zoning described herein.

SECTION 4. That all ordinances of the City of Rockwall in conflict with the provisions of this ordinance be, and the same are hereby repealed to the extent of that conflict.

SECTION 5. Any person, firm, or corporation violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction shall be punished by a penalty of fine not to exceed the sum of *TWO THOUSAND DOLLARS (\$2,000.00)* for each offence and each and every day such offense shall continue shall be deemed to constitute a separate offense.

SECTION 6. If any section or provision of this ordinance or the application of that section or provision to any person, firm, corporation, situation or circumstance is for any reason judged invalid, the adjudication shall not affect any other section or provision of this ordinance or the application of any other section or provision to any other person, firm, corporation, situation or circumstance, and the City Council declares that it would have adopted the valid portions and applications of the ordinance without the invalid parts and to this end the provisions of this ordinance shall remain in full force and effect.

SECTION 7. That this ordinance shall take effect immediately from and after its passage.

PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, THIS THE 5TH DAY OF MAY, 2025.

Trace Johannesen, Mayor

ATTEST:

Kristy Teague, *City Secretary*

APPROVED AS TO FORM:

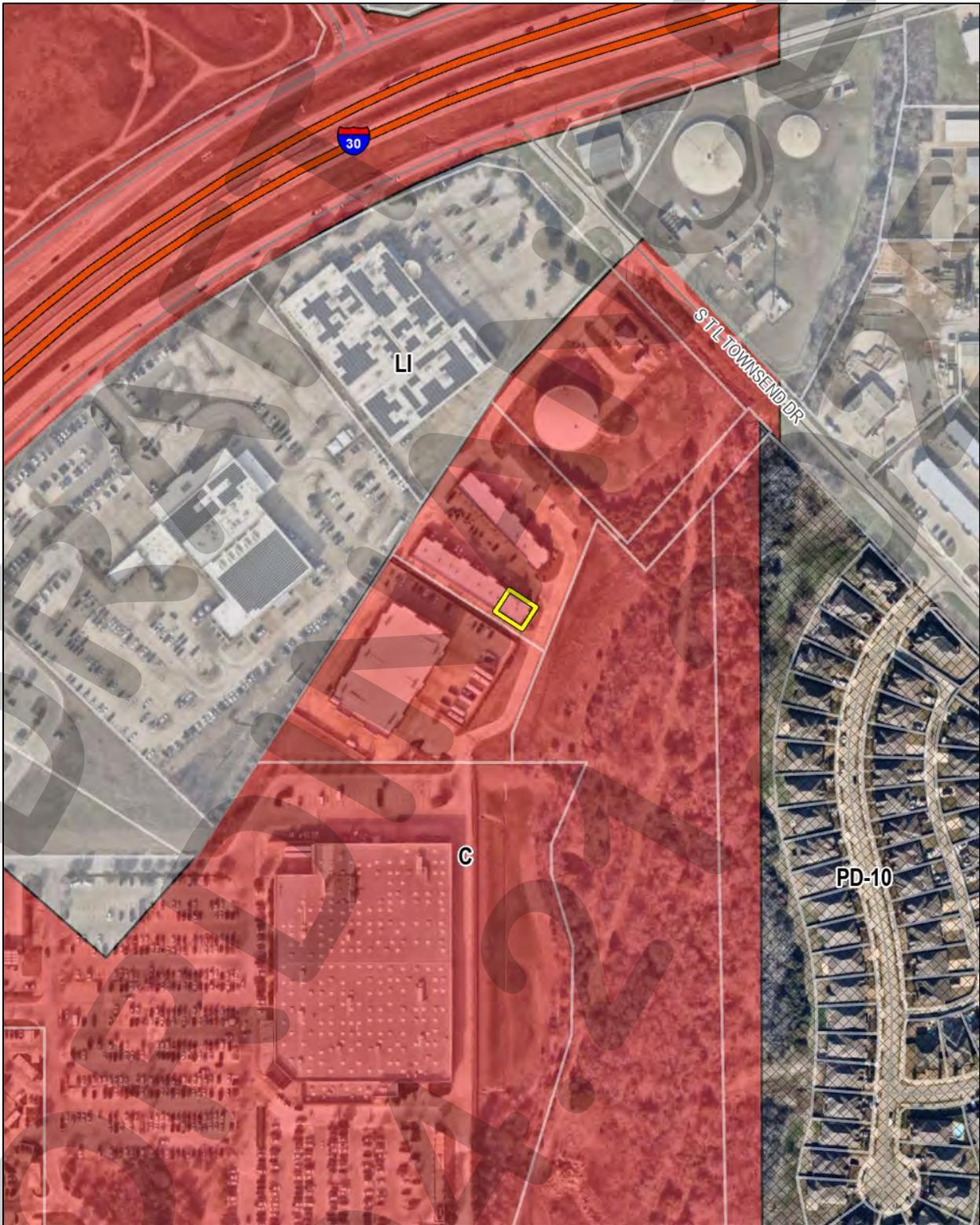
Frank J. Garza, *City Attorney*

1st Reading: April 21, 2025

2nd Reading: May 5, 2025

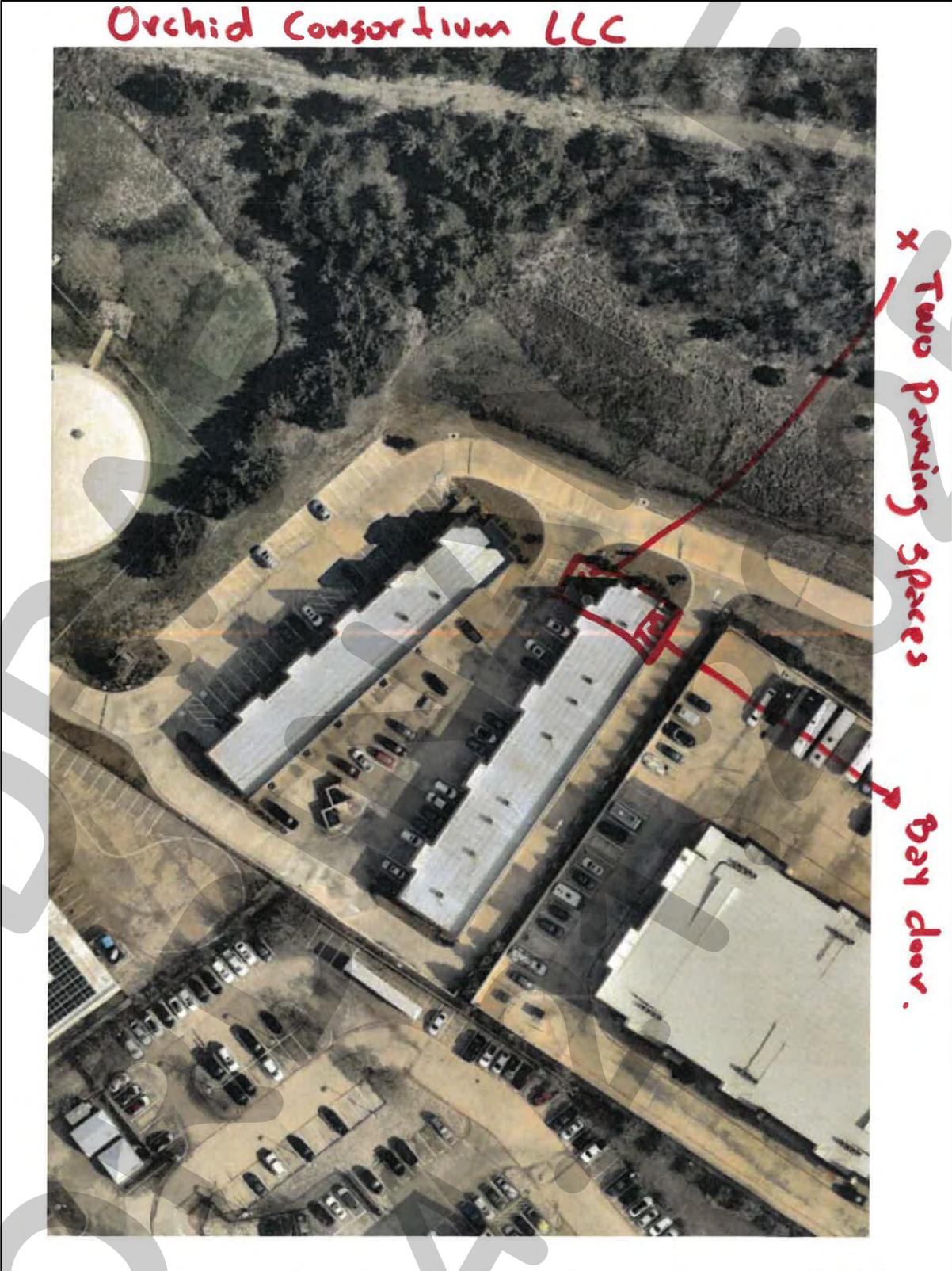
Exhibit 'A'
Location Map

Legal Description: Lot 5, Block A, Platinum Storage Addition
Address: 1460 T.L. Townsend Drive



Z2025-014: SUP for a Minor Automotive Repair
Garage at 1460 T.L. Townsend Drive
Ordinance No. 25-XX; SUP # S-3XX

Exhibit 'B':
Site Plan





CITY OF ROCKWALL

CITY COUNCIL CASE MEMO

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council

DATE: April 21, 2025

APPLICANT: Javier Silva; *JMS Custom Homes*

CASE NUMBER: Z2025-015; *Specific Use Permit (SUP) for a Residential Infill in an Established Subdivision and a Guest Quarters/Secondary Living Unit at 403B S. Clark Street*

SUMMARY

Hold a public hearing to discuss and consider a request by Javier Silva of JMS Custom Homes for the approval of a Specific Use Permit (SUP) for *Residential Infill in an Established Subdivision* and a *Guest Quarters/Secondary Living Unit* on a 0.42-acre parcel of land identified as Lot 2, Block 1, Shaw Addition, City of Rockwall, Rockwall County, Texas, zoned Single-Family 7 (SF-7) District, addressed as 403B S. Clark Street, and take any action necessary.

BACKGROUND

The subject property was annexed prior to 1934 based on the April 1934 Sanborn Map. According to the City's historic zoning maps, the subject property was zoned Single-Family 3 (SF-3) District as of January 3, 1972. Based on the May 16, 1983 zoning map, this designation changed to a Single-Family 7 (SF-7) District. The subject property has remained vacant and zoned Single-Family 7 (SF-7) District. On July 7, 2012, the subject property was platted as Lot 2, Block 1, Shaw Addition.

PURPOSE

The applicant -- *Javier Silva of JMS Custom Homes* -- is requesting the approval of a Specific Use Permit (SUP) for the purpose of constructing a single-family home and a *Guest Quarters/Secondary Living Unit* on the subject property in accordance with Subsection 02.03(B) (11), *Residential Infill in or Adjacent to an Established Subdivision*, and Subsection 02.03, *Conditional Land Use Standards*, of Article 04, *Permissible Uses*, of the Unified Development Code (UDC).

ADJACENT LAND USES AND ACCESS

The subject property is located at 403B S. Clark Street. The land uses adjacent to the subject property are as follows:

North: Directly north of the subject property is one (1) parcel of land [*i.e. 403A S. Clark Street*] which is the remainder of the Shaw Addition. Beyond this are five (5) parcels of land [*i.e. 301, 307, 401 S. Clark Street and 706 & 708 Hartman Street*] that make up a part of the B.F. Boydston Addition. All of these properties are developed with single-family homes and are zoned Single-Family 7 (SF-7) District. North of this is Hartman Street, which is classified as a R2 (*i.e. residential, two [2] lane, undivided roadway*) on the City's Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan.

South: Directly south of the subject property is the Clark Street Homesite Addition, which consists of two (2) lots [*i.e. 405 & 407 S. Clark Street*] developed with single-family homes. Beyond this is the Haley and Kyle Subdivision, which consists of two (2) lots [*i.e. 501 & 503 S. Clark Street*] that are developed with single-family homes. South of this is one (1) parcel of land [*i.e. Block 107 of the B.F. Boydston Addition*] which is developed with a single-family home. All of these properties are zoned Single-Family 7 (SF-7) District.

East: Directly east of the subject property is a vacant 6.60-acre parcel of land [*i.e. Lot 8, Block A, Richard Harris No. 2 Addition*] zoned Single-Family 7 (SF-7) District. Beyond this are seven (7) parcels of land [*i.e. 400, 402, 404, 406,*

408, 410 & 412 Renfro Street] that are developed with single-family homes and make up the remainder of the Richard Harris No. 2 Addition. All of these properties are zoned Single-Family 7 (SF-7) District. East of this is Renfro Street, which is classified as a *Minor Collector* on the City’s Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan.

West: Directly west of the subject property is S. Clark Street, which is classified as a *Minor Collector* on the City’s Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Beyond this is the Shields. Subdivision, which consists of 4 lots on 0.85-acres. West of this are various parcels of land that make up part of three (3) different subdivisions [i.e. B.F. Boydston, Dodson Hardin, and Tovar Subdivisions]. These properties are all developed with single-family homes and zoned Single-Family 7 (SF-7) District.

CHARACTERISTICS OF THE REQUEST AND CONFORMANCE TO THE CITY’S CODES

Article 13, *Definitions*, of the Unified Development Code (UDC) defines *Residential Infill in or Adjacent to an Established Subdivision* as “(t)he new development of a single-family home or duplex on an existing vacant or undeveloped parcel of land or the redevelopment of a developed parcel of land for a new single-family home or duplex within an established subdivision that is mostly or entirely built-out.” An *established subdivision* is further defined in Subsection 02.03(B) (11) of Article 04, *Permissible Uses*, of the UDC as “...a subdivision that consists of five (5) or more lots, that is 90% developed, and that has been in existence for more than ten (10) years.” In this case, the subject property is located within the B.F. Boydston Addition, which is considered to be an established subdivision, and has been in existence for more than ten (10) years, consists of more than five (5) lots, and is greater than 90% developed. The *Permissible Use Charts* contained in Article 04, *Permissible Uses*, of the UDC, requires a Specific Use Permit (SUP) for *Residential Infill in or Adjacent to an Established Subdivision* in all single-family zoning districts, the Two-Family (2F) District, the Downtown (DT) District, and the Residential-Office (RO) District. This property, being within 500-feet of an established subdivision and being zoned Single-Family 7 (SF-7) District, requires a Specific Use Permit (SUP).

In addition, Subsection 02.03(B)(11) of Article 04, *Permissible Uses*, of the UDC states that, “...the Planning and Zoning Commission and City Council shall consider the proposed size, location, and architecture of the home compared to the existing housing ... [and] (a)ll housing proposed under this section [i.e. *Residential Infill in or Adjacent to an Established Subdivision*] shall be constructed to be architecturally and visually similar or complimentary to the existing housing ...” The following is a summary of observations concerning the housing on S. Clark Street compared to the house proposed by the applicant:

| Housing Design and Characteristics | Existing Housing on S. Clark Street | Proposed Housing |
|------------------------------------|--|--|
| Building Height | One (1) & Two (2) Story | One (1) Story |
| Building Orientation | All of the homes are oriented toward the street they are built along. | The front elevation of the home will face S. Clark Street |
| Year Built | 1900 - 2022 | N/A |
| Building SF on Property | 752 SF – 5,408 SF | 3,110 SF |
| Building Architecture | Mostly Single-Family Homes and One (1) Vacant Lot | Comparable Architecture to the Surrounding New Single-Family Homes |
| Building Setbacks: | | |
| Front | 20-Feet or Greater | 20-Feet |
| Side | Estimated between zero (0) and greater than ten (10) feet. | 6-Feet |
| Rear | The rear yard setbacks appear to be greater than ten (10) feet. | Greater Than Ten (10) Feet |
| Building Materials | Brick, Siding, and Stone | Board & Batten Siding |
| Paint and Color | Grey, White, Red, Orange, Brown | N/A |
| Roofs | Composite & Asphalt Shingles | Composite Shingle and Metal |
| Driveways/Garages | Driveways all front the same street the single-family home faces. Front-facing and some with no garages. | The proposed garage will be set eight (8) feet, 1-2-inch in front of the front façade of the home. |

In this case, the applicant is proposing a home that is not conforming to one (1) standard outlined in the Unified Development Code (UDC). This is as follows:

- (1) *Garage*. According to Subsection 04.01(B), *Lots Less Than Five Acres*, of Article 06, *Parking and Loading*, garages located in single family districts must be located 20-feet behind the front façade of the building. In the current request, the proposed garage is located 8-feet, ½-inch in front of the front façade of the home. In this case, the proposed garage is situated 8-feet, ½-inch in front of the front façade of the home which will require a variance from the Planning and Zoning Commission. When looking at this non-conformity, staff should point out that this is not uncommon along the existing housing on S. Clark Street and staff does not feel this is will create a negative impact within the subdivision.

With this being said, this is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission. If approved as part of the Specific Use Permit (SUP) request, the Planning and Zoning Commission and City Council will be waiving this requirement. With the exception of this deviation from the *District Development Standards*, the rest of the request for the single-family home does appear to be in conformance with the density and dimensional requirements stipulated by the Unified Development Code (UDC). For the purpose of comparing the proposed home to the existing single-family housing located adjacent to or in the vicinity of the subject property, staff has provided photos of the properties along S. Clark Street and the proposed building elevations in the attached packet.

According to Article 13, *Definitions*, of the Unified Development Code (UDC) a *Guest Quarters/Secondary Living Unit* is defined as “(a)n accessory building designed for the temporary occupancy of guests of the primary dwelling for which there is no remuneration and is not rented or otherwise used as separate domicile.” According to the *Permissible Use Charts* contained in Article 04, *Permissible Uses*, of the Unified Development Code (UDC) a *Guest Quarters/Secondary Living Unit/Accessory Dwelling Unit* requires a Specific Use Permit (SUP) in a Single-Family 7 (SF-7) District and -- according to Subsection 02.03 -- is subject to the following *Conditional Land Use Standards*: [1] the structure must be ancillary to a single-family home, [2] the square footage of the structure shall not exceed 30.00% of the square footage of the primary structure, and [3] the structure may not be sold or conveyed separately from the single-family home without meeting the zoning requirements for the district. In addition to these requirements, a *Guest Quarters/Secondary Living Unit/Accessory Dwelling Unit* is subject to the dimensional requirements contained in Subsection 07.04, *Accessory Structure Development Standards*, of Article 05, *District Development Standards*, and the parking requirements of Article 06, *Parking and Loading*, of the Unified Development Code (UDC). As stated above, *Guest Quarters/Secondary Living Unit* are permitted to be 30.00% of the square footage of the primary structure. In this case, the applicant is permitted a 933 SF *Guest Quarters/Secondary Living Unit* (i.e. 3,110 SF x 30.00% = 933 SF) based on the operational conditions for a *Guest Quarters/Secondary Living Unit*. With that being said, the applicant is only requesting a 626 SF *Guest Quarters/Secondary Living Unit*, which represents 20.13% of the primary structure. This is approximately 9.87% less than what the Unified Development Code (UDC) allows.

If the applicant's Specific Use Permit (SUP) is approved, staff has included operational conditions in the Specific Use Permit (SUP) ordinance that tie down the size, height, and general architecture of the proposed single-family home and *Guest Quarters/Secondary Living Unit*. With this being said, the approval of a Specific Use Permit (SUP) request is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission.

NOTIFICATIONS

On March 18, 2025, staff mailed 84 notices to property owners and occupants within 500-feet of the subject property. Staff also notified the Park Place Homeowner's Association (HOA), which is the only HOA within 1,500-feet of the subject property participating in the Neighborhood Notification Program. Additionally, staff posted a sign on the subject property, and advertised the public hearings in the Rockwall Herald Banner as required by the Unified Development Code (UDC). At the time this report was written, staff had not received any notices back regarding the applicant's request.

CONDITIONS OF APPROVAL

If City Council chooses to approve of the applicant's request for a *Specific Use Permit (SUP)* for *Residential Infill Adjacent to an Established Subdivision and Guest Quarters/Secondary Living Unit*, then staff would propose the following conditions of approval:

- (1) The applicant shall be responsible for maintaining compliance with the operational conditions contained in the *Specific Use Permit (SUP)* ordinance and which are detailed as follows:
 - (a) The development of the *Subject Property* shall generally conform to the *Residential Plot Plan* as depicted in *Exhibit 'B'*

of the draft ordinance.

- (b) The construction of a *single-family home and guest quarters/secondary living unit* on the *Subject Property* shall generally conform to the *Building Elevations* depicted in *Exhibit 'C'* of the draft ordinance.
 - (c) The Guest Quarters/Secondary Living Unit shall not exceed a maximum of 630 SF.
 - (d) The Guest Quarters/Secondary Living Unit shall not incorporate full kitchen facilities.
 - (e) The Guest Quarters/Secondary Living Unit shall not be sold or conveyed separately from the single-family home without meeting the requirements of the zoning district and subdivision ordinance; and,
 - (f) Once construction of the single-family home has been completed, inspected, and accepted by the City, the Specific Use Permit (SUP) shall expire, and no further action by the property owner shall be required.
- (2) Any construction resulting from the approval of this *Specific Use Permit (SUP)* shall conform to the requirements set forth by the Unified Development Code (UDC), the International Building Code (IBC), the Rockwall Municipal Code of Ordinances, city adopted engineering and fire codes and with all other applicable regulatory requirements administered and/or enforced by the state and federal government.

PLANNING AND ZONING COMMISSION

On April 15, 2025, the Planning and Zoning Commission approved a motion to recommend approval of the *Specific Use Permit (SUP)* by a vote of 6-0, with Commissioner Thompson absent.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY

PLANNING & ZONING CASE NO. _____

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING: _____

CITY ENGINEER: _____

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE) ¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE) ¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE) ¹
- REPLAT (\$300.00 + \$20.00 ACRE) ¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE) ¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE) ¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE) ^{1 & 2}
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE) ¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00) ²

NOTES:

¹ IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.
² A \$1,000.00 FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS 403 BOKA Clark St. Rockwall TX 75087

SUBDIVISION Shaw Addition LOT 2 BLOCK 1

GENERAL LOCATION _____

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING Residential CURRENT USE Land

PROPOSED ZONING Residential PROPOSED USE New Build

ACREAGE 0.42 LOTS [CURRENT] _____ LOTS [PROPOSED] _____

SITE PLANS AND PLATS: BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

| | | | |
|--------------------------------|-----------------------------------|------------------------------------|-----------------------------------|
| <input type="checkbox"/> OWNER | <u>JMS Custom Homes</u> | <input type="checkbox"/> APPLICANT | <u>Javier Silva</u> |
| CONTACT PERSON | <u>Javier Silva</u> | CONTACT PERSON | <u>Javier Silva</u> |
| ADDRESS | <u>58 Windsor DR.</u> | ADDRESS | <u>58 Windsor DR.</u> |
| CITY, STATE & ZIP | <u>Rockwall TX 75082</u> | CITY, STATE & ZIP | <u>Rockwall TX 75082</u> |
| PHONE | <u>972-814-9462</u> | PHONE | <u>972-814-9462</u> |
| E-MAIL | <u>support@JMSCustomhomes.net</u> | E-MAIL | <u>support@JMSCustomhomes.net</u> |

NOTARY VERIFICATION [REQUIRED]

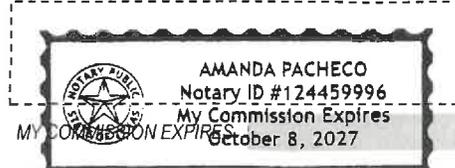
BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Javier Silva [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

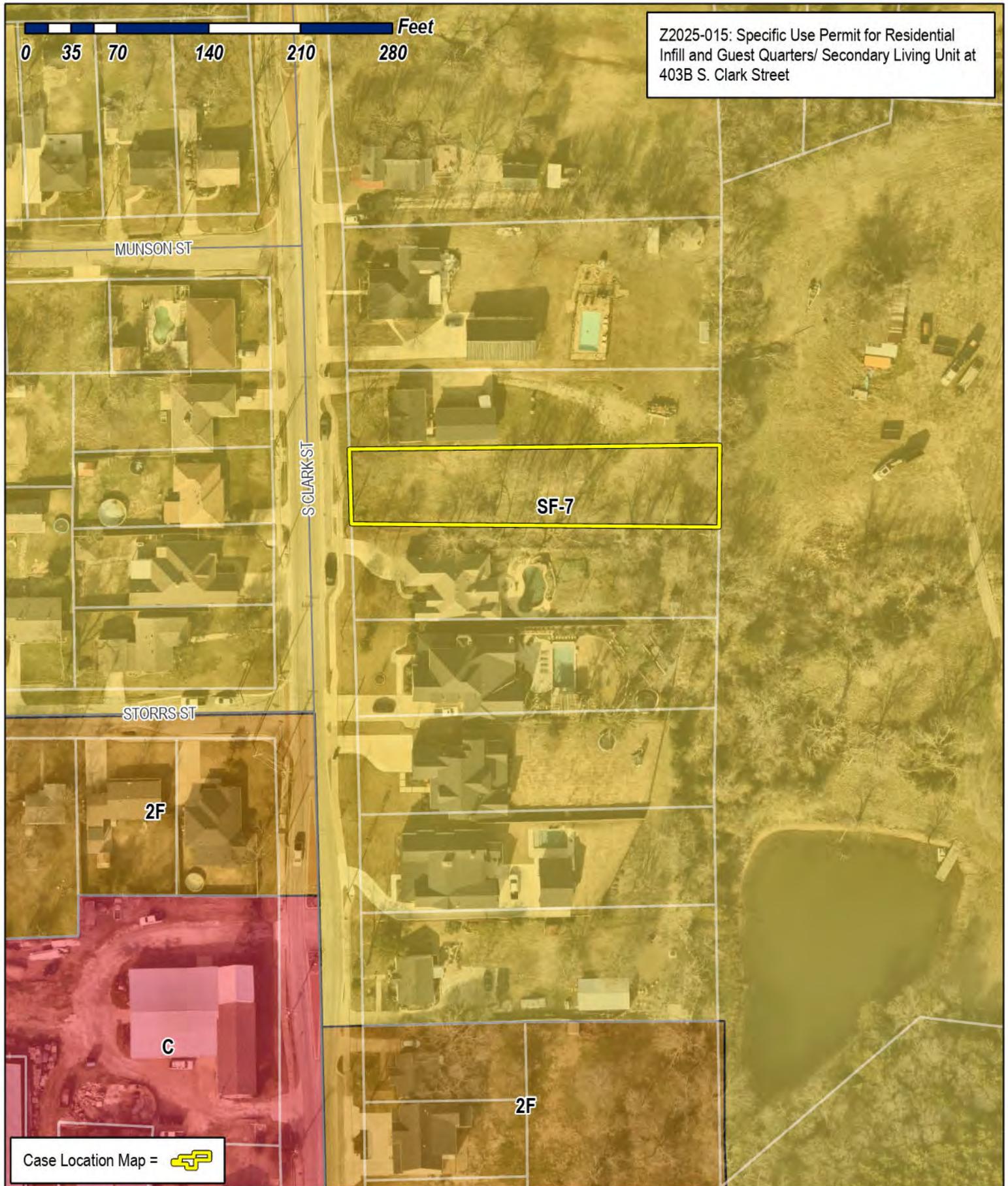
"I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 215.00 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE 14th DAY OF March 2025 BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 14th DAY OF March, 2025

OWNER'S SIGNATURE _____

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS Amanda Pacheco





City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75087
 (P): (972) 771-7745
 (W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.

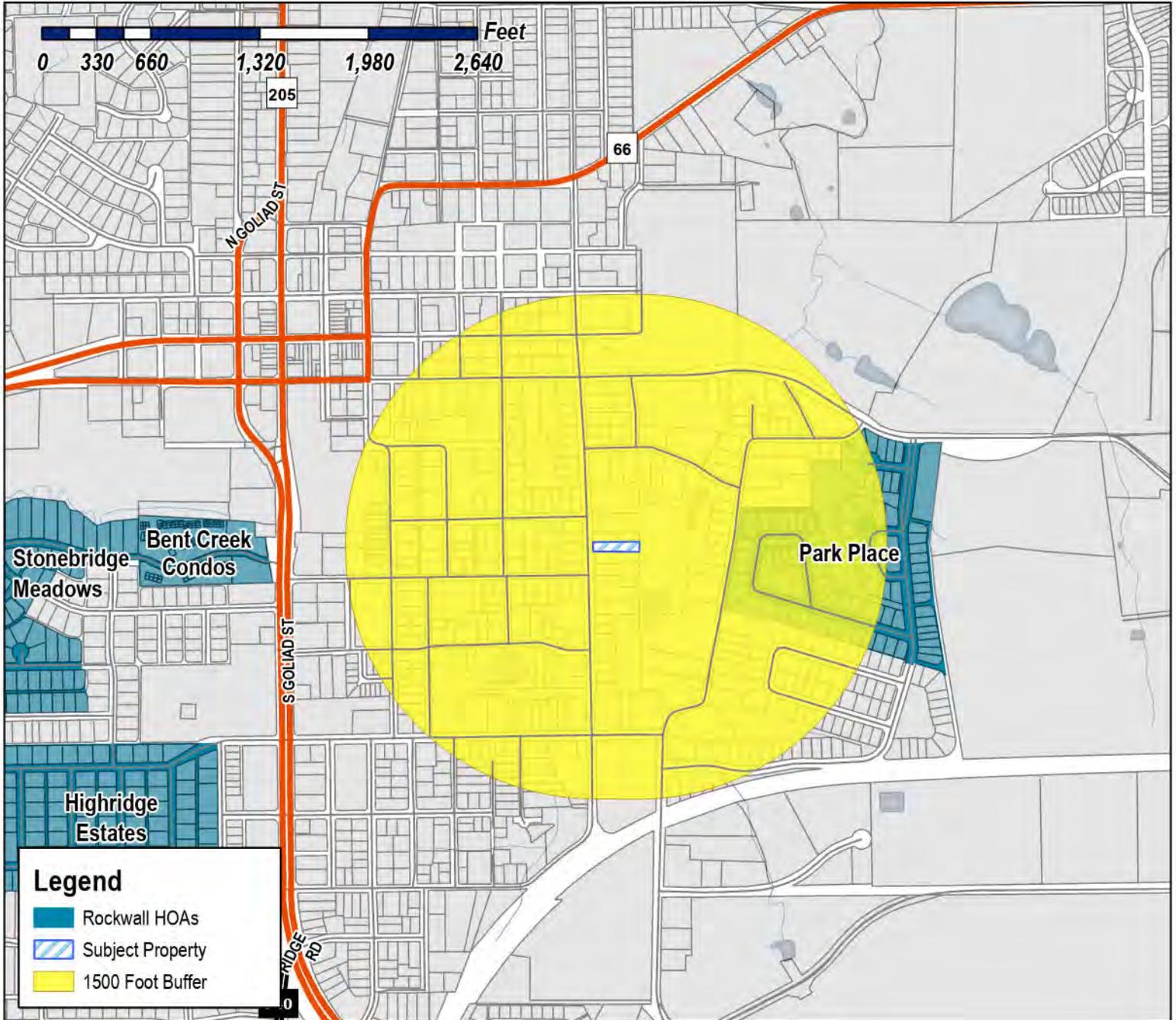




City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

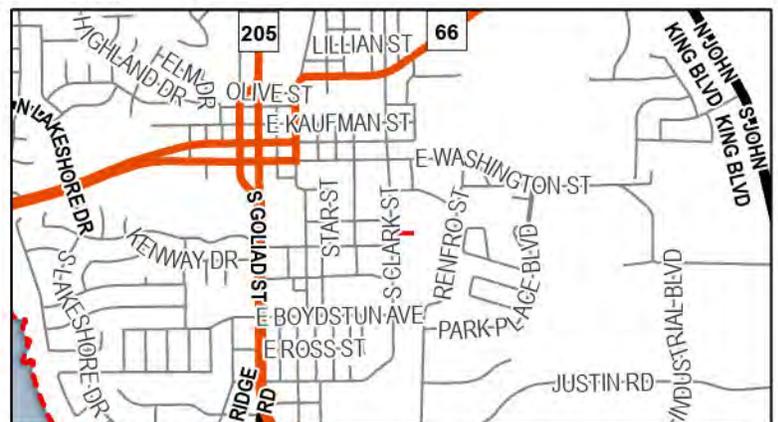
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Case Number: Z2025-015
Case Name: Specific Use Permit (SUP) for Residential Infill & Guest Quarters/ Secondary Living Unit
Case Type: Zoning
Zoning: Single-Family 7 (SF-7) District
Case Address: 403B S. Clark Street

Date Saved: 3/14/2025

For Questions on this Case Call (972) 771-7745



From: [Zavala, Melanie](#)
Cc: [Miller, Ryan](#); [Lee, Henry](#); [Ross, Bethany](#); [Guevara, Angelica](#)
Subject: Neighborhood Notification Program [Z2025-015]
Date: Wednesday, March 19, 2025 9:43:50 AM
Attachments: [Public Notice \(03.17.2025\).pdf](#)
[HOA Map \(03.19.2025\).pdf](#)

HOA/Neighborhood Association Representative:

Per your participation in the [Neighborhood Notification Program](#), you are receiving this notice to inform your organization that a zoning case has been filed with the City of Rockwall that is located within 1,500-feet of the boundaries of your neighborhood. As the contact listed for your organization, you are encouraged to share this information with the residents of your subdivision. Please find the attached map detailing the property requesting to be rezoned in relation to your subdivision boundaries. Additionally, below is the summary of the zoning case that will be published in the Rockwall Herald Banner on [Friday, March 21, 2025](#). The Planning and Zoning Commission will hold a public hearing on [Tuesday, April 15, 2025 at 6:00 PM](#), and the City Council will hold a public hearing on [Monday, April 21, 2025 at 6:00 PM](#). Both hearings will take place at 6:00 PM at City Hall, 385 S. Goliad, Rockwall, TX 75087.

All interested parties are encouraged to submit public comments via email to Planning@rockwall.com at least 30 minutes in advance of the meeting. Please include your name, address, and the case number your comments are referring to. These comments will be read into the record during each of the public hearings. Additional information on all current development cases can be found on the City's website: <https://sites.google.com/site/rockwallplanning/development/development-cases>.

-
Z2025-015: Specific Use Permit (SUP) for a Residential Infill and a Guest Quarters/ Secondary Living Unit
Hold a public hearing to discuss and consider a request by Javier Silva of JMS Custom Homes for the approval of a [Specific Use Permit \(SUP\)](#) for Residential Infill in an Established Subdivision and a Guest Quarters/Secondary Living Unit on a 0.42-acre parcel of land identified as Lot 2, Block 1, Shaw Addition, City of Rockwall, Rockwall County, Texas, zoned Single-Family 7 (SF-7) District, addressed as 403B S. Clark Street, and take any action necessary.

Thank you,

Melanie Zavala

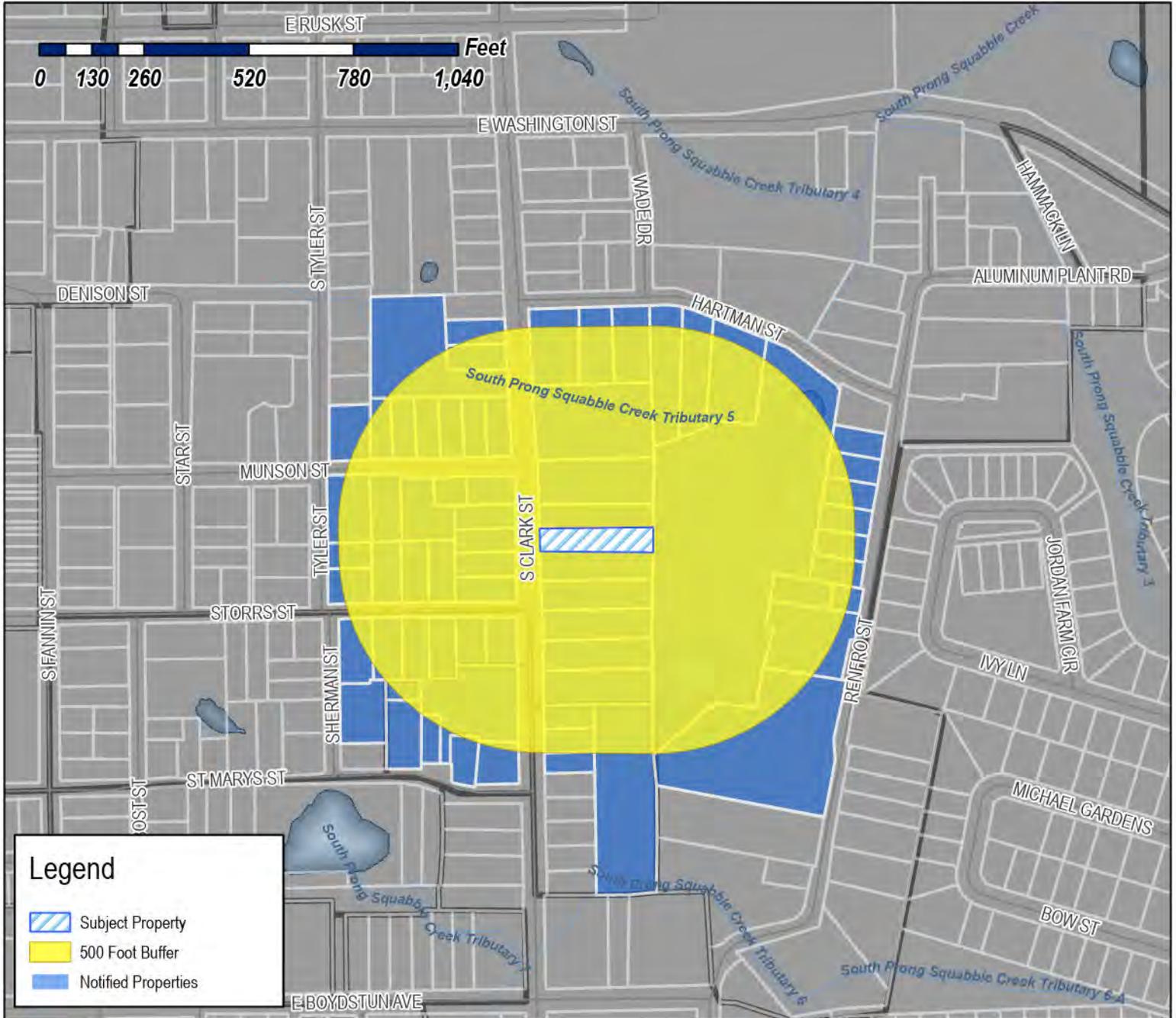
Planning & Zoning Coordinator | Planning Dept. | City of Rockwall
385 S. Goliad Street | Rockwall, TX 75087
[Planning & Zoning Rockwall](#)
972-771-7745 Ext. 6568



City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
(W): www.rockwall.com

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Case Number: Z2025-015
Case Name: Specific Use Permit (SUP) for Residential Infill & Guest Quarters/ Secondary Living Unit
Case Type: Zoning
Zoning: Single-Family 7 (SF-7) District
Case Address: 403B S. Clark Street

Date Saved: 3/14/2025

For Questions on this Case Call: (972) 771-7745



MCCALLUM V LLC
DARRELL ALAN MCCALLUM AND SHARON
FRANCES MCCALLUM AS MEMBERS
1 SOAPBERRY LN
ROCKWALL, TX 75087

OLIVARES JAIME
1209 QUAIL DR
GARLAND, TX 75040

GADDIS DANNY E
12922 EPPS FIELD RD
FARMERS BRANCH, TX 75234

HOGUE MIKE
1498 HUBBARD DRIVE
FORNEY, TX 75126

BOREN TERRY L ETUX
207 GNARLY OAKS WAY
LTL RVR ACAD, TX 76554

LOWREY SUSAN
2070 PONTCHARTRAIN DR
ROCKWALL, TX 75087

RICHARD HARRIS AND JUDY HARRIS FAMILY
TRUST
RICHARD AND JUDY HARRIS- TRUSTEES
210 GLENN AVENUE
ROCKWALL, TX 75087

CASTRO DEVELOPMENT LLC
2212 Ridge Crest Dr
Richardson, TX 75080

MUNSON PARTNERS 1 LLC
2241 AUBURN AVE
DALLAS, TX 75214

CONFIDENTIAL
2420 Conrad Cir
Heath, TX 75032

CAUBLE LINDA
301 S CLARK ST
ROCKWALL, TX 75087

BURGESS JULIA ANN
302 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
306 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
307 S CLARK ST
ROCKWALL, TX 75087

STARK ROBERT SCOTT
3090 N GOLIAD ST SUITE 102 #213
ROCKWALL, TX 75087

CLARK STREET VENTURES LLC
401 COUNTRY RIDGE RD
ROCKWALL, TX 75087

COMBS MARK HILTON AND VERONICA RUTH
401 SOUTH CLARK STREET
ROCKWALL, TX 75087

RESIDENT
402 RENFRO ST
ROCKWALL, TX 75087

SADLER LESLIE A
402 S CLARK ST
ROCKWALL, TX 75087

LIVINGSTON JUSTIN AND BROOKE
403 S CLARK STREET
ROCKWALL, TX 75087

RESIDENT
404 RENFRO ST
ROCKWALL, TX 75087

RESIDENT
404 S CLARK ST
ROCKWALL, TX 75087

BRAMLETT DAVID KYLE & DEIDRE MONIQUE
405 S CLARK ST
ROCKWALL, TX 75087

TOVAR LUIS & MARICELA
405 TYLER ST
ROCKWALL, TX 75087

TOVAR LUIS & MARICELA
405 TYLER ST
ROCKWALL, TX 75087

RESIDENT
406 RENFRO ST
ROCKWALL, TX 75087

RESIDENT
406 S CLARK ST
ROCKWALL, TX 75087

BOWEN CHASE AND
PERRY BOWEN
407 S. CLARK ST.
ROCKWALL, TX 75087

RESIDENT
408 RENFRO ST
ROCKWALL, TX 75087

GADDIS CAMILLE D
408 SOUTH CLARK STREET
ROCKWALL, TX 75087

RESIDENT
410 RENFRO ST
ROCKWALL, TX 75087

SIMS CHRIS AND TERESA
410 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
412 RENFRO ST
ROCKWALL, TX 75087

RESIDENT
500 RENFRO ST
ROCKWALL, TX 75087

RESIDENT
501 SHERMAN ST
ROCKWALL, TX 75087

HUDSON KATIE
501 MUNSON ST
ROCKWALL, TX 75087

BRYAN KYLE AND HALEY BROOKE BOWEN
501 S CLARK ST
ROCKWALL, TX 75087

HOLLAND TRENTON A AND
ROD HOLLAND
502 MUNSON STREET
ROCKWALL, TX 75087

SAMPLES ELVA NELL
502 RENFRO ST
ROCKWALL, TX 75087

RICHARDSON JEANETTE
503 MUNSON ST
ROCKWALL, TX 75087

GARTH GARY AND CYNTHIA
503 SOUTH CLARK ST
ROCKWALL, TX 75087

RESIDENT
504 MUNSON ST
ROCKWALL, TX 75087

RESIDENT
505 MUNSON ST
ROCKWALL, TX 75087

RESIDENT
506 RENFRO ST
ROCKWALL, TX 75087

EARNHEART JOHN L
506 MUNSON ST
ROCKWALL, TX 75087

WYCKOFF MICHELE M
507 MUNSON ST
ROCKWALL, TX 75087

LECOUR DAVID & RENEE
507 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
508 MUNSON ST
ROCKWALL, TX 75087

CIELO BLUE FAMILY HOLDINGS LLC - SERIES 1
508 HIGHVIEW LANE
ROCKWALL, TX 75087

RESIDENT
509 MUNSON ST
ROCKWALL, TX 75087

CASTRO RENE AND BETSY
509 SOUTH CLARK STREET
ROCKWALL, TX 75087

RESIDENT
510 S CLARK
ROCKWALL, TX 75087

RESIDENT
511 MUNSON ST
ROCKWALL, TX 75087

RESIDENT
511 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
512 S CLARK
ROCKWALL, TX 75087

RESIDENT
513 MUNSON ST
ROCKWALL, TX 75087

JIMENEZ ANTONIO P CRUZ AND
NORMA L CRUZ HERNANDEZ
513 S CLARK ST
ROCKWALL, TX 75087

RESIDENT
601 STORRS ST
ROCKWALL, TX 75087

LEE STEPHANIE
602 Storrs St
Rockwall, TX 75087

JONES PEGGY
604 STORRS ST
ROCKWALL, TX 75087

DAVIS JIMMY JACK
605 STORRS ST
ROCKWALL, TX 75087

DAVIS AMY M AND WESLEY D
606 STORRS STREET
ROCKWALL, TX 75087

RESIDENT
607 ST MARY
ROCKWALL, TX 75087

HALDEMAN MICHAEL
607 STORRS ST
ROCKWALL, TX 75087

RESIDENT
608 STORRS ST
ROCKWALL, TX 75087

RESIDENT
609 STORRS ST
ROCKWALL, TX 75087

JOHNSTON SHERRI A
610 STORRS ST
ROCKWALL, TX 75087

H & M TOOL AND DIE CO
611 SAINT MARY ST
ROCKWALL, TX 75087

RESIDENT
612 STORRS ST
ROCKWALL, TX 75087

RESIDENT
613 ST MARYS PL
ROCKWALL, TX 75087

RESIDENT
706 HARTMAN ST
ROCKWALL, TX 75087

MOORE LAKISHA Q AND JASON
708 HARTMAN ST
ROCKWALL, TX 75087

501 SHERMAN, A SERIES OF STARK FAMILY
PROPERTIES, LLC
710 AGAPE CIR
ROCKWALL, TX 75087

507-509 SHERMAN, A SERIES OF STARK FAMILY
PROPERTIES, LLC
C/O ROBERT STARK
710 AGAPE CIR
ROCKWALL, TX 75087

RENDON MARCELINO J JR
710 HARTMAN ST
ROCKWALL, TX 75087

GILLIAM ROBERT W JR & PERRILYN
712 HARTMAN ST
ROCKWALL, TX 75087

HOOVER LINDA WEST-
716 HARTMAN STREET
ROCKWALL, TX 75087

SIMMONS JOHN V & JOAN M
802 AGAPE CIR
ROCKWALL, TX 75087

WARDELL JOHN P AND JULIE ANN C
880 IVY LANE
ROCKWALL, TX 75087

SOLID ROCK HOLDINGS LLC
904 CAMPTON CT
ROCKWALL, TX 75032

TUTTLE LEON ETUX
963 W Yellowjacket Ln Apt 122
Rockwall, TX 75087

DEL BOSQUE RODOLFO
PO BOX 2437
ROCKWALL, TX 75087

DEL BOSQUE RODOLFO
PO BOX 2437
ROCKWALL, TX 75087

AUBE JEAN-PAUL III
PO BOX 868
ROCKWALL, TX 75087

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

Property Owner and/or Resident of the City of Rockwall:

You are hereby notified that the City of Rockwall Planning and Zoning Commission and City Council will consider the following application:

Z2025-015: Specific Use Permit (SUP) for Residential Infill and a Guest Quarters/ Secondary Living Unit

Hold a public hearing to discuss and consider a request by Javier Silva of JMS Custom Homes for the approval of a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision* and a *Guest Quarters/Secondary Living Unit* on a 0.42-acre parcel of land identified as Lot 2, Block 1, Shaw Addition, City of Rockwall, Rockwall County, Texas, zoned Single-Family 7 (SF-7) District, addressed as 403B S. Clark Street, and take any action necessary.

For the purpose of considering the effects of such a request, the Planning and Zoning Commission will hold a public hearing on Tuesday, April 15, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, April 21, 2025 at 6:00 PM. These hearings will be held in the City Council Chambers at City Hall, 385 S. Goliad Street.

As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Angelica Guevara
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, April 21, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

PLEASE RETURN THE BELOW FORM

Case No. Z2025-015: Specific Use Permit (SUP) for Residential Infill and a Guest Quarters/ Secondary Living Unit

Please place a check mark on the appropriate line below:

- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Name:

Address:

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

ADJACENT HOUSING ATTRIBUTES

| ADDRESS | HOUSING TYPE | YEAR BUILT | HOUSE SF | ACCESSORY BUILDING | EXTERIOR MATERIALS |
|----------------------|--------------------|------------|----------|--------------------|--------------------|
| 401 S. Clark Street | Single-Family Home | 1900 | 3,925 | 1,140 | Siding |
| 402 S. Clark Street | Single-Family Home | 1987 | 1,688 | 374 | Brick and Siding |
| 403A S. Clark Street | Single-Family Home | 1966 | 752 | N/A | Stone and Siding |
| 403B S. Clark Street | Vacant | N/A | N/A | N/A | N/A |
| 404 S. Clark Street | Single-Family Home | 1983 | 1,529 | N/A | Brick |
| 405 S. Clark Street | Single-Family Home | 2006 | 2,783 | N/A | Brick and Stone |
| 406 S. Clark Street | Single-Family Home | 1990 | 1,588 | 144 | Brick |
| 407 S. Clark Street | Single-Family Home | 2019 | 5,408 | N/A | Siding |
| 408 S. Clark Street | Single-Family Home | 2006 | 3,251 | N/A | Brick and Stone |
| 410 S. Clark Street | Single-Family Home | 1989 | 1,772 | N/A | Siding |
| 501 S. Clark Street | Single-Family Home | 2021 | 4,561 | N/A | Siding |
| 503 S. Clark Street | Single-Family Home | 2022 | 4,990 | N/A | Siding |
| AVERAGES: | | 1990 | 2,932 | 553 | |



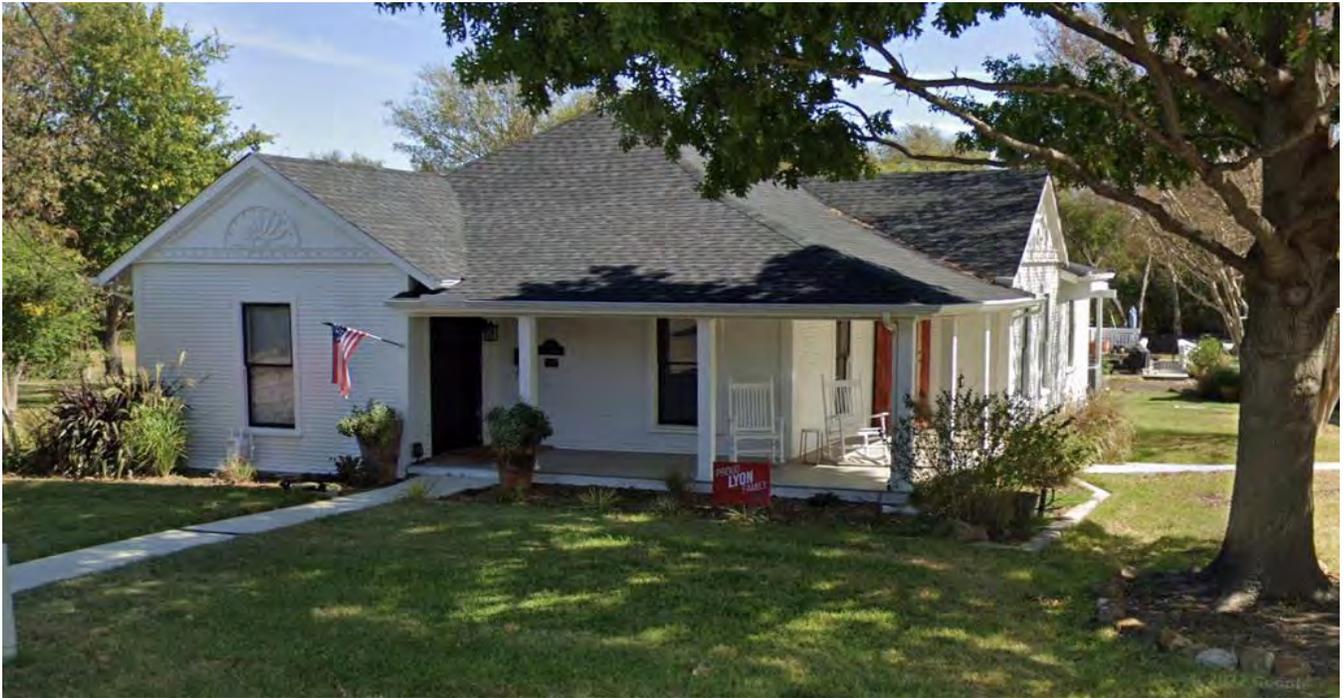
CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



401 S. Clark Street



402 S. Clark Street



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

PLANNING AND ZONING DEPARTMENT

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PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

403A S. Clark Street



403B S. Clark Street



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



404 S. Clark Street



405 S. Clark Street



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

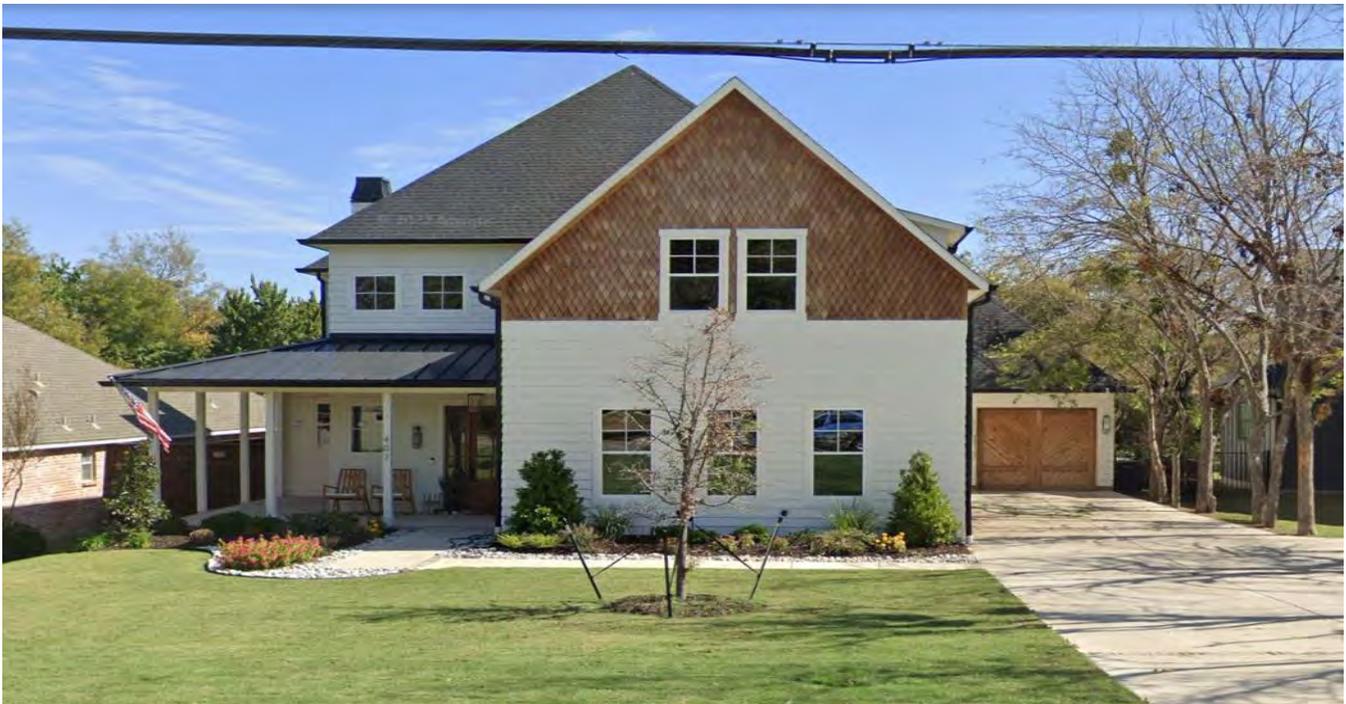
PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



406 S. Clark Street



407 S. Clark Street



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

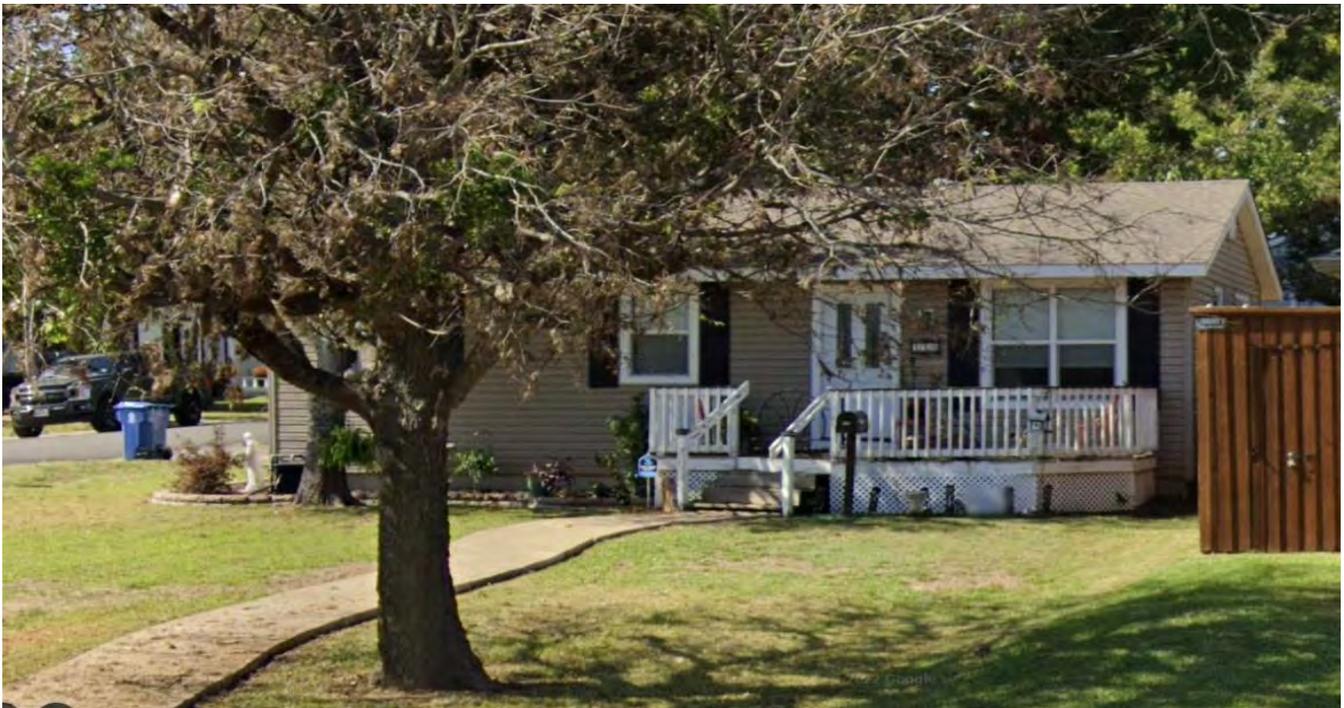
PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



408 S. Clark Street



410 S. Clark Street



CITY OF ROCKWALL

HOUSING ANALYSIS FOR CASE NO. Z2025-015

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM



501 S. Clark Street



503 S. Clark Street

CITY OF ROCKWALL

ORDINANCE NO. 25-XX

SPECIFIC USE PERMIT NO. S-3XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING THE UNIFIED DEVELOPMENT CODE (UDC) [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) FOR RESIDENTIAL INFILL IN AN ESTABLISHED SUBDIVISION TO ALLOW THE CONSTRUCTION OF A SINGLE-FAMILY HOME AND GUEST QUARTERS/SECONDARY LIVING UNIT ON A 0.42-ACRE PARCEL OF LAND, IDENTIFIED AS LOT 2, BLOCK 1, SHAW ADDITION, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; AND MORE SPECIFICALLY DESCRIBED AND DEPICTED IN EXHIBIT 'A' OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City has received a request by Javier Silva of JMS Custom Homes for the approval of a *Specific Use Permit (SUP)* for *Residential Infill in an Established Subdivision and Guest Quarters/Secondary Living Unit* on a 0.42-acre parcel of land identified as Lot 2, Block 1, Shaw Addition, City of Rockwall, Rockwall County, Texas, zoned Single-Family 7 (SF-7) District, addressed as 403B S, Clark Street, and being more specifically described and depicted in *Exhibit 'A'* of this ordinance, which herein after shall be referred to as the *Subject Property* and incorporated by reference herein; and

WHEREAS, the Planning and Zoning Commission of the City of Rockwall and the governing body of the City of Rockwall, in compliance with the laws of the State of Texas and the ordinances of the City of Rockwall, have given the requisite notices by publication and otherwise, and have held public hearings and afforded a full and fair hearing to all property owners generally, and to all persons interested in and situated in the affected area and in the vicinity thereof, the governing body in the exercise of its legislative discretion has concluded that the Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall should be amended as follows:

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Rockwall, Texas;

SECTION 1. That the Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall, as heretofore amended, be and the same is hereby amended so as to grant a Specific Use Permit (SUP) for *Residential Infill in an Established Subdivision* and a *Guest Quarters/Secondary Living Unit* in accordance with Article 04, *Permissible Uses*, of the Unified Development Code (UDC) [Ordinance No. 20-02] on the *Subject Property*; and,

SECTION 2. That the Specific Use Permit (SUP) shall be subject to the requirements set forth in Subsection 03.01, *General Residential District Standards*, and Subsection 03.09, *Single-Family 7 (SF-7) District*, of Article 05, *District Development Standards*, of the Unified Development Code (UDC) [Ordinance No. 20-02] -- as heretofore amended and may be amended in the future -- and

with the following conditions:

2.1 OPERATIONAL CONDITIONS

The following conditions pertain to the construction of a single-family home on the *Subject Property* and conformance to these operational conditions are required:

- 1) The development of the *Subject Property* shall generally conform to the *Residential Plot Plan* as depicted in *Exhibit 'B'* of this ordinance.
- 2) The construction of a *single-family home and guest quarters/secondary living unit* on the *Subject Property* shall generally conform to the *Building Elevations* depicted in *Exhibit 'C'* of this ordinance.
- 3) The *Guest Quarters/Secondary Living Unit* shall not exceed a maximum of 630 SF.
- 4) The *Guest Quarters/Secondary Living Unit* shall not incorporate full kitchen facilities.
- 5) The *Guest Quarters/Secondary Living Unit* shall not be sold or conveyed separately from the single-family home without meeting the requirements of the zoning district and subdivision ordinance.
- 6) Once construction of the single-family home has been completed, inspected, and accepted by the City of Rockwall, this Specific Use Permit (SUP) shall expire, and no further action by the property owner shall be required.

2.2 COMPLIANCE

Approval of this ordinance in accordance with Subsection 02.02, *Specific Use Permits (SUP)* of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC) will require the *Subject Property* to comply with the following:

- 1) Upon obtaining a *Building Permit*, should the contractor operating under the guidelines of this ordinance fail to meet the minimum operational requirements set forth herein and outlined in the Unified Development Code (UDC), the City may (*after proper notice*) initiate proceedings to revoke the Specific Use Permit (SUP) in accordance with Subsection 02.02(F), *Revocation*, of Article 11, *Development Applications and Revision Procedures*, of the Unified Development Code (UDC) [*Ordinance No. 20-02*].

SECTION 3. That the official zoning map of the City be corrected to reflect the changes in zoning described herein.

SECTION 4. That all ordinances of the City of Rockwall in conflict with the provisions of this ordinance be, and the same are hereby repealed to the extent of that conflict.

SECTION 5. Any person, firm, or corporation violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction shall be punished by a penalty of fine not to exceed the sum of *TWO THOUSAND DOLLARS (\$2,000.00)* for each offence and each and every day such offense shall continue shall be deemed to constitute a separate offense.

SECTION 6. If any section or provision of this ordinance or the application of that section or provision to any person, firm, corporation, situation or circumstance is for any reason judged invalid,

the adjudication shall not affect any other section or provision of this ordinance or the application of any other section or provision to any other person, firm, corporation, situation or circumstance, and the City Council declares that it would have adopted the valid portions and applications of the ordinance without the invalid parts and to this end the provisions of this ordinance shall remain in full force and effect.

SECTION 7. That this ordinance shall take effect immediately from and after its passage.

**PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS,
THIS THE 5th DAY OF MAY, 2025.**

Trace Johannesen, *Mayor*

ATTEST:

Kristy Teague, *City Secretary*

APPROVED AS TO FORM:

Frank J. Garza, *City Attorney*

1st Reading: April 21, 2025

2nd Reading: May 5, 2025

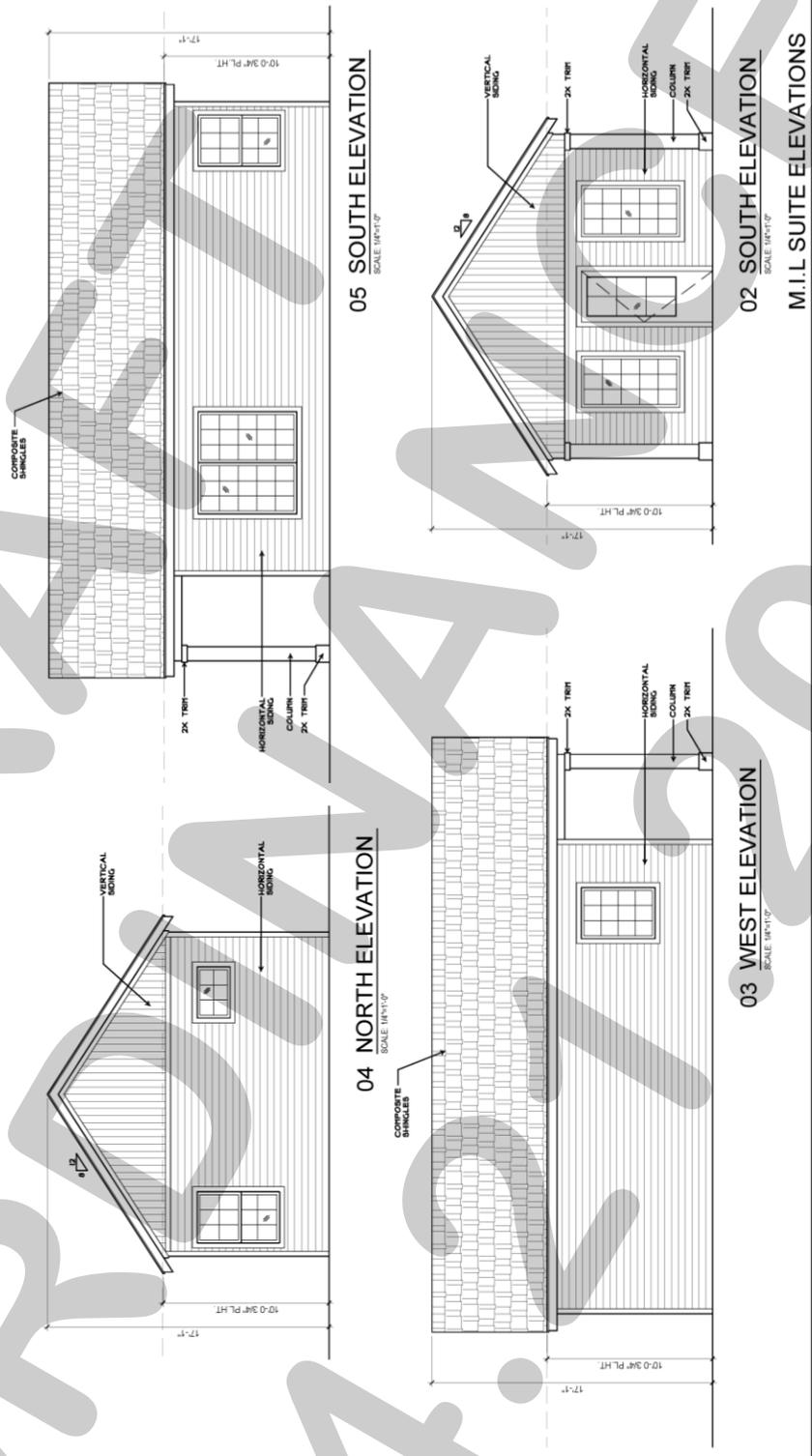
Exhibit 'A':
Location Map

Address: 403B S. Clark Street

Legal Description: Lot 2, Block 1, Shaw Addition



Exhibit 'C':
Building Elevations



M.I.L SUITE ELEVATIONS



CITY OF ROCKWALL

CITY COUNCIL CASE MEMO

PLANNING AND ZONING DEPARTMENT

385 S. GOLIAD STREET • ROCKWALL, TX 75087

PHONE: (972) 771-7745 • EMAIL: PLANNING@ROCKWALL.COM

TO: Mayor and City Council
DATE: April 21, 2025
APPLICANT: Ryan Joyce
CASE NUMBER: Z2025-016; *Specific Use Permit (SUP) for an Accessory Building at 2201 Sanderson Lane*

SUMMARY

Hold a public hearing to discuss and consider a request by Ryan Joyce for the approval of a Specific Use Permit (SUP) for an *Accessory Building* on a 2.71-acre parcel of land identified as Lot 6, Block B, Northgate Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 88 (PD-88) [*Ordinance No. 19-26*] for Single-Family 1 (SF-1) District land uses, addressed as 2201 Sanderson Lane, and take any action necessary.

BACKGROUND

The subject property was annexed into the City of Rockwall on December 1, 2008 by *Ordinance No. 08-66* [*Case No. A2008-002*]. At the time of annexation, the subject property was zoned Agricultural (AG) District. On July 1, 2019, the City Council approved a zoning change [*Case No. Z2019-012; Ordinance No. 19-26*] that establish the subject property as part of Planned Development District 88 (PD-88) for Single-Family 1 (SF-1) District land uses. On August 13, 2019, the Planning and Zoning Commission approved a PD site plan [*Case No. SP2019-028*] for the Northgate Subdivision, which consist of 40 residential lots. On August 19, 2019, the City Council approved a preliminary plat [*Case No. P2019-029*] and a master plat [*Case No. P2019-030*] for the Northgate Subdivision. On July 20, 2020, the City Council approved a final plat [*Case No. P2020-022*] that establish the subject property as Lot 6, Block B, Northgate Addition. According to the Rockwall Central Appraisal District (RCAD) there is an existing 7,441 SF single-family that was constructed in 2022 on the subject property.

PURPOSE

The applicant -- *Ryan Joyce* -- is requesting approval of a Specific Use Permit (SUP) for the purpose of allowing the construction of a 5,300 SF *Accessory Building* on the subject property.

ADJACENT LAND USES AND ACCESS

The subject property is located at 2201 Sanderson Lane. The land uses adjacent to the subject property are as follows:

North: Directly north of the subject property are five (5) parcels of land (*i.e. Lots 1-5, Block B, Northgate Addition*) zoned Planned Development District 88 (PD-88) for Single-Family 1 (SF-1) District land uses. Beyond this is Clem Road, which is identified as a *Minor Collector* on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan. Following this is the Mustang Acres Subdivision, which consists of six (6) lots zoned for residential land uses.

South: Directly south of the subject property is the Corporate Limits for the City of Rockwall. Beyond this are six (6) tracts of land situated within Rockwall County.

East: Directly east of the subject property is the remainder of the Northgate Subdivision, zoned Planned Development District 88 (PD-88) for Single-Family 1 (SF-1) District land uses. Beyond this is the Corporate Limits for the City of Rockwall. Following this is N. Stodghill Road, which is identified as a A4D (*i.e. arterial, four [4] lane, divided roadway*) on the Master Thoroughfare Plan contained in the OURHometown Vision 2040 Comprehensive Plan.

West: Directly west of the subject property is a 19.06-acre tract of land (i.e. Tract 24 of the W. M. Dalton Survey, Abstract No. 72) zoned Agricultural (AG) District. Beyond this is the Winding Creek Subdivision, which consists of 132 residential lots zoned Planned Development District 91 (PD-91) for Single-Family 16 (SF-16) District land uses.

MAP 1: LOCATION MAP
YELLOW: SUBJECT PROPERTY



CHARACTERISTICS OF THE PROJECT

The applicant is requesting approval of a Specific Use Permit (SUP) for an *Accessory Building* on the subject property. The structure is a total of 5,300 SF in size, where the enclosed building is 4,000 SF and the canopy is 1,300 SF. The accessory structure is situated on a concrete foundation, has an approximate total height of 24.42-feet or 19.21-feet at the midpoint of the roof, and incorporates a 3:12 roof pitch. The proposed building elevations provided by the applicant indicate the front façade will be faced in stone, the front columns for the canopy will have stone footings, and there will a stone wainscot on the side facades.

CHARACTERISTICS OF THE REQUEST AND CONFORMANCE TO THE CITY'S CODES

According to Article 13, *Definitions*, of the Unified Development Code (UDC) a *Residential Accessory Building or Structure* is defined as “(a) supplementary structure or building on a residential property that is secondary to the primary dwelling and serves a supportive or complementary function. These structures are typically used for purposes such as storage, recreation, or housing equipment and are ancillary to the primary residential use of the property.” In addition, according to Subsection 07.04, *Accessory Structure Development Standards*, of Article 05, *District Development Standards*, of the Unified Development Code (UDC) an *Accessory Building* in a Single-Family (SF-1) District is permitted by-right up to 144 SF and 15-feet in height. In this case, the proposed *Accessory Building* is exceeding both the size and height requirements. The proposed *Accessory Building* is approximately 5,300 SF in size and 24.42-feet in total height. That being said, the height of an *Accessory Building* with a pitched roof is taken at the midpoint of the roof pitch. Given this, the height of the proposed *Accessory Building* is 19.21-feet.

According to Planned Development District 88 (PD-88) [Ordinance No. 19-26], “(t)he minimum masonry requirement for the exterior façades of all buildings shall be 80%” and “(a) minimum of an 8:12 roof pitch is required on all structures...” In this case,

the applicant is requesting to utilize stone on the front façade of the *Accessory Building*, provide a stone footing for the front columns, provide a stone wainscot on the sides of the *Accessory Building*, and utilize a 3:12 roof pitch. The applicant has indicated that the lower roof pitch is being utilized in order to keep the "... height of the building below the tree line." The City Council pending a recommendation from the Planning and Zoning Commission shall consider the material exception and roof pitch exception as part of the Specific Use Permit (SUP) request.

In summary, the applicant is requesting approval of an *Accessory Building* that exceeds the maximum permissible size by 5,156 SF, exceeds the maximum height by 4.21-feet, utilizes less than 80% masonry materials, and utilizes less than an 8:12 roof pitch. If the applicant's Specific Use Permit (SUP) is approved, staff has included operational conditions in the Specific Use Permit (SUP) ordinance that tie down the size, height, and general architecture of the proposed structure. With this being said, the approval of a Specific Use Permit (SUP) request is a discretionary decision for the City Council pending a recommendation from the Planning and Zoning Commission.

NOTIFICATIONS

On March 18, 2025, staff mailed 24 property owner notifications to property owners and occupants within 500-feet of the subject property. There were no Homeowner's Associations (HOAs) or Neighborhood Organizations within 1,500-feet of the subject property participating in the Neighborhood Notification Program. Additionally, staff posted a sign on the subject property, and advertised the public hearings in the Rockwall Herald Banner as required by the Unified Development Code (UDC). At the time this report was drafted, staff has not received any notices in reference to the applicant's request.

CONDITIONS OF APPROVAL

If the City Council chooses to approve the applicant's request for a Specific Use Permit (SUP) to construct an *Accessory Building* on the subject property, then staff would propose the following conditions of approval:

- (1) The applicant shall be responsible for maintaining compliance with the operational conditions contained in the Specific Use Permit (SUP) ordinance and which are detailed as follows:
 - (a) The development of the *Accessory Building* shall generally conform to the Site Plan as depicted in *Exhibit 'B'* of the Specific Use Permit (SUP) ordinance; and,
 - (b) The *Accessory Building* shall generally conform to the Building Rendering as depicted in *Exhibit 'C'* of the Specific Use Permit (SUP) ordinance; and,
 - (c) The *Accessory Building* shall not exceed a maximum size of 5,400 SF; and,
 - (d) The *Accessory Building* shall not be sold or conveyed separately from the single-family home without meeting the requirements of the zoning district and subdivision ordinance; and,
 - (e) No additional *Accessory Buildings* may be constructed on the *Subject Property*.
- (2) Any construction resulting from the approval of this Specific Use Permit (SUP) request shall conform to the requirements set forth by the Unified Development Code (UDC), the International Building Code (IBC), the Rockwall Municipal Code of Ordinances, city adopted engineering and fire codes and with all other applicable regulatory requirements administered and/or enforced by the state and federal government.

PLANNING AND ZONING COMMISSION

On April 15, 2025, the Planning and Zoning Commission approved a motion to recommend approval of the SUP by a vote of 6-0, with Commissioner Thompson absent.



DEVELOPMENT APPLICATION

City of Rockwall
Planning and Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087

STAFF USE ONLY

PLANNING & ZONING CASE NO.

NOTE: THE APPLICATION IS NOT CONSIDERED ACCEPTED BY THE CITY UNTIL THE PLANNING DIRECTOR AND CITY ENGINEER HAVE SIGNED BELOW.

DIRECTOR OF PLANNING:

CITY ENGINEER:

PLEASE CHECK THE APPROPRIATE BOX BELOW TO INDICATE THE TYPE OF DEVELOPMENT REQUEST [SELECT ONLY ONE BOX]:

PLATTING APPLICATION FEES:

- MASTER PLAT (\$100.00 + \$15.00 ACRE) ¹
- PRELIMINARY PLAT (\$200.00 + \$15.00 ACRE) ¹
- FINAL PLAT (\$300.00 + \$20.00 ACRE) ¹
- REPLAT (\$300.00 + \$20.00 ACRE) ¹
- AMENDING OR MINOR PLAT (\$150.00)
- PLAT REINSTATEMENT REQUEST (\$100.00)

SITE PLAN APPLICATION FEES:

- SITE PLAN (\$250.00 + \$20.00 ACRE) ¹
- AMENDED SITE PLAN/ELEVATIONS/LANDSCAPING PLAN (\$100.00)

ZONING APPLICATION FEES:

- ZONING CHANGE (\$200.00 + \$15.00 ACRE) ¹
- SPECIFIC USE PERMIT (\$200.00 + \$15.00 ACRE) ^{1 & 2} 5.22
- PD DEVELOPMENT PLANS (\$200.00 + \$15.00 ACRE) ¹

OTHER APPLICATION FEES:

- TREE REMOVAL (\$75.00)
- VARIANCE REQUEST/SPECIAL EXCEPTIONS (\$100.00) ²

NOTES:

¹: IN DETERMINING THE FEE, PLEASE USE THE EXACT ACREAGE WHEN MULTIPLYING BY THE PER ACRE AMOUNT. FOR REQUESTS ON LESS THAN ONE ACRE, ROUND UP TO ONE (1) ACRE.
²: A \$1,000.00 FEE WILL BE ADDED TO THE APPLICATION FEE FOR ANY REQUEST THAT INVOLVES CONSTRUCTION WITHOUT OR NOT IN COMPLIANCE TO AN APPROVED BUILDING PERMIT.

PROPERTY INFORMATION [PLEASE PRINT]

ADDRESS 2201 Sanderson Ln, Rockwall, TX

SUBDIVISION North Gate

LOT 6 BLOCK B

GENERAL LOCATION intersection of Sanderson Ln and Boyett st.

ZONING, SITE PLAN AND PLATTING INFORMATION [PLEASE PRINT]

CURRENT ZONING PB

CURRENT USE SF

PROPOSED ZONING

PROPOSED USE

ACREAGE

LOTS [CURRENT]

LOTS [PROPOSED]

- SITE PLANS AND PLATS:** BY CHECKING THIS BOX YOU ACKNOWLEDGE THAT DUE TO THE PASSAGE OF HB3167 THE CITY NO LONGER HAS FLEXIBILITY WITH REGARD TO ITS APPROVAL PROCESS, AND FAILURE TO ADDRESS ANY OF STAFF'S COMMENTS BY THE DATE PROVIDED ON THE DEVELOPMENT CALENDAR WILL RESULT IN THE DENIAL OF YOUR CASE.

OWNER/APPLICANT/AGENT INFORMATION [PLEASE PRINT/CHECK THE PRIMARY CONTACT/ORIGINAL SIGNATURES ARE REQUIRED]

OWNER Michael Ryan Joyce

APPLICANT

CONTACT PERSON Ryan Joyce

CONTACT PERSON

ADDRESS 2201 Sanderson Ln
Rockwall, TX

ADDRESS

CITY, STATE & ZIP Rockwall, TX 75087

CITY, STATE & ZIP

PHONE 512-985-6280

PHONE

E-MAIL ryan@michaeljoyceproperties.com

E-MAIL

NOTARY VERIFICATION [REQUIRED]

BEFORE ME, THE UNDERSIGNED AUTHORITY, ON THIS DAY PERSONALLY APPEARED Michael R Joyce [OWNER] THE UNDERSIGNED, WHO STATED THE INFORMATION ON THIS APPLICATION TO BE TRUE AND CERTIFIED THE FOLLOWING:

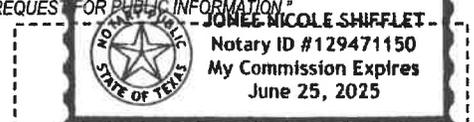
"I HEREBY CERTIFY THAT I AM THE OWNER FOR THE PURPOSE OF THIS APPLICATION; ALL INFORMATION SUBMITTED HEREIN IS TRUE AND CORRECT; AND THE APPLICATION FEE OF \$ 278.30 TO COVER THE COST OF THIS APPLICATION, HAS BEEN PAID TO THE CITY OF ROCKWALL ON THIS THE 14 DAY OF March 2025. BY SIGNING THIS APPLICATION, I AGREE THAT THE CITY OF ROCKWALL (I.E. "CITY") IS AUTHORIZED AND PERMITTED TO PROVIDE INFORMATION CONTAINED WITHIN THIS APPLICATION TO THE PUBLIC. THE CITY IS ALSO AUTHORIZED AND PERMITTED TO REPRODUCE ANY COPYRIGHTED INFORMATION SUBMITTED IN CONJUNCTION WITH THIS APPLICATION, IF SUCH REPRODUCTION IS ASSOCIATED OR IN RESPONSE TO A REQUEST FOR PUBLIC INFORMATION."

GIVEN UNDER MY HAND AND SEAL OF OFFICE ON THIS THE 14 DAY OF March 2024

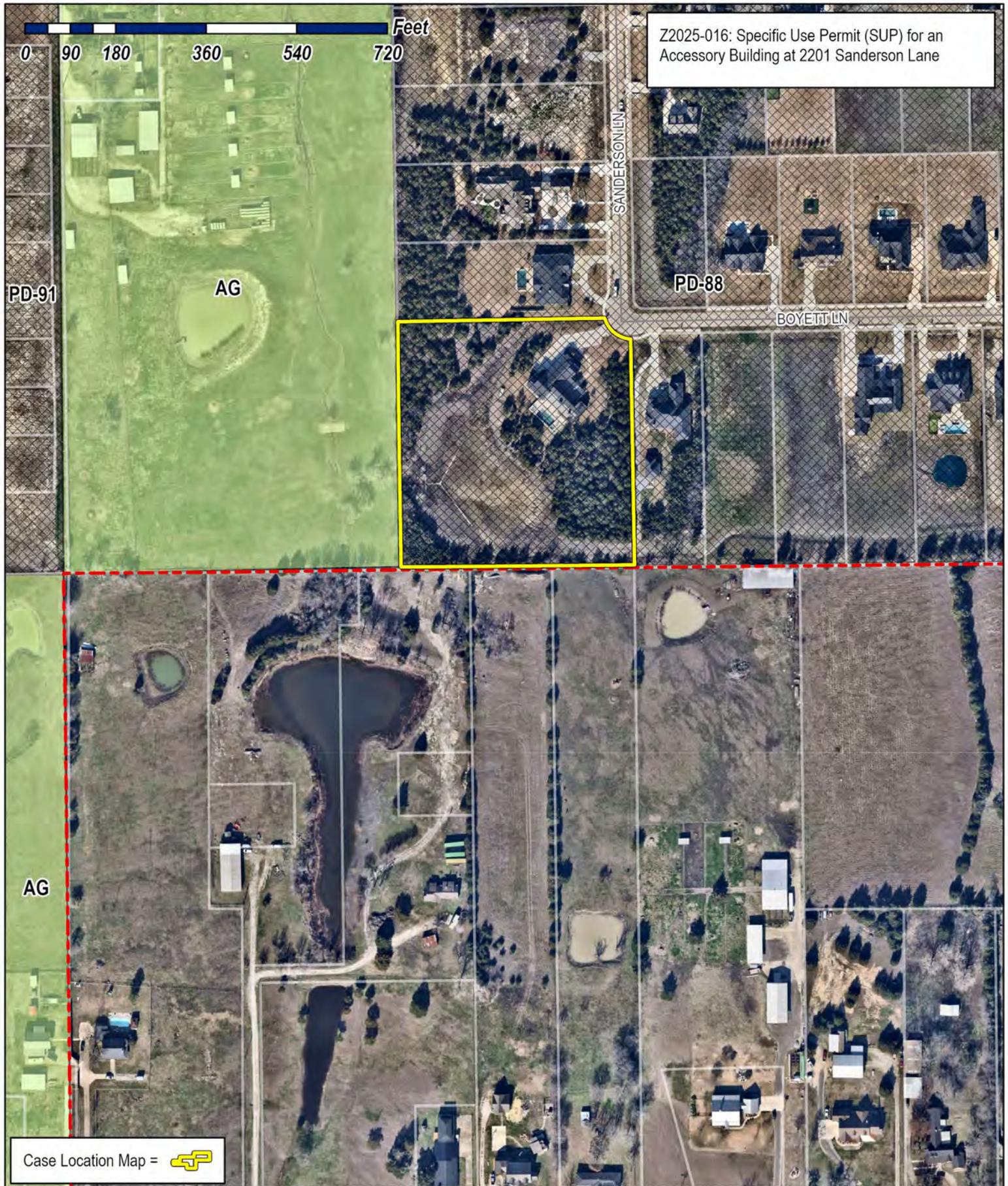
OWNER'S SIGNATURE

NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS

Joni Nicole Shifflet



MY COMMISSION EXPIRES 6-25-25



Z2025-016: Specific Use Permit (SUP) for an Accessory Building at 2201 Sanderson Lane

0 90 180 360 540 720 Feet

PD-91

AG

PD-88

BOYETT LN

SANDERSON LN

AG

Case Location Map = 



City of Rockwall

Planning & Zoning Department
 385 S. Goliad Street
 Rockwall, Texas 75087
 (P): (972) 771-7745
 (W): www.rockwall.com

The City of Rockwall GIS maps are continually under development and therefore subject to change without notice. While we endeavor to provide timely and accurate information, we make no guarantees. The City of Rockwall makes no warranty, express or implied, including warranties of merchantability and fitness for a particular purpose. Use of the information is the sole responsibility of the user.

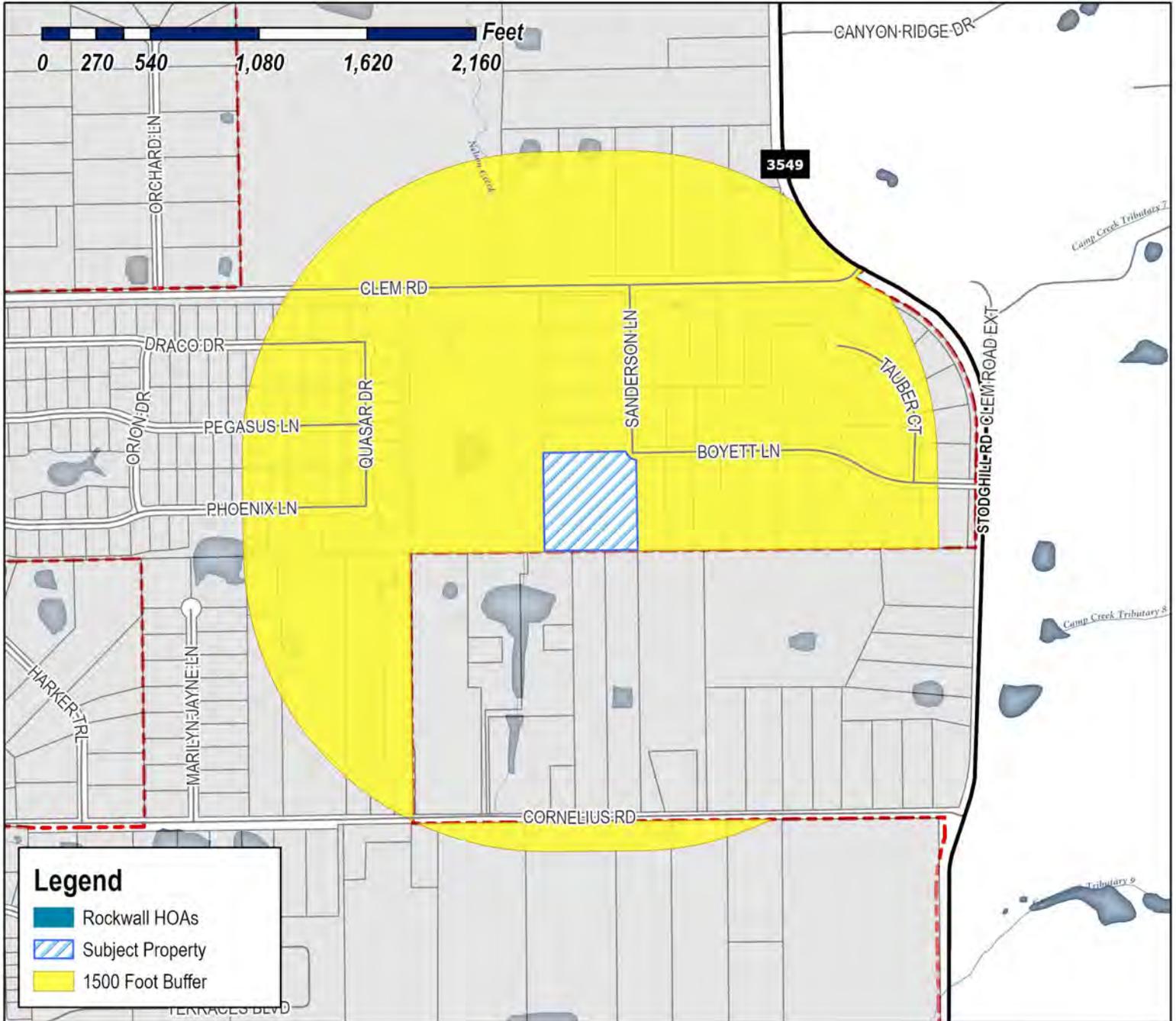




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Case Number: Z2025-016
Case Name: SUP for an Accessory Building
Case Type: Zoning
Zoning: Planned Development District 88 (PD-88)
Case Address: 2201 Sanderson Lane



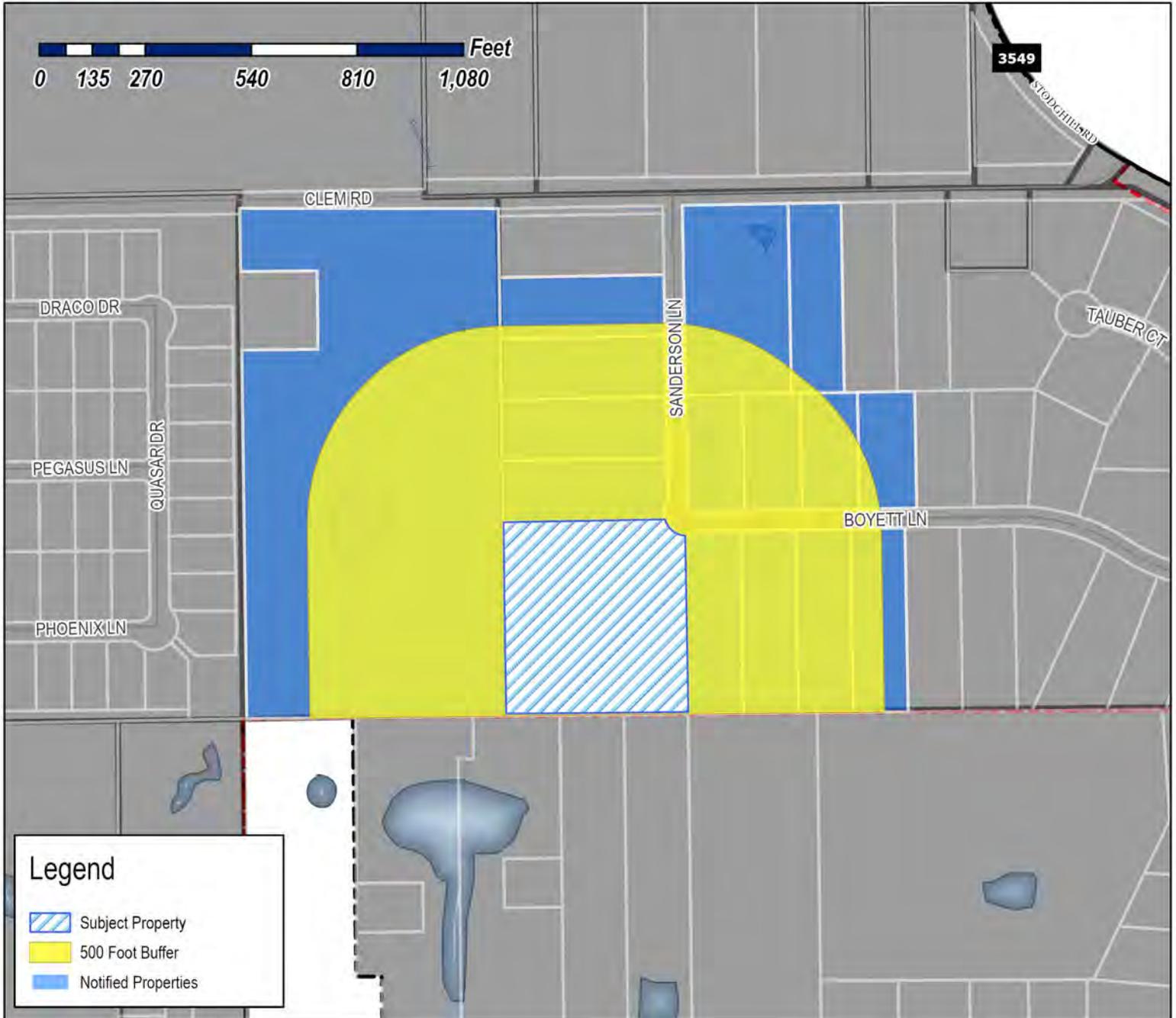
Date Saved: 3/14/2025
 For Questions on this Case Call (972) 771-7745



City of Rockwall

Planning & Zoning Department
385 S. Goliad Street
Rockwall, Texas 75087
(P): (972) 771-7745
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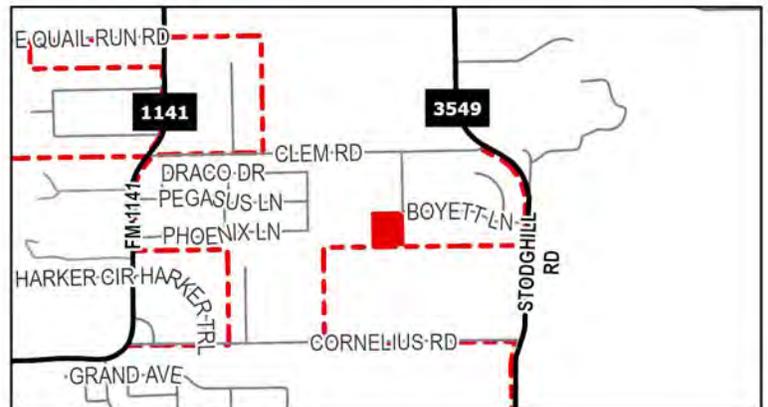
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Zoning: Planned Development District 88 (PD-88)
Case Address: 2201 Sanderson Lane

Date Saved: 3/14/2025

For Questions on this Case Call: (972) 771-7745



MCCALLUM CRAIG WILLIAM
1983 N STODGHILL RD
ROCKWALL, TX 75087

JOYCE MICHAEL RYAN AND MEREDITH MCLEROY
2201 SANDERSON LN
ROCKWALL, TX 75087

SANDERSON PERRY AND AMY
2207 SANDERSON LN
ROCKWALL, TX 75087

RESIDENT
2213 SANDERSON LN
ROCKWALL, TX 75087

RESIDENT
2219 SANDERSON LN
ROCKWALL, TX 75087

RESIDENT
2225 SANDERSON LN
ROCKWALL, TX 75087

OWNBY MITCH AND SANDRA
255 COUNTRY CLUB DR
HEATH, TX 75032

WALRAVEN KEITH & MEREDITH
402 FLORENCE DR
FATE, TX 75087

RESIDENT
611 CLEM RD
ROCKWALL, TX 75087

RECHTIENE JOSEPH S AND LARISA A
619 ELEANOR DRIVE
FATE, TX 75087

MASON RICHARD L
682 CANNON DRIVE
ROCKWALL, TX 75087

BATES CHARLES
7540 EDNA COURT #5509
PLANO, TX 75024

COUCH DAVID AND JULIE
803 BOYETT LN
ROCKWALL, TX 75087

RESIDENT
804 BOYETT LN
ROCKWALL, TX 75087

RESIDENT
807 BOYETT LN
ROCKWALL, TX 75087

PAYNE ADAM J AND ELIZABETH A
808 BOYETT LN
ROCKWALL, TX 75087

RESIDENT
813 BOYETT LN
ROCKWALL, TX 75087

WOLFF RYAN & RACHEL
814 BOYETT LN
ROCKWALL, TX 75087

RESIDENT
819 BOYETT LN
ROCKWALL, TX 75087

MEREDITH WILLIAM AND AMBER
820 BOYETT LN
ROCKWALL, TX 75087

TUCKER JANA
835 CLEM RD
ROCKWALL, TX 75087

CONFIDENTIAL
89 Stone Hinge Dr
Fairview, TX 75069

STANLEY STEVEN B AND ROBIN C
891 CLEM RD
ROCKWALL, TX 75087

NAKAMURA DEREK & CAITLYN
9620 COLQUITT RD
TERRELL, TX 75160

PUBLIC NOTICE



CITY OF ROCKWALL
PLANNING AND ZONING DEPARTMENT
PHONE: (972) 771-7745
EMAIL: PLANNING@ROCKWALL.COM

Property Owner and/or Resident of the City of Rockwall:

You are hereby notified that the City of Rockwall Planning and Zoning Commission and City Council will consider the following application:

Z2025-016: Specific Use Permit (SUP) for an Accessory Building

Hold a public hearing to discuss and consider a request by Ryan Joyce for the approval of a *Specific Use Permit (SUP)* for an *Accessory Building* on a 2.71-acre parcel of land identified as Lot 6, Block B, Northgate Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 88 (PD-88) [Ordinance No. 19-26] for Single-Family 1 (SF-1) District land uses, addressed as 2201 Sanderson Lane, and take any action necessary.

For the purpose of considering the effects of such a request, the Planning and Zoning Commission will hold a public hearing on Tuesday, April 15, 2025 at 6:00 PM, and the City Council will hold a public hearing on Monday, April 21, 2025 at 6:00 PM. These hearings will be held in the City Council Chambers at City Hall, 385 S. Goliad Street.

As an interested property owner, you are invited to attend these meetings. If you prefer to express your thoughts in writing please return the form to:

Henry Lee
Rockwall Planning and Zoning Dept.
385 S. Goliad Street
Rockwall, TX 75087

You may also email your comments to the Planning Department at planning@rockwall.com. If you choose to email the Planning Department please include your name and address for identification purposes.

Your comments must be received by Monday, April 21, 2025 at 4:00 PM to ensure they are included in the information provided to the City Council.

Sincerely,

Ryan Miller, AICP
Director of Planning & Zoning



MORE INFORMATION ON THIS CASE CAN BE FOUND AT: <https://sites.google.com/site/rockwallplanning/development/development-cases>

PLEASE RETURN THE BELOW FORM

Case No. Z2025-016: Specific Use Permit (SUP) for an Accessory Building

Please place a check mark on the appropriate line below:

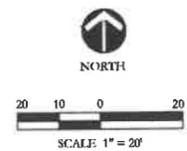
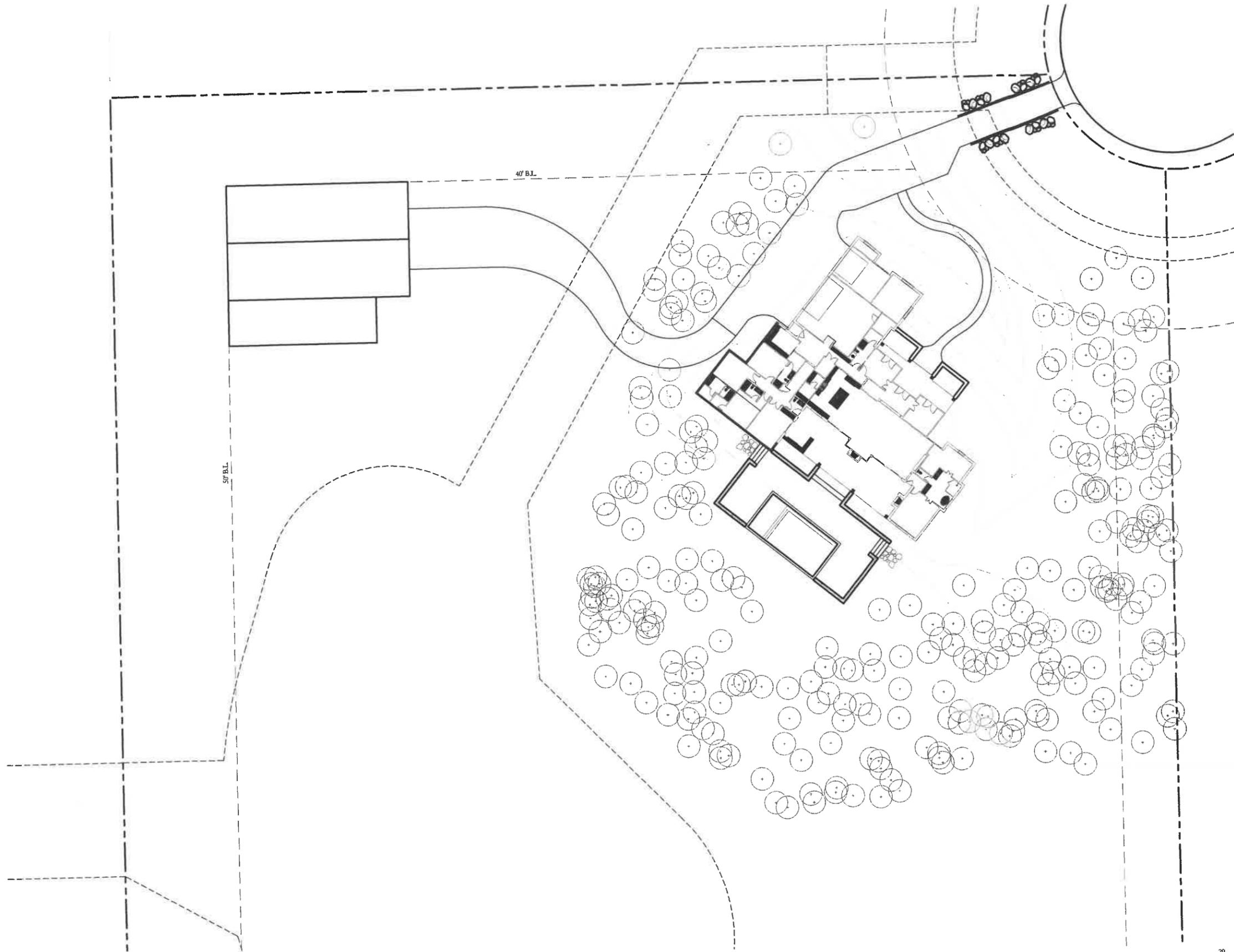
- I am in favor of the request for the reasons listed below.
- I am opposed to the request for the reasons listed below.

Name:

Address:

Tex. Loc. Gov. Code, Sec. 211.006 (d) If a proposed change to a regulation or boundary is protested in accordance with this subsection, the proposed change must receive, in order to take effect, the affirmative vote of at least three-fourths of all members of the governing body. The protest must be written and signed by the owners of at least 20 percent of either: (1) the area of the lots or land covered by the proposed change; or (2) the area of the lots or land immediately adjoining the area covered by the proposed change and extending 200 feet from that area.

PLEASE SEE LOCATION MAP OF SUBJECT PROPERTY ON THE BACK OF THIS NOTICE



2201 Sanderson Ln





Endwall B
50'





CITY OF ROCKWALL

ORDINANCE NO. 25-XX

SPECIFIC USE PERMIT NO. S-3XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS, AMENDING PLANNED DEVELOPMENT DISTRICT 88 (PD-88) AND THE UNIFIED DEVELOPMENT CODE (UDC) [ORDINANCE NO. 20-02] OF THE CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS, AS PREVIOUSLY AMENDED, SO AS TO GRANT A SPECIFIC USE PERMIT (SUP) FOR AN ACCESSORY BUILDING ON A 5.222-ACRE PARCEL OF LAND IDENTIFIED AS LOT 6, BLOCK B, NORTHGATE ADDITION, CITY OF ROCKWALL, ROCKWALL COUNTY, TEXAS; AND MORE SPECIFICALLY DEPICTED AND DESCRIBED IN EXHIBIT 'A' OF THIS ORDINANCE; PROVIDING FOR SPECIAL CONDITIONS; PROVIDING FOR A PENALTY OF FINE NOT TO EXCEED THE SUM OF TWO THOUSAND DOLLARS (\$2,000.00) FOR EACH OFFENSE; PROVIDING FOR A SEVERABILITY CLAUSE; PROVIDING FOR A REPEALER CLAUSE; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City has received a request from Ryan Joyce for the approval of a *Specific Use Permit (SUP)* for an *Accessory Building* on a 5.222-acre parcel of land identified as Lot 6, Block B, Northgate Addition, City of Rockwall, Rockwall County, Texas, zoned Planned Development District 88 (PD-88), addressed as 2201 Sanderson Lane, and being more specifically depicted and described in *Exhibit 'A'* of this ordinance, which herein after shall be referred to as the *Subject Property* and incorporated by reference herein; and

WHEREAS, the Planning and Zoning Commission of the City of Rockwall and the governing body of the City of Rockwall, in compliance with the laws of the State of Texas and the ordinances of the City of Rockwall, have given the requisite notices by publication and otherwise, and have held public hearings and afforded a full and fair hearing to all property owners generally, and to all persons interested in and situated in the affected area and in the vicinity thereof, the governing body in the exercise of its legislative discretion has concluded that Planned Development District 88 (PD-88) and the Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall should be amended as follows:

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Rockwall, Texas;

SECTION 1. That Planned Development District 88 (PD-88) and the Unified Development Code (UDC) [Ordinance No. 20-02] of the City of Rockwall, as heretofore amended, be and the same is hereby amended so as to grant a Specific Use Permit (SUP) for an *Accessory Building* as stipulated by Subsection 01.02, *Land Use Schedule*, of Article 04, *Permissible Uses*, of the Unified Development Code (UDC) [Ordinance No. 20-02] on the *Subject Property*; and,

SECTION 2. That the Specific Use Permit (SUP) shall be subject to the requirements set forth in Planned Development District 88 (PD-88); and Subsection 03.01, *General Residential Standards*, and Subsection 07.04, *Accessory Structure Development Standards*, of Article 05, *District Development Standards*, of the Unified Development Code (UDC) [Ordinance No. 20-02], and with the following conditions:

2.1. OPERATIONAL CONDITIONS

The following conditions pertain to the construction of an *Accessory Building* on the *Subject Property* and conformance to these conditions are required for continued operations:

- (1) The development of the *Accessory Building* shall generally conform to the Site Plan as depicted in *Exhibit 'B'* of the Specific Use Permit (SUP) ordinance; and,
- (2) The *Accessory Building* shall generally conform to the Building Rendering as depicted in *Exhibit 'C'* of the Specific Use Permit (SUP) ordinance; and,
- (3) The *Accessory Building* shall not exceed a maximum size of 5,400 SF; and,
- (4) The *Accessory Building* shall not be sold or conveyed separately from the single-family home without meeting the requirements of the zoning district and subdivision ordinance; and,
- (5) No additional *Accessory Buildings* may be constructed on the *Subject Property*.

2.2. COMPLIANCE

Approval of this ordinance in accordance with Subsection 02.02, *Specific Use Permits (SUP)* of Article 11, *Development Applications and Review Procedures*, of the Unified Development Code (UDC) will require the *Subject Property* to comply with the following:

- (1) Upon obtaining a *Building Permit*, should the contractor operating under the guidelines of this ordinance fail to meet the minimum operational requirements set forth herein and outlined in the Unified Development Code (UDC), the City may (*after proper notice*) initiate proceedings to revoke the Specific Use Permit (SUP) in accordance with Subsection 02.02(F), *Revocation*, of Article 11, *Development Applications and Revision Procedures*, of the Unified Development Code (UDC) [*Ordinance No. 20-02*].

SECTION 3. That the official zoning map of the City be corrected to reflect the changes in zoning described herein.

SECTION 4. That all ordinances of the City of Rockwall in conflict with the provisions of this ordinance be, and the same are hereby repealed to the extent of that conflict.

SECTION 5. Any person, firm, or corporation violating any of the provisions of this ordinance shall be deemed guilty of a misdemeanor and upon conviction shall be punished by a penalty of fine not to exceed the sum of *TWO THOUSAND DOLLARS (\$2,000.00)* for each offence and each and every day such offense shall continue shall be deemed to constitute a separate offense.

SECTION 6. If any section or provision of this ordinance or the application of that section or provision to any person, firm, corporation, situation or circumstance is for any reason judged invalid, the adjudication shall not affect any other section or provision of this ordinance or the application of any other section or provision to any other person, firm, corporation, situation or circumstance, and the City Council declares that it would have adopted the valid portions and applications of the ordinance without the invalid parts and to this end the provisions of this ordinance shall remain in full force and effect.

SECTION 7. That this ordinance shall take effect immediately from and after its passage.

**PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF ROCKWALL, TEXAS,
THIS THE 5th DAY OF MAY, 2025.**

Trace Johannesen, *Mayor*

ATTEST:

Kristy Teague, *City Secretary*

APPROVED AS TO FORM:

Frank J. Garza, *City Attorney*

1st Reading: April 21, 2025

2nd Reading: May 5, 2025

Exhibit 'A'
Legal Description

Address: 2201 Sanderson Lane

Legal Description: Lot 6, Block B, Northgate Addition

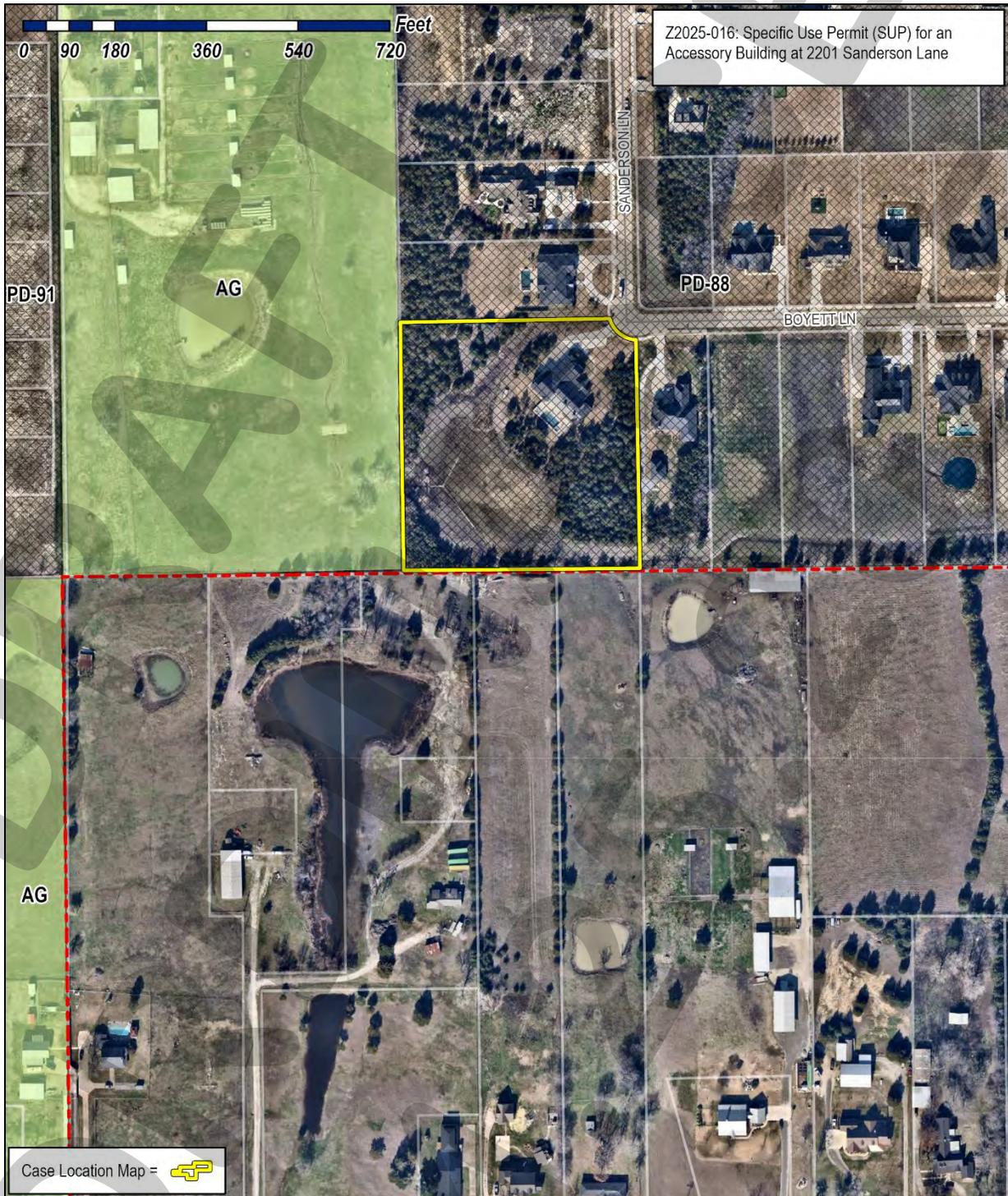


Exhibit 'B'
Site Plan

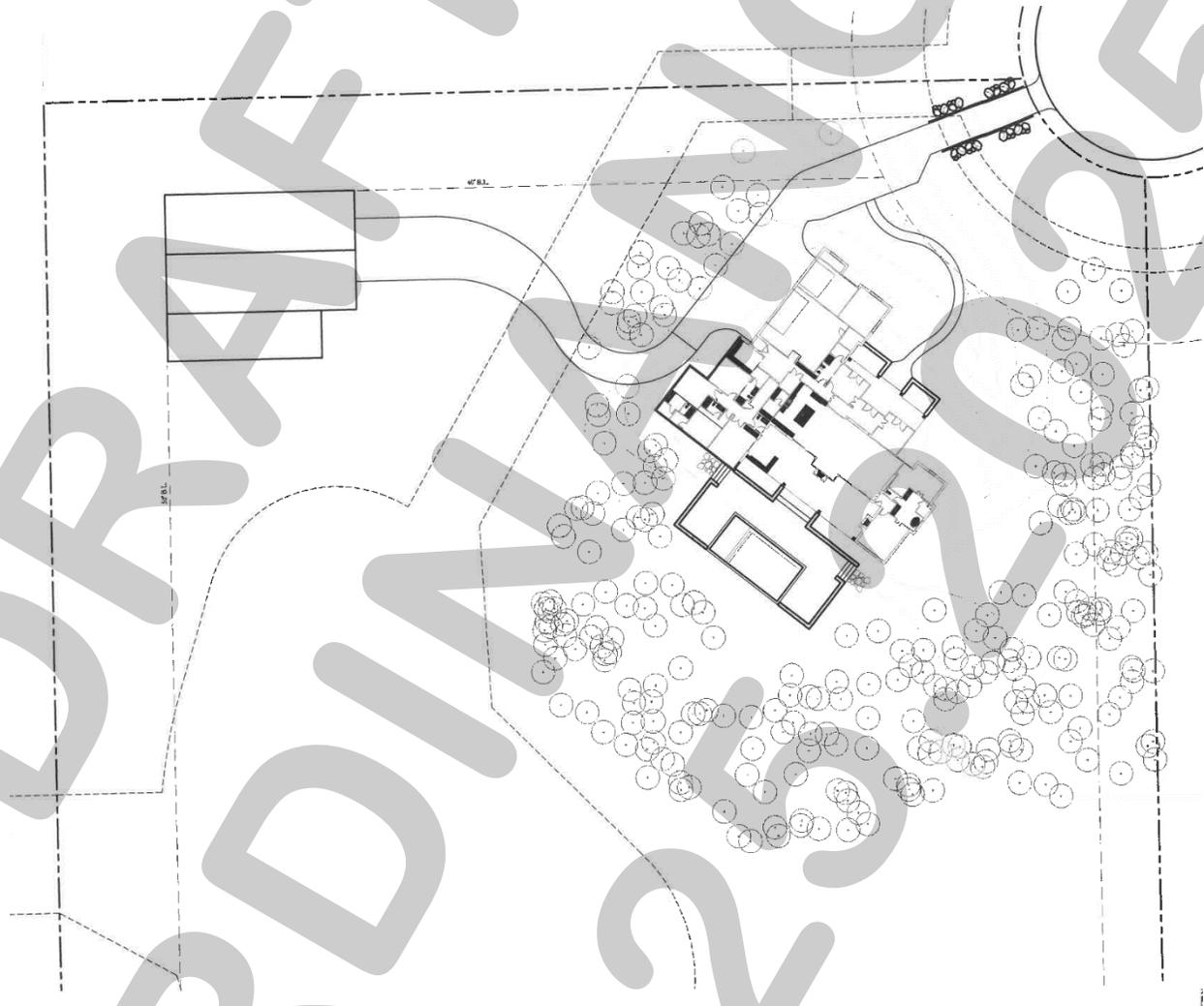


Exhibit 'C':
Building Rendering

